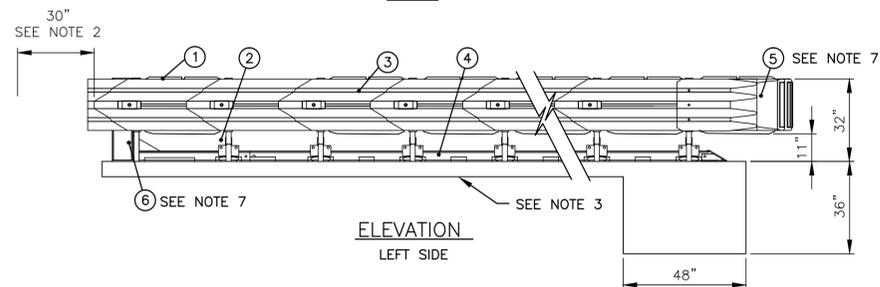
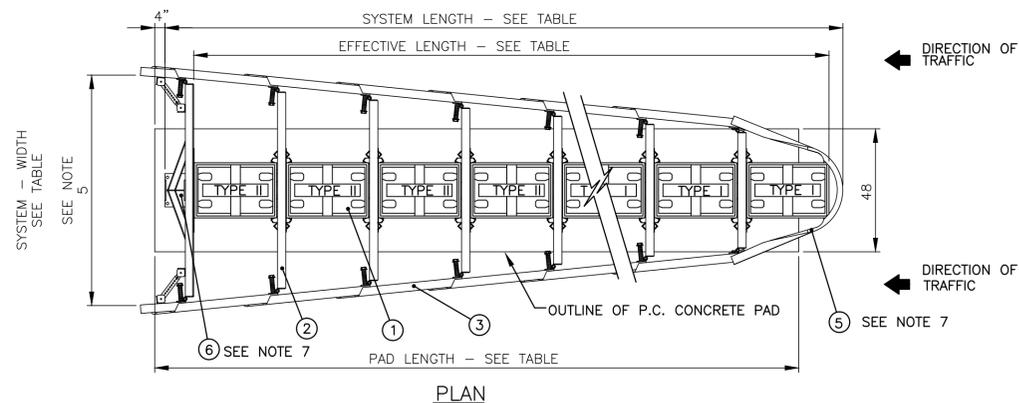


QUADGUARD SYSTEM FOR NARROW HAZARDS
N.T.S. TD70.01.02



QUADGUARD SYSTEM FOR WIDE HAZARDS
N.T.S. TD70.01.03

KEY	①	QUADGUARD CARTRIDGE	③	FENDER PANEL	⑤	NOSE ASSEMBLY
	②	DIAPHRAGM	④	MONORAIL	⑥	BACKUP

TD70.01.04

NOTES:
TD70.01.01

1. IN COMPLIANCE WITH THE AASHTO 1996 ROADSIDE DESIGN GUIDE, MANUFACTURER RECOMMENDS REMOVAL OF ALL CURBS AND ISLANDS TO ENSURE PROPER IMPACT PERFORMANCE.
2. PROVISION SHALL BE MADE FOR REAR FENDER PANELS TO SLIDE REARWARD UPON IMPACT 30" MIN.
3. 6" MIN. EINFORCED (4000 PSI) CONCRETE PAD OR 8" MIN. NON-REINFORCED (4000 PSI) PORTLAND CEMENT (P.C.) CONCRETE ROADWAY.
4. SEE THE "QUADGUARD SYSTEM DESIGN MANUAL" DEVELOPED BY TRANSPO INDUSTRIES, INC. FOR A DESCRIPTION OF ITS IMPACT PERFORMANCE CHARACTERISTICS AND DESIGN LIMITATIONS BEFORE PLACING A SYSTEM AT A GIVEN SITE.
5. WHERE NECESSARY, PROVIDE A TRANSITION FROM THE QUADGUARD SYSTEM TO THE OBJECT BEING SHIELDED.
6. UNITS OF MEASUREMENT ARE INCHES, UNLESS OTHERWISE NOTED.
7. BACKUP AND TRANSITION ASSEMBLIES NOT INCLUDED IN MODEL NUMBER.
8. THE BAY LENGTHS INDICATED IN THE TABLE ARE BASED UPON CALCULATED VALUES FOR 2000kg VEHICLES TRAVELING AT THE SPEEDS INDICATED AND HAVING ENOUGH CAPACITY TO DISSIPATE THE VEHICLES LONGITUDINAL IMPACT ENERGY.
9. FOR NOSE COVER ASSEMBLY SEE DRAWING TD70.07.

QUADGUARD SYSTEM
FOR NARROW HAZARDS
TD70.01.05

BAYS	24" WIDTH MODELS	30" WIDTH MODELS	36" WIDTH MODELS	SYSTEM LENGTH ft-in	EFFECTIVE LENGTH ft-in	PAD LENGTH ft-in	MAX DESIGN SPEED (MPH)	NO. OF CARTRIDGES	
								TYPE I (FRONT OF SYSTEM)	TYPE II (REAR OF SYSTEM)
3	QS2403*	QS3003*	QS3603*	13'-1"	11'-8"	12'-0"	44	3	1
6	QS2406*	QS3006*	QS3606*	22'-1"	20'-8"	21'-0"	62	4	3
7	QS2407*	QS3007*	QS3607*	25'-1"	23'-8"	24'-0"	65	4	4
9	QS2409*	QS3009*	QS3609*	31'-1"	29'-8"	30'-0"	71	4	6
11	QS2411*	QS3011*	QS3611*	37'-1"	35'-8"	36'-0"	75	5	7

*G=GREY OR Y=YELLOW

QUADGUARD SYSTEM
FOR WIDE HAZARDS
TD70.01.06

BAYS	69" WIDTH MODELS	90" WIDTH MODELS	SYSTEM LENGTH ft-in	EFFECTIVE LENGTH ft-in	PAD LENGTH ft-in	MAX DESIGN SPEED (MPH)	NO. OF CARTRIDGES	
							TYPE I (FRONT OF SYSTEM)	TYPE II (REAR OF SYSTEM)
3	QS6903*	QS9003*	13'-1"	11'-8"	12'-0"	44	3	1
6	QS6906*	QS9006*	22'-1"	20'-8"	21'-0"	62	4	3
7	QS6907*	QS9007*	25'-1"	23'-8"	24'-0"	65	4	4
9	QS6909*	QS9009*	31'-1"	29'-8"	30'-0"	71	4	6
11	QS6911*	QS9011*	37'-1"	35'-8"	36'-0"	75	5	7

*G=GREY OR Y=YELLOW

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

PANYNJ
Traffic Standard
Details

TRAFFIC

Title
PERMANENT IMPACT ATTENUATORS

QUADGUARD IMPACT ATTENUATORS WITH TENSION STRUT BACKUP

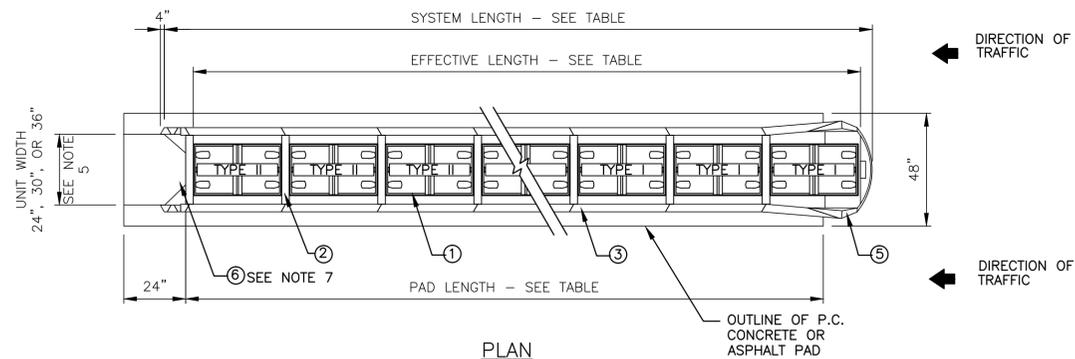
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Designed by _____ Drawn by _____ Checked by _____

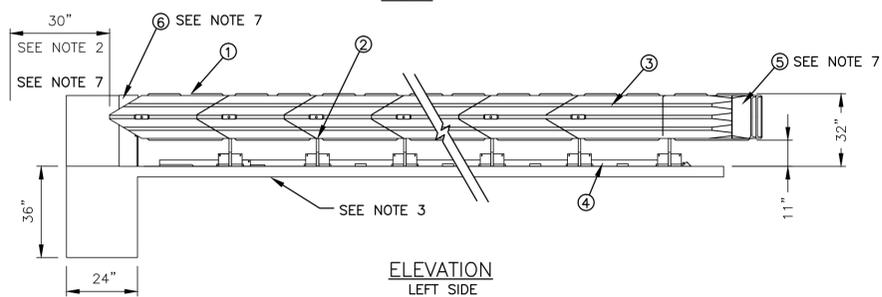
Date **11/01/2010**

Contract Number _____

Drawing Number **TD70.01**
PID# _____

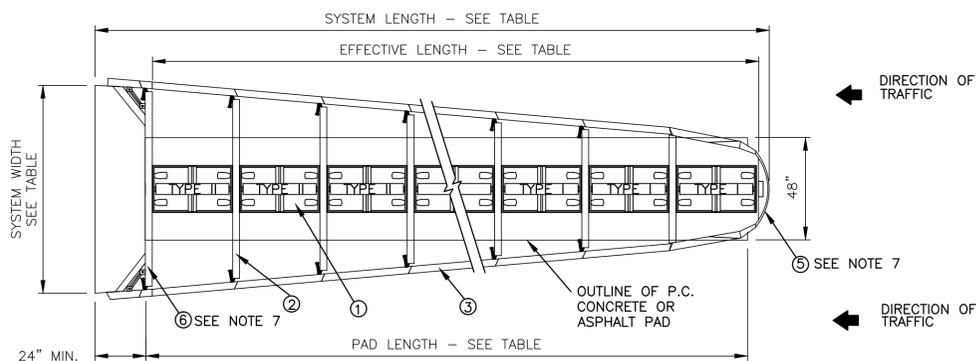


PLAN

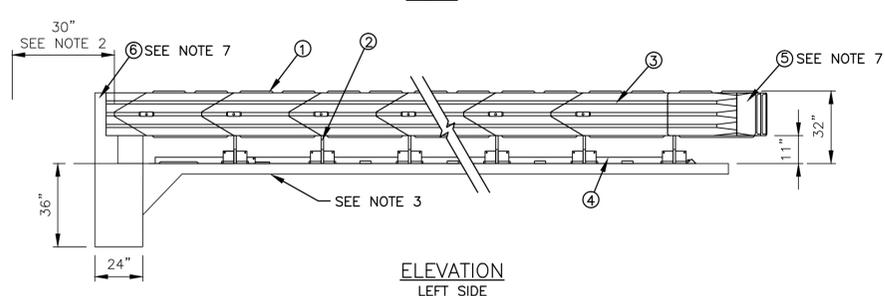


ELEVATION
LEFT SIDE

QUADGUARD SYSTEM FOR NARROW HAZARDS
N.T.S. TD70.02.02



PLAN



ELEVATION
LEFT SIDE

QUADGUARD SYSTEM FOR WIDE HAZARDS
N.T.S. TD70.02.03

KEY	①	QUADGUARD CARTRIDGE	③	FENDER PANEL	⑤	NOSE ASSEMBLY
	②	DIAPHRAGM	④	MONORAIL	⑥	BACKUP

TD70.02.04

NOTES:
TD70.02.01

- IN COMPLIANCE WITH THE AASHTO 1996 ROADSIDE DESIGN GUIDE, MANUFACTURER RECOMMENDS REMOVAL OF ALL CURBS AND ISLANDS TO ENSURE PROPER IMPACT PERFORMANCE.
- PROVISION SHALL BE MADE FOR REAR FENDER PANELS TO SLIDE REARWARD UPON IMPACT 30" MIN.
- 6" MIN. REINFORCED (4000 PSI) CONCRETE PAD OR 8" MIN. NON-REINFORCED (4000 PSI) PORTLAND CEMENT (P.C.) CONCRETE ROADWAY.
- SEE THE "QUADGUARD SYSTEM DESIGN MANUAL" DEVELOPED BY TRANSPRO INDUSTRIES, INC. FOR A DESCRIPTION OF ITS IMPACT PERFORMANCE CHARACTERISTICS AND DESIGN LIMITATIONS BEFORE PLACING A SYSTEM AT A GIVEN SITE.
- WHERE NECESSARY, PROVIDE A TRANSITION FROM THE QUADGUARD SYSTEM TO THE OBJECT BEING SHIELDED.
- UNITS OF MEASUREMENT ARE INCHES, UNLESS OTHERWISE NOTED.
- BACKUP AND TRANSITION ASSEMBLIES NOT INCLUDED IN MODEL NUMBER.
- THE BAY LENGTHS INDICATED IN THE TABLE ARE BASED UPON CALCULATED VALUES FOR 2000kg VEHICLES TRAVELING AT THE SPEEDS INDICATED AND HAVING ENOUGH CAPACITY TO DISSIPATE THE VEHICLES LONGITUDINAL IMPACT ENERGY.
- FOR NOSE COVER ASSEMBY SEE DRAWING TD70.07.

QUADGUARD SYSTEM
FOR NARROW HAZARDS
TD70.02.05

BAYS	2' WIDTH MODELS	2.5' WIDTH MODELS	3' WIDTH MODELS	SYSTEM LENGTH ft-in	EFFECTIVE LENGTH ft-in	PAD LENGTH ft-in	MAX DESIGN SPEED (MPH)	NO. OF CARTRIDGES	
								TYPE I (FRONT OF SYSTEM)	TYPE II (REAR OF SYSTEM)
3	QS2403*	QS3003*	QS3603*	14'-6"	11'-8"	11'-6"	44	3	1
6	QS2406*	QS3006*	QS3606*	23'-6"	20'-8"	20'-6"	62	4	3
7	QS2407*	QS3007*	QS3607*	26'-6"	23'-8"	23'-6"	65	4	4
9	QS2409*	QS3009*	QS3609*	32'-6"	29'-8"	29'-6"	71	4	6
11	QS2411*	QS3011*	QS3611*	38'-6"	35'-8"	35'-6"	75	5	7

*G=GREY OR Y=YELLOW

QUADGUARD SYSTEM
FOR WIDE HAZARDS
TD70.02.06

BAYS	5.7' WIDTH MODELS	7.5' WIDTH MODELS	SYSTEM LENGTH ft-in	EFFECTIVE LENGTH ft-in	PAD LENGTH ft-in	MAX DESIGN SPEED (MPH)	NO. OF CARTRIDGES	
							TYPE I (FRONT OF SYSTEM)	TYPE II (REAR OF SYSTEM)
3	QS6903*	QS9003*	14'-6"	11'-8"	11'-6"	44	3	1
6	QS6906*	QS9006*	23'-6"	20'-8"	20'-6"	62	4	3
7	QS6907*	QS9007*	26'-6"	23'-8"	23'-6"	65	4	4
9	QS6909*	QS9009*	32'-6"	29'-8"	29'-6"	71	4	6
11	QS6911*	QS9011*	38'-6"	35'-8"	35'-6"	75	5	7

*G=GREY OR Y=YELLOW

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

PANYNJ
Traffic Standard
Details

TRAFFIC

Title
PERMANENT IMPACT ATTENUATORS

QUADGUARD IMPACT
ATTENUATORS WITH
CONCRETE BACKUP

This drawing subject to conditions in contract. All inventions, ideas, designs and methods herein are reserved to Port Authority and may not be used without its written consent. All recipients of Contract documents, including bidders and those who do not bid and their prospective subcontractors and suppliers who may receive all or a part of the Contract documents or copies thereof, shall make every effort to ensure the secure and appropriate disposal of the Contract documents to prevent further disclosure of the information contained in the documents. Secure and appropriate disposal includes methods of document destruction such as shredding or arrangements with release handlers that ensure that third persons will not have access to the documents' contents either before, during, or after disposal. Documents may also be returned for disposal purposes to the Contract Desk on the 3rd Floor, 3 Gateway Center, Newark NJ 07102 or the office of the Director of Procurement, One Madison Avenue, 7th Floor, New York NY 10010. It is a violation of law for any person to alter a document in any way, unless acting under the direction of a licensed professional engineer or registered architect. If this document bearing the seal of an engineer/architect is altered, the altering engineer/architect shall affix to the document their seal and the notation "altered by" followed by their signature and the date of such alteration, and a specific description of the alteration.

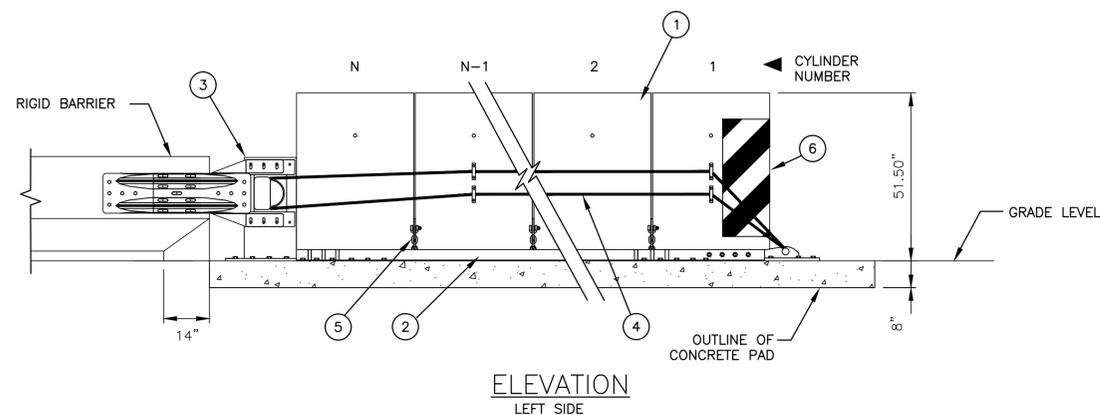
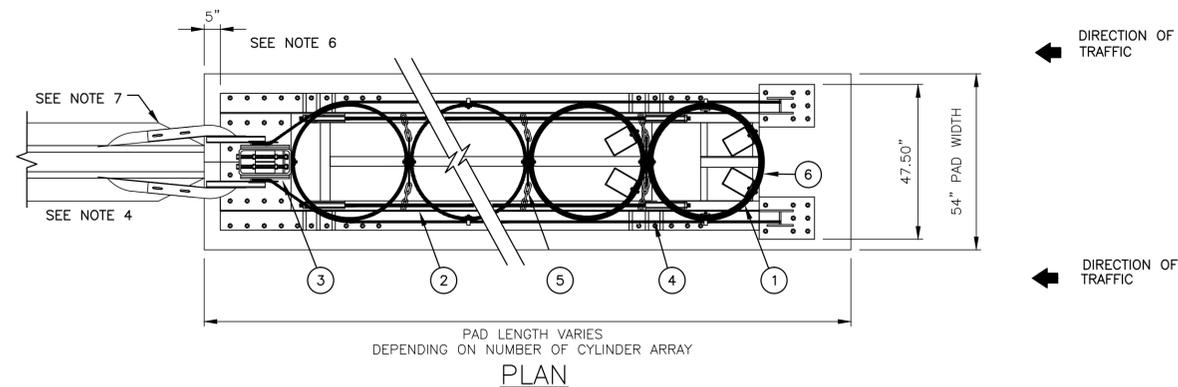
Designed by _____ Drawn by _____ Checked by _____

Date _____ 11/01/2010

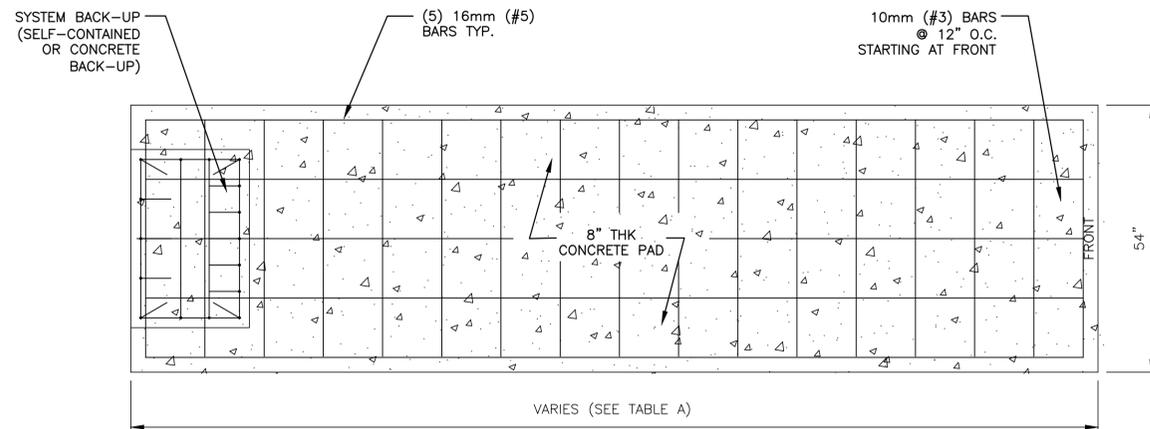
Contract Number _____

Drawing Number **TD70.02**

PID#



KEY	① CYLINDER	④ CABLE
	② BASE TRACK	⑤ STABILIZER CHAIN
	③ SYSTEM BACKUP	⑥ REFLECTIVE NOSE COVER



CONCRETE PAD REINFORCEMENT PLAN
N.T.S. TD70.03.02

TABLE A			
NO. CYLINDERS	SYSTEM		LENGTH
4 CYLINDER	REACT-43B036	REACT-43C036	16'-7"
6 CYLINDER	REACT-55B036	REACT-55C036	22'-7"
9 CYLINDER	REACT-62B036	REACT-62C036	31'-7"

TD70.03.03

NOTES:

TD70.03.01

- IN COMPLIANCE WITH THE AASHTO 1996 ROADSIDE DESIGN GUIDE, MANUFACTURER RECOMMENDS REMOVAL OF ALL CURBS AND ISLANDS TO ENSURE PROPER IMPACT PERFORMANCE.
- 8" MIN. REINFORCED 28 MPa (4000 PSI) P.C. CONCRETE PAD OR 8" MIN. NON-REINFORCED 28 MPa (4000 PSI) P.C. CONCRETE ROADWAY, MEASURING AT LEAST 12'-0" WIDE BY 50'-0" LONG.
- REACT 350 IS A PROPRIETARY SYSTEM VENDED BY TRANSPRO INDUSTRIES. FOR FULL DETAILS OF THE SYSTEM, CONTACT MANUFACTURER DIRECTLY. SERVICE DEPARTMENT AT (312) 467-6750.
- WHERE NECESSARY, A TRANSITION ASSEMBLY MUST BE PLACED FROM THE SYSTEM TO THE OBJECT BEING SHIELDED TO PREVENT SNAGGING OF VEHICLES. TRANSITION ASSEMBLIES ARE SUPPLIED AS ACCESSORIES AND ARE ORDERED SEPARATELY. END SHOES BY OTHERS.
- UNITS OF MEASUREMENT ARE MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.
- PROVIDE 5" GAP. SYSTEM AND SAFETY SHAPED BARRIER TO MATCH CENTERLINES.
- THE SIDE OF THE BARRIER THAT FACES ON-COMING TRAFFIC MUST BE TRIMMED TO REDUCE WHEEL SNAGGING POTENTIAL.
- FOR NOSE COVER ASSEMBLY SEE DRAWING TD70.07.
- ALL CONCRETE WORK AND RE-BAR DETAILS SHALL CONFORM TO THE LATEST ACI CODE AND MANUAL.
- ALL CONCRETE TO BE 8" MINIMUM THICK 28 MPa (4000 PSI) COMPRESSIVE STRENGTH AT 28 DAY TEST.
- ALL REINFORCING BARS SHALL BE A615 GRADE 60 NEW BILLET STEEL.
- PROVIDE MIN. 2" CLEAR CONCRETE COVER OVER REINFORCING STEEL.
- THE SLAB DETAILED ON THIS SHEET REQUIRES IT TO BE PLACED AGAINST AND SUPPORTED BY A RIGID BARRIER OR OTHER STRUCTURE. THE SUPPORT STRUCTURE OR BARRIER WILL RESIST PAD AND SYSTEM SLIDE DURING IMPACTS. USE THE BELOW GRADE ANCHOR FOR AND INDEPENDENT, SOIL SUPPORTED PAD. THE SYSTEM COULD TRANSFER IMPACT LOADING TO ADJACENT STRUCTURES. PROVIDE ADEQUATE ANCHORAGE.
- CROSS SLOPE OF PAD SHALL NOT EXCEED 8% AND NOT VARY MORE THAN 2% FROM FRONT TO BACK.

No.	Date	Revision	Approved
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ENGINEERING DEPARTMENT

PANYNJ
Traffic Standard
Details

TRAFFIC

Title
PERMANENT IMPACT ATTENUATORS

REACT 350

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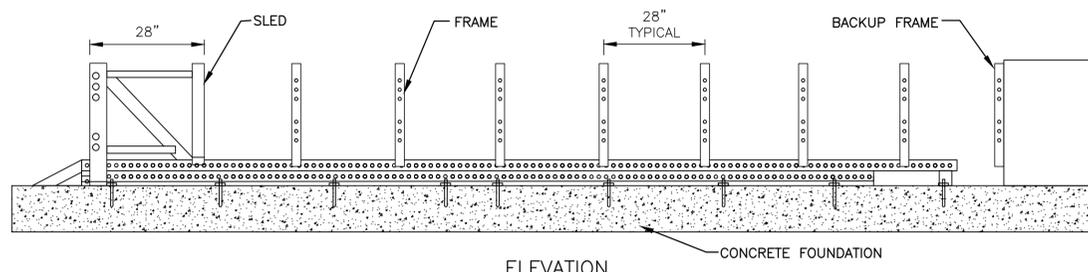
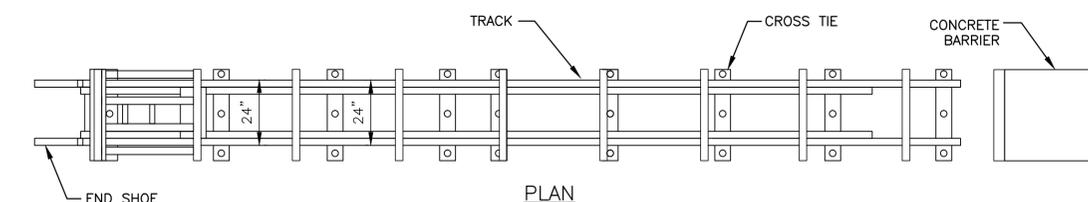
Designed by _____ Drawn by _____ Checked by _____

Date _____ 11/01/2010

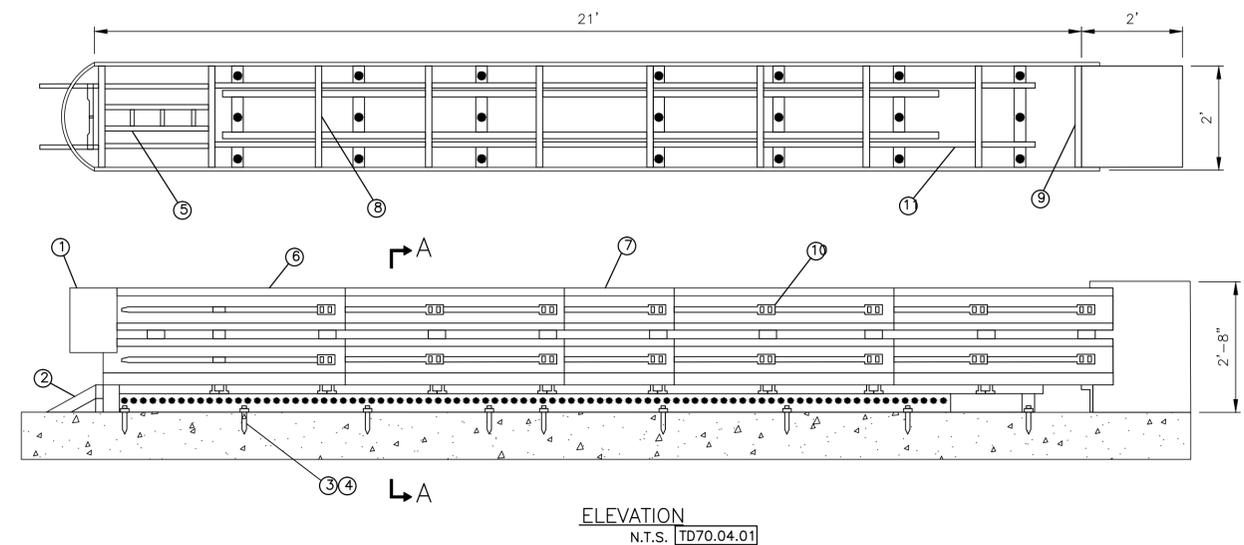
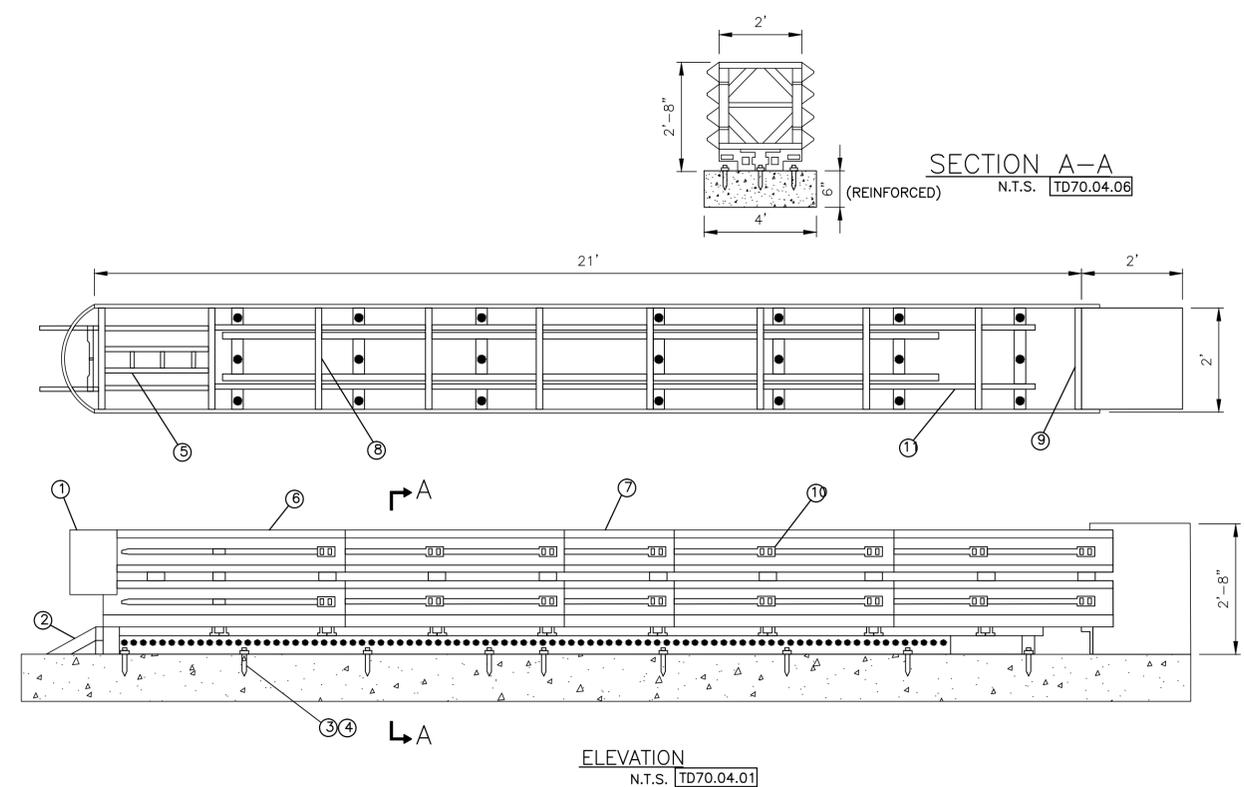
Contract Number _____

Drawing Number **TD70.03**

PID#



THE STRUCTURAL COMPONENTS
N.T.S. TD70.04.02

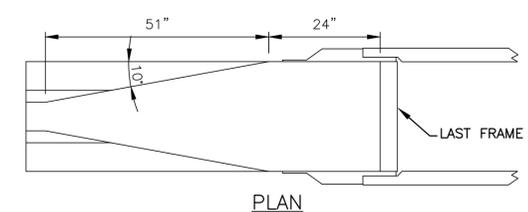


ELEVATION
N.T.S. TD70.04.01

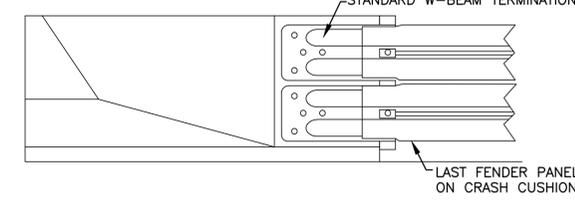
TRACC ANCHORING OPTIONS

A	B	C	D	E
ASPHALT	CONCRETE	SUBBASE (COMPACTED)	DEPTH	LENGTH
0"	0" REINFORCED	~	6"	7.5"
0"	0" UNREINFORCED	~	6"	7.5"
3"	3"	~	16.5"	18"
8"	0"	~	16.5"	18"
6"	0"	6"	16.5"	18"

TD70.04.03

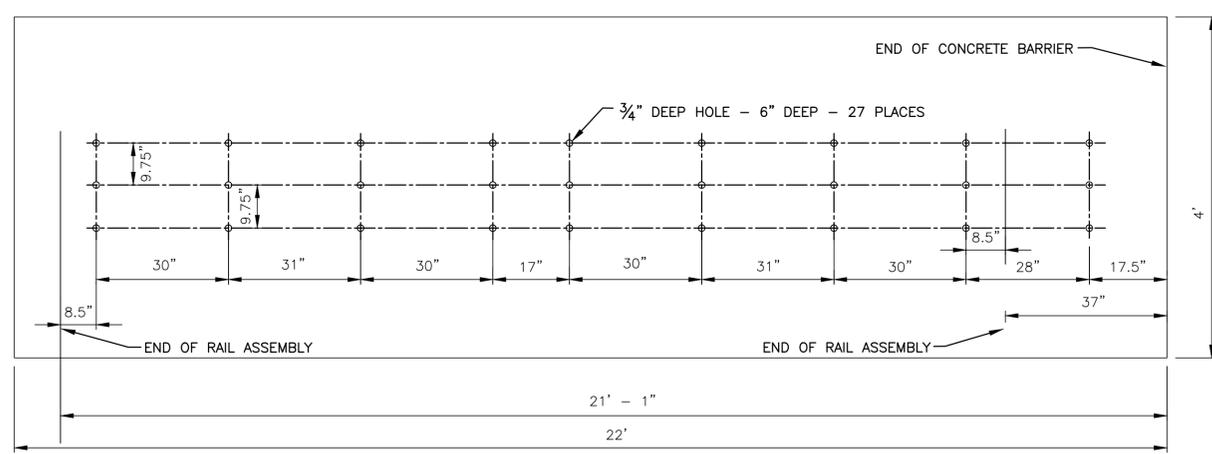


PLAN

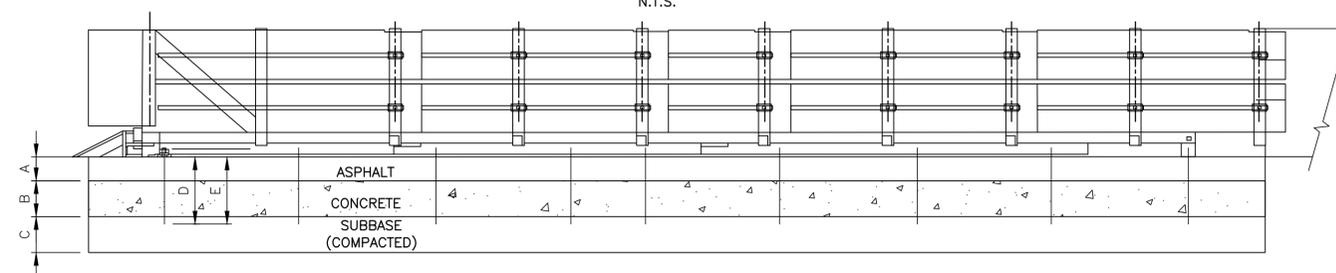


ELEVATION

**CONCRETE TRANSITION
FOR BI-DIRECTIONAL TRAFFIC APPLICATION
OF TRINITY EXODYNE CRASH CUSHION**
N.T.S. TD70.04.04



PLAN
N.T.S.



ELEVATION
N.T.S.

FOUNDATION
TD70.04.05

**TRINITY INDUSTRIES, INC.
NARROW CRASH CUSHION**

ITEM	DESCRIPTION
1	PLASTIC SHEET WITH REFLECTIVE DELINEATOR
2	SNAG ELIMINATING RAMP
3	5/8" X 7.5" ANCHOR STUD
4	CHEMICAL GROUT
5	SLED
6	2-BAY FENDER PANEL
7	1-BAY FENDER PANEL
8	INTERMEDIATE FRAME
9	BACKUP FRAME
10	SLOT PLATE
11	GUIDE ANGLES

TD70.04.01

LENGTH.....	21'
WIDTH (EFFECTIVE).....	2'
(OUT-TO-OUT).....	2'7"
WEIGHT.....	260 LBS

TD70.04.05

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

**PANYNJ
Traffic Standard
Details**

TRAFFIC

Title
PERMANENT IMPACT ATTENUATORS

TRACC

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Designed by _____ Drawn by _____ Checked by _____

Date _____ 11/01/2010

Contract Number _____

Drawing Number **TD70.04**
PID# _____

CHIEF

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

PANYNJ
Traffic Standard
Details

TRAFFIC

Title
PERMANENT IMPACT ATTENUATORS

CAT-350

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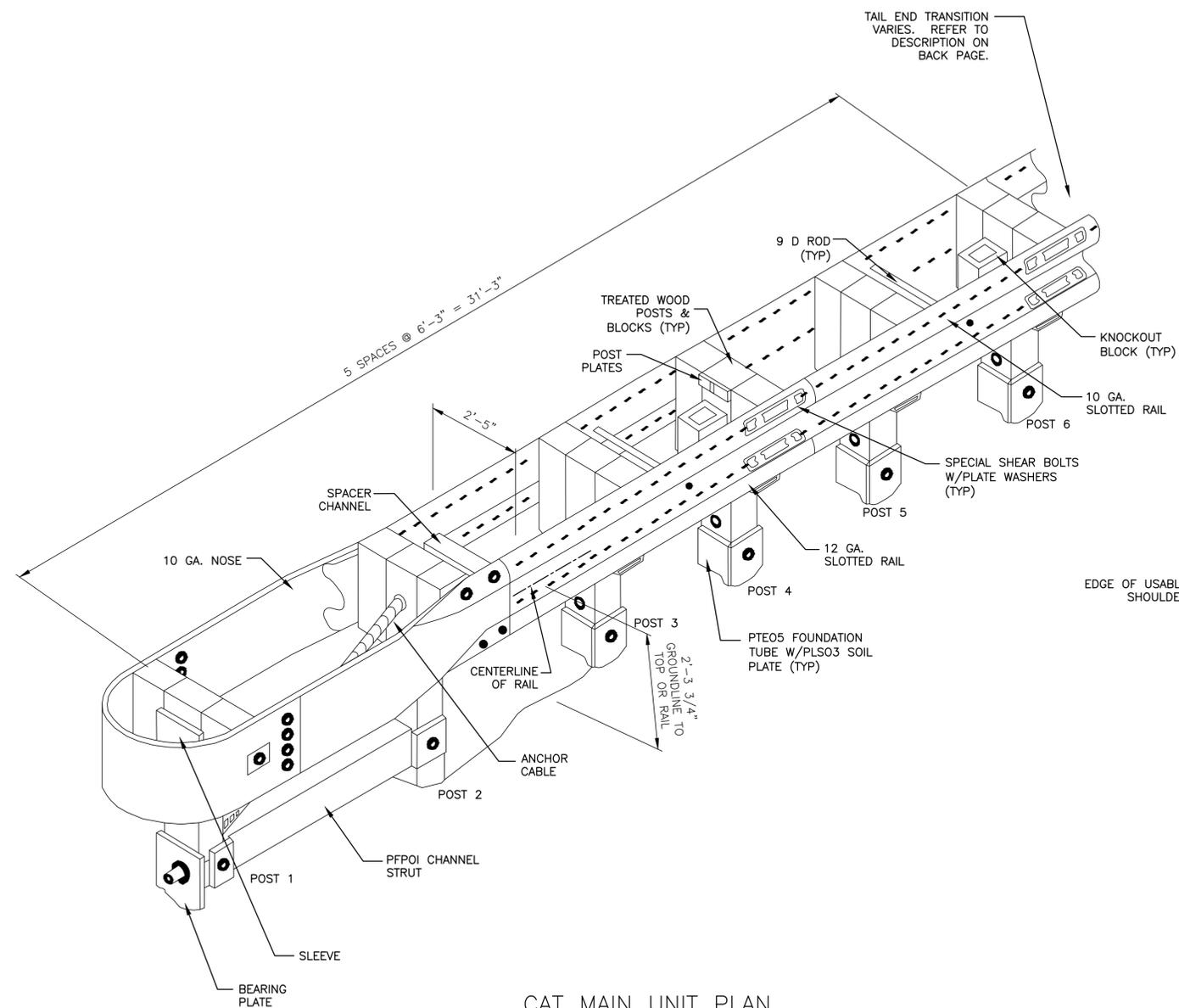
Designed by _____ Drawn by _____ Checked by _____

Date _____ 11/01/2010

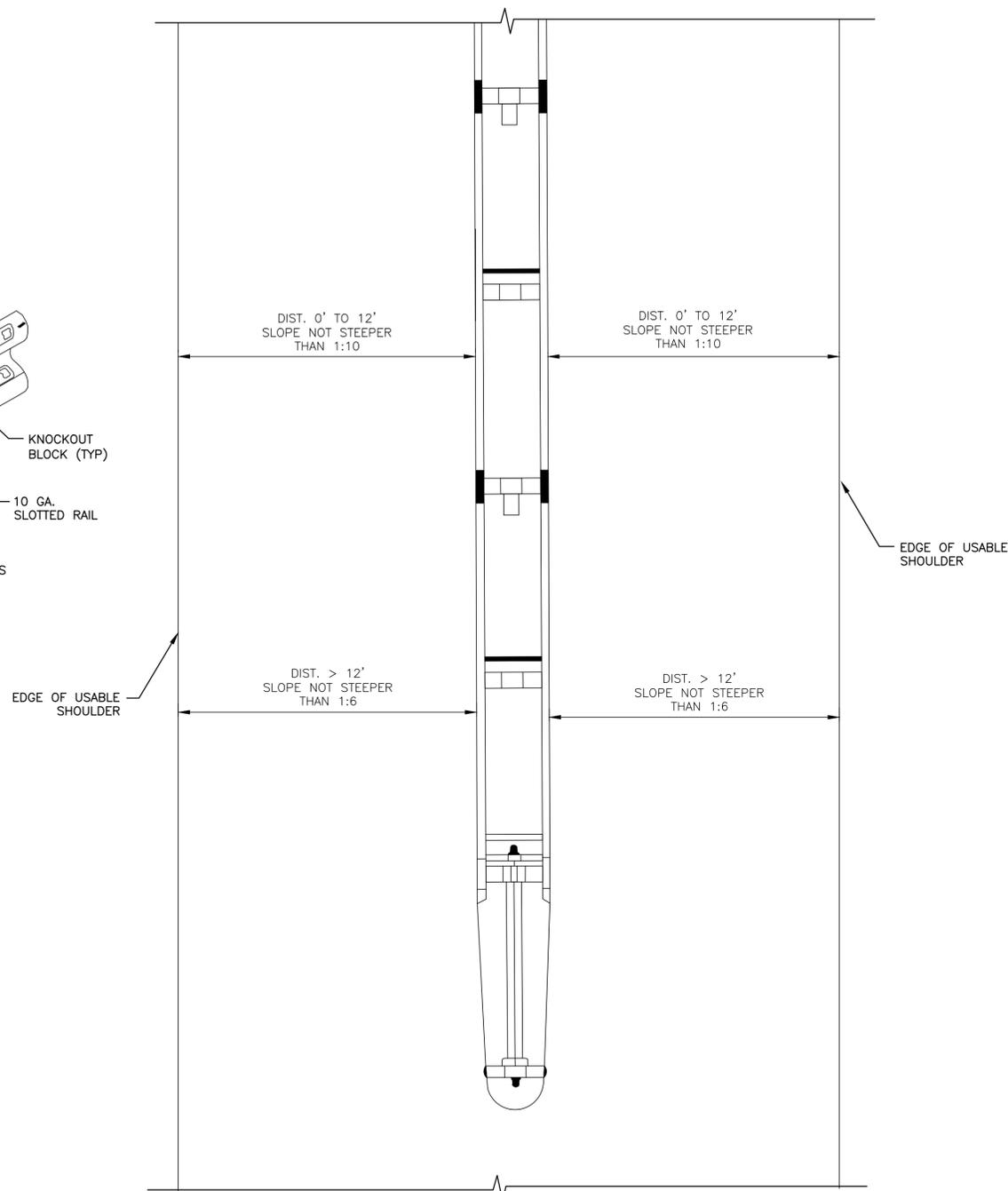
Contract Number _____

Drawing Number **TD70.05**

PID#



CAT MAIN UNIT PLAN
N.T.S. **TD70.05.01**



MEDIAN BARRIER AND TREATMENT PLAN
N.T.S. **TD70.05.02**

CHIEF

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

**PANYNJ
Traffic Standard
Details**

TRAFFIC

Title
PERMANENT IMPACT ATTENUATORS

**INERTIAL SAND
FILLED
BARREL ARRAYS**

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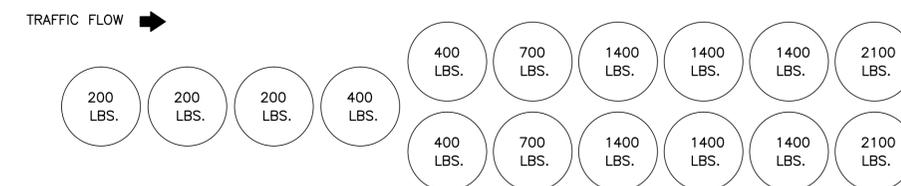
Designed by _____ Drawn by _____ Checked by _____

Date **11/01/2010**

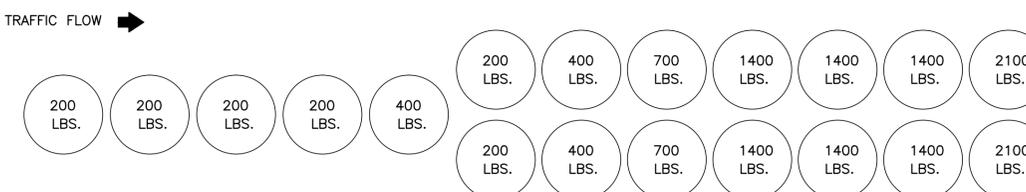
Contract Number _____

Drawing Number **TD70.06**

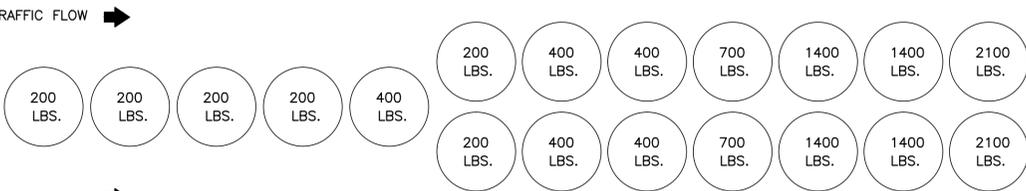
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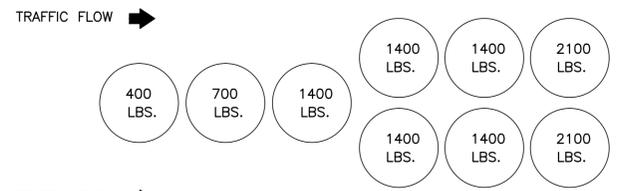
60 MPH DESIGN – 4500 lbs. VEHICLE
TD70.06.03



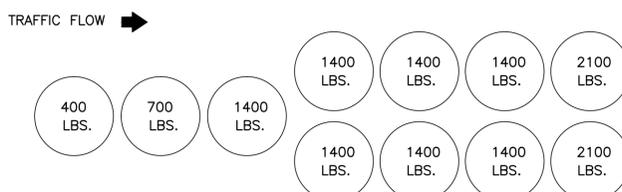
65 MPH DESIGN – 4500 lbs. VEHICLE
TD70.06.06



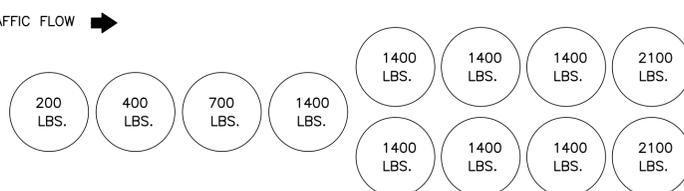
70 MPH DESIGN – 4500 lbs. VEHICLE
TD70.06.10



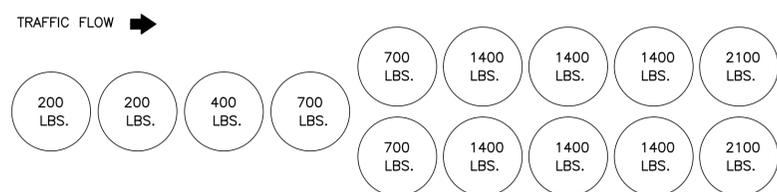
40 MPH DESIGN – 4500 lbs. VEHICLE
TD70.06.02



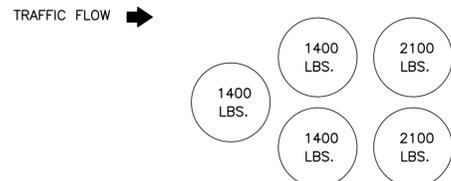
45 MPH DESIGN – 4500 lbs. VEHICLE
TD70.06.05



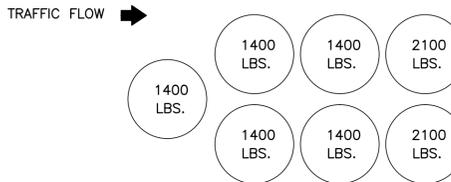
50 MPH DESIGN – 4500 lbs. VEHICLE
TD70.06.07



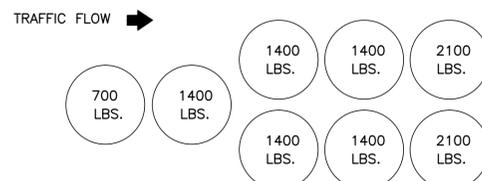
55 MPH DESIGN – 4500 lbs. VEHICLE
TD70.06.09



25 MPH DESIGN – 4500 lbs. VEHICLE
TD70.06.01



30 MPH DESIGN – 4500 lbs. VEHICLE
TD70.06.04

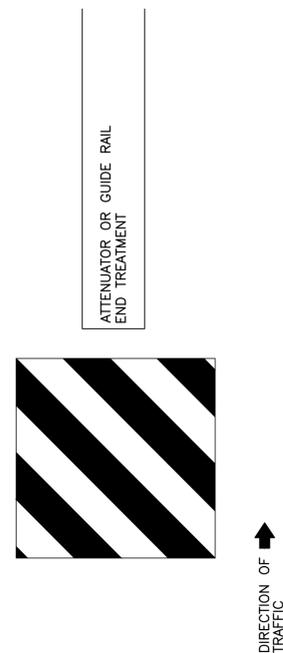


35 MPH DESIGN – 4500 lbs. VEHICLE
TD70.06.08

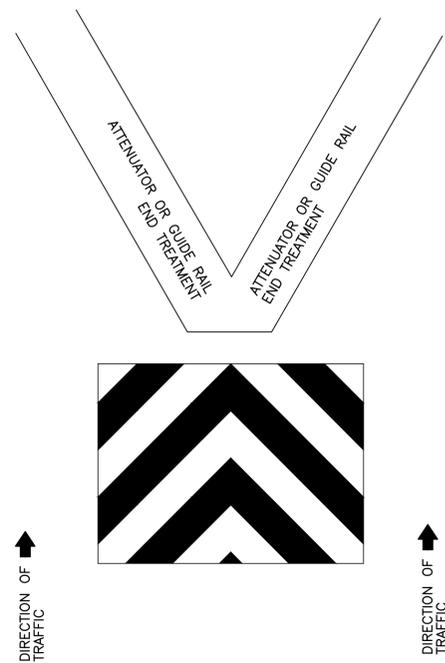
CHIEF

NOTES:
[TD70.06.01]

1. STRIPES SHALL SLOPE DOWNWARD AT AN ANGLE OF 45 DEGREES TOWARD THE SIDE ON WHICH TRAFFIC IS TO PASS. WHERE TRAFFIC MAY TURN, OR PASS, EITHER LEFT OR RIGHT, THE STRIPES ON THE LEFT HALF OF THE BARRICADE SHOULD SLOPE DOWNWARD TO THE LEFT, AND THOSE ON THE RIGHT HALF SHOULD SLOPE DOWNWARD TO THE RIGHT. WHERE THE TRAFFIC MUST TURN AROUND, THE STRIPES ON THE LEFT HALF SHOULD SLOPE DOWNWARD TO THE RIGHT, AND THOSE ON THE RIGHT SHOULD SLOPE DOWNWARD TO THE LEFT. STRIPES SHALL BE FOUR TO SIX INCHES WIDE.
2. MANUFACTURER VARIATION ALLOWABLE UPON APPROVAL BY THE ENGINEER.



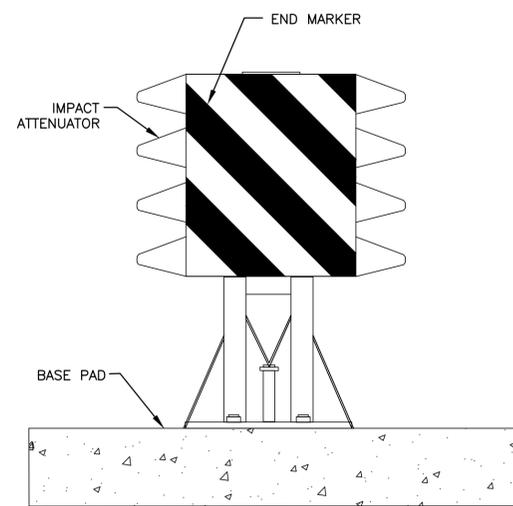
PLAN
[TD70.06.02]



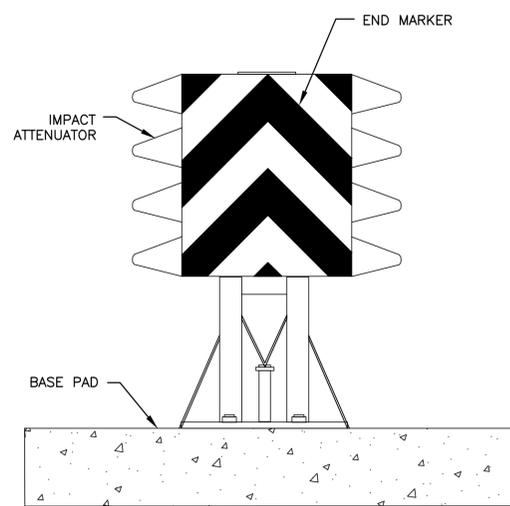
PLAN
[TD70.06.03]



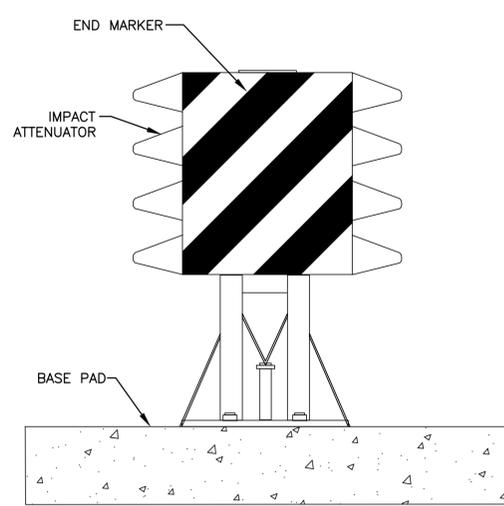
PLAN
[TD70.06.04]



ELEVATION
[TD70.06.02]



ELEVATION
[TD70.06.03]



ELEVATION
[TD70.06.04]

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

PANYNJ
Traffic Standard
Details

TRAFFIC

Title
PERMANENT IMPACT ATTENUATORS
NOSE COVER MARKER FOR ATTENUATOR WND TREATMENT

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Designed by _____ Drawn by _____ Checked by _____

Date **11/01/2010**

Contract Number _____

Drawing Number **TD70.07**
PID# _____