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Appendix E.2  
Historic Architectural Resources Survey Report  
New York\*

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\*The Historic Architectural Resources Study – New Jersey, submitted July 2008, replaces the previous submission regarding New Jersey Historic Architectural properties contained in this August 2007 report (Refer to Appendix E-1 for Historic Architecture in the New Jersey APE)

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GOETHALS BRIDGE REPLACEMENT

RICHMOND COUNTY, NEW YORK

AND

THE CITY OF ELIZABETH, UNION COUNTY,  
NEW JERSEY

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HISTORIC RESOURCES SURVEY REPORT

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Prepared For:

**The United States Coast Guard**



Project Applicant:

**The Port Authority of New York and New Jersey**



Prepared By:

**The Louis Berger Group, Inc./Parsons Brinckerhoff JV**



**August 2007**

**GOETHALS BRIDGE REPLACEMENT**  
**04PR03162**  
**RICHMOND COUNTY, NEW YORK**  
**AND**  
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The Louis Berger Group, Inc./Parsons Brinckerhoff JV

Stuart P. Dixon  
Kristie Baynard  
Kristofer M. Beadenkopf, RPA  
Zachary J. Davis, RPA

**MANAGEMENT SUMMARY**

**SHPO Project Review Number:** NYSOPRHP # 04PR03162

**Involved State and Federal Agencies:** Port Authority of New York and New Jersey (PANYNJ);  
United States Coast Guard (USCG);  
U.S. Army Corps of Engineers (USACE)

**Phase of Survey:** Historic Resources [Eligibility] Survey

**Location Information:**

**Location:** Goethals Bridge Corridor spanning the Arthur Kill beginning near the convergence of the east and west lanes of I-278 at Route 440 in Staten Island, New York, and extending to Mattano Park in Elizabeth, New Jersey

**Minor Civil Division:** Staten Island; Elizabeth

**County:** Richmond County, New York; Union County, New Jersey

**Survey Area (Metric; English):**

**Length:** N/A

**Width:** N/A

**Depth:** N/A

**Number of Acres Surveyed:** 1,239.72 acres (NY); 424.19 acres (NJ)

**USGS 7.5 Minute Quadrangle Map(s):** *Elizabeth, NJ-NY; Arthur Kill, NY-NJ*

**Archaeological Survey Overview:** N/A

**Results of Archaeological Survey:** N/A

**Results of Architectural Survey:**

**Number of buildings/structures/cemeteries within project area:** 93

**Number of buildings/structures/cemeteries adjacent to project area:** 0

**Number of previously determined NR listed or eligible buildings/structures/cemeteries/districts:** 4

**Number of identified eligible buildings/structures/cemeteries/districts:** 0

**Report Author(s):** Stuart Dixon; Kristie Baynard; Kristofer M. Beadenkopf, RPA; Zachary J. Davis, RPA

**Date of Report:** August 2007

## EXECUTIVE SUMMARY

As part of the Goethals Bridge Replacement Environmental Impact Statement (GBR EIS) for the United States Coast Guard (USCG), The Louis Berger Group, Inc./Parsons Brinckerhoff Joint Venture (Berger/PB JV) undertook a historic resources survey and prepared documentation pursuant to the National Environmental Policy Act of 1969, as amended. The survey was conducted by Berger/PB JV in accordance with Section 106 of the National Historic Preservation Act of 1966, as amended. Field survey and historic research for the architectural resources investigations were completed in July and August 2006. The purpose of the survey was to: (1) identify historic properties previously listed or determined eligible for listing in the National Register of Historic Places within the project's area of potential effect (APE), (2) identify and document other, previously unidentified resources within the APE meeting the National Register's 50-year age or exceptional significance criteria, and (3) evaluate the National Register eligibility and provide eligibility recommendations for the previously unidentified historic resources. The APE as determined for these investigations in New York consists of a one-half mile corridor surrounding the proposed improvements to the Goethals Bridge, and in New Jersey is bounded by the Arthur Kill on the east, the Elizabeth River and Mattano Park on the north, Clifton and Pulaski Streets on the west, Interchange 13 and associated ramps on the southwest, and Morses Creek on the south.

The Goethals Bridge, originally built in 1928, provides direct connection between Elizabeth, New Jersey, and Staten Island, New York. Part of the Port Authority's Interstate Transportation Network, the Goethals Bridge serves as one of the principal vehicular transportation arteries linking northern New Jersey and New York City. As a result of the architectural resources investigations, Berger/PB JV identified 93 resources meeting the National Register's 50-year age criteria within the project's APE for architectural investigations.

Of the 93 resources within the APE, 28 had been previously identified, documented, and evaluated for National Register eligibility. Six of these 28 resources were determined eligible for listing in the National Register via consensus determinations between the New Jersey Historic Preservation Office (NJHPO) and federal and state agencies. These six resources, all located in the New Jersey portion of the APE, included the Goethals Bridge, the Staten Island Railroad Historic District, the Perth Amboy and Elizabethport Branch of the Central Railroad of New Jersey (CNJ), the CNJ Bridge over the Elizabeth River, the South Front Street Bridge over the Elizabeth River, and the South First Street Bridge over the Elizabeth River. The remaining 22 previously identified resources were determined not eligible via consensus determination.

The remaining 65 historic architectural resources identified by Berger/PB JV consisted of 50 resources standing within the New Jersey portion of the APE and 15 resources standing within the New York portion of the APE. Berger/PB JV field survey documented 60 of the historic resources (fieldworkers were denied permission to document properties by five owners/residents) through narrative notes and photographs, and completed appropriate state historic preservation office architectural survey forms for each surveyed property. Berger/PB JV then evaluated the significance and integrity of each property utilizing National Register eligibility criteria (36 CFR 60). As a result of these activities, Berger/PB JV recommends none of the 65 newly identified historic resources within the APE as meeting National Register eligibility criteria because they lack historical and/or architectural significance and have lost integrity.

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## 1.0 INTRODUCTION

The Port Authority of New York and New Jersey (PANYNJ) has proposed to erect a new span over the Arthur Kill to replace the existing Goethals Bridge linking Elizabeth, New Jersey, and Staten Island, New York. The Goethals Bridge is part of the Port Authority's Interstate Transportation Network and serves as a major link between northern New Jersey and New York City for vehicular traffic, along with the George Washington Bridge, the Holland and Lincoln Tunnels, the Outerbridge Crossing Bridge, and the Bayonne Bridge. The Goethals Bridge, built in 1928, is also considered a primary transportation route within the metropolitan area's Southern Corridor, connecting the New Jersey Turnpike (Interstate 95) and U.S. Routes 1 and 9 in New Jersey with Brooklyn and ultimately Long Island, New York, via the Verrazano Narrows Bridge and the Staten Island Expressway (Interstate 278) roughly paralleling Staten Island's north shore.

This report summarizes the results of the historic resources survey undertaken by The Louis Berger Group, Inc./ Parsons Brinckerhoff Joint Venture (Berger/PB JV), on behalf of the United States Coast Guard (USCG) as part of the Goethals Bridge Replacement Environmental Impact Statement (GBR EIS).

### 1.1 Purpose of Study

The historic resources survey conducted by Berger/PB JV as part of this study served to: (1) identify historic properties previously listed or determined eligible for listing in the National Register of Historic Places within the project's area of potential effect (APE), (2) identify and document other, previously unidentified resources within the APE meeting the National Register's 50-year age or exceptional significance criteria, and (3) evaluate the National Register eligibility and provide eligibility recommendations for the previously unidentified historic resources.

These investigations were conducted in accordance with Section 106 of the National Historic Preservation Act of 1966, as amended, the Advisory Council on Historic Preservation's guidelines implementing Section 106 (*Protection of Historic Properties*, 36 CFR 800), and the National Environmental Policy Act of 1969. These activities were also conducted following the New Jersey Historic Preservation Office's (NJHPO) guidelines for architectural surveys and the New York State Office of Parks, Recreation and Historic Preservation's (NYSOPRHP) directives for architectural surveys. The Berger/PB JV staff performing the fieldwork, documentation, and National Register eligibility evaluation activities described herein meet or exceed the Secretary of the Interior's Professional Qualifications Standards (36 CFR 61) for History and/or Architectural History.

### 1.2 Project Description and Setting

The PANYNJ's GBR EIS involves construction of a new bridge span over the Arthur Kill linking Union County, New Jersey, with Staten Island Borough, Richmond County, New York. The Primary Study Area for the project encompasses approximately 1 square mile of industrial waterfront in New Jersey, principally in the City of Elizabeth with a smaller portion in the City of Linden, and nearly 2 square miles of less developed acreage in the northwestern portion of New York City's Staten Island Borough (Figure 1). Development on the New Jersey side of the Goethals Bridge Study Area is relatively dense and almost completely built up. The New Jersey Turnpike and a parallel railroad line divide the area into a predominantly industrial area on the east and a western section consisting mostly of residential and commercial uses and parkland. The existing Goethals Bridge approach (Interstate 278) further divides the area into a northern section and a southern section. The Staten Island portion of the study area is mostly undeveloped, although scattered transportation, utility, residential, and commercial uses are also present.

### 1.3 Project Personnel

Principal Architectural Historian Martha H. Bowers and Assistant Director Susan Grzybowski served as Project Managers for the architectural resource investigations. Senior Architectural Historian Stuart Dixon conducted the architectural field survey and National Register evaluations and prepared the present report. Archaeologist Kristofer M. Beadenkopf, RPA prepared the historic background. Mr. Dixon and Architectural Historian Kristie Baynard prepared survey forms for the previously unidentified historic resources. Zachary J. Davis, RPA prepared report graphics. Anne Moiseev provided editorial review of the report.

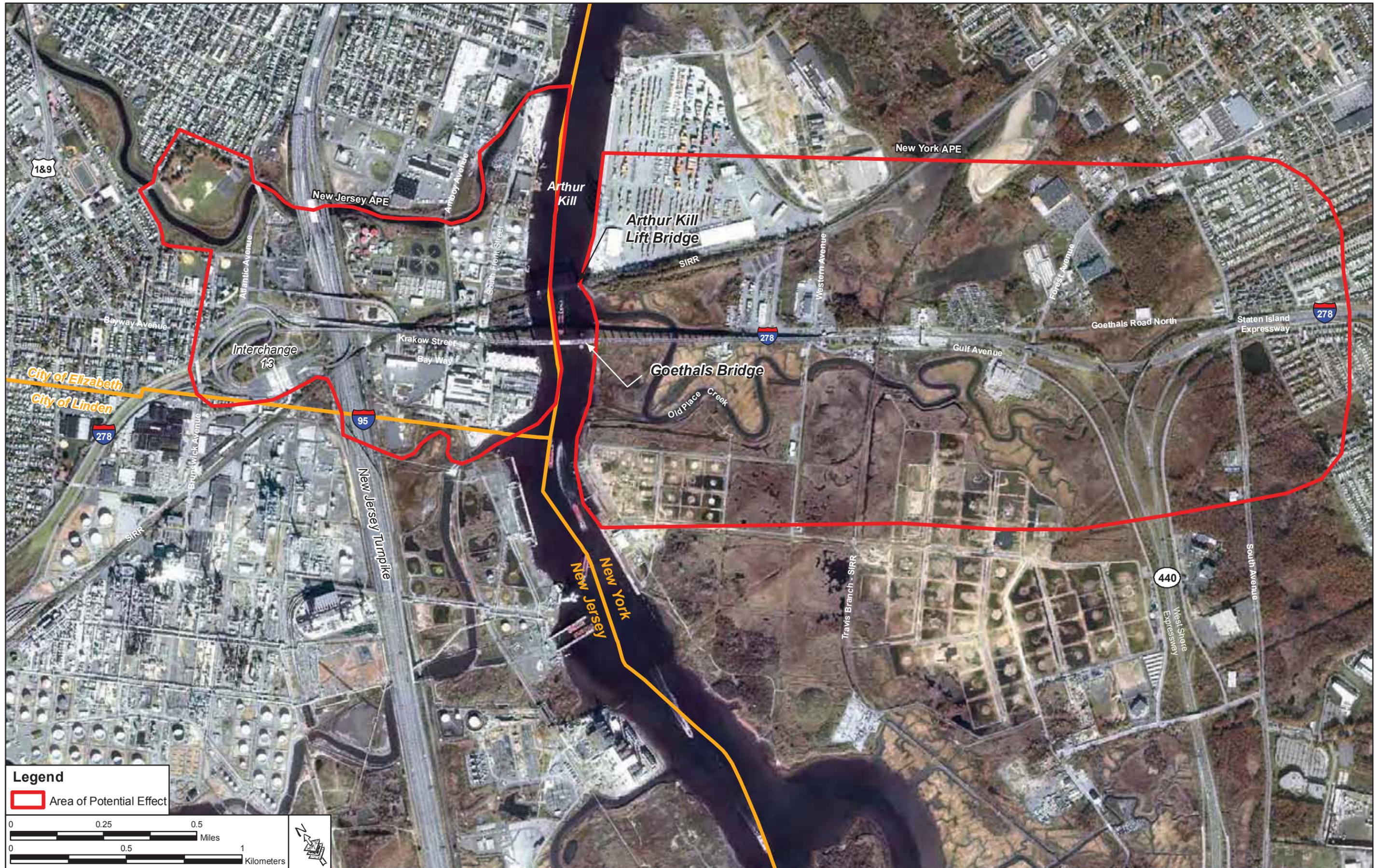


FIGURE 1: New Jersey and New York Areas of Potential Effect

Source: Port Authority of New York and New Jersey, 2002.

## 2.0 RESEARCH DESIGN

### 2.1 Introduction

Section 106 of the National Historic Preservation Act (NHPA) requires that federal agencies or applicants for federal funding, permits, and authorizations take into account the effects of their undertaking on historic properties. Historic properties include buildings, districts, objects, sites, and structures, as well as prehistoric and historic archaeological sites and objects, listed in or eligible for listing in the National Register of Historic Places. The steps required to be undertaken by federal agencies in order to comply with Section 106 are outlined in the Advisory Council on Historic Preservation's regulations implementing Section 106, *Protection of Historic Properties* (36 CFR 800). The implementing regulations require federal agencies to consult with the respective state historic preservation office (SHPO) to identify historic properties listed in or eligible for listing in the National Register potentially affected by the undertaking, to assess the undertaking's effects on the listed or eligible National Register historic properties, and to avoid, minimize, or mitigate any adverse effects on historic properties. Section 106 consultation with the New Jersey and New York SHPOs regarding the Goethals Bridge Replacement Project began in June 2005.

In the State of New Jersey, the responsible state agency is the New Jersey Historic Preservation Office (NJHPO), a part of the New Jersey Department of Environmental Protection. Consultation and review of historic architectural issues in New Jersey is conducted under authority of the New Jersey Register of Historic Places Act of 1970 (N.J.S.A. 13:1B-15.1328 et. seq.).

In the State of New York, the New York State Office of Parks, Recreation and Historic Preservation (NYSOPRHP) serves as the SHPO. Consultation and review of historic architectural issues in New York is conducted under authority of the New York State Historic Preservation Act of 1980 (Chapter 354 of Parks, Recreation and Historic Preservation Law).

Under New York City's Landmark Preservation Law (Landmarks Law), the New York City Landmarks Preservation Commission (NYCLPC) has the authority to designate City Landmarks, Interior Landmarks, Scenic Landmarks, and Historic Districts, and to regulate any construction, reconstruction, alteration, or demolition of such Landmarks and Districts. The historic architectural survey conducted for this project were performed according to the procedures for conducting architectural surveys within New York City outlined in the *City Environmental Quality Review Technical Manual* 2001.

### 2.2 Methodology, Approach, and Data Sources

The historic resources study began with the identification of an area of potential effect (APE) for historic architectural resources in consultation with the New Jersey and New York SHPOs. The APE is the geographic area in which the undertaking may directly or indirectly cause changes in the character or use of any existing historic properties. The Staten Island Bridges Program Final Environmental Impact Statement (FEIS) in 1997 utilized an APE for historic architecture consisting of a one-half mile corridor surrounding the proposed improvements to the Goethals Bridge (USCG 1997). The APE for the current project also proposed a one-half mile corridor corresponding with the combined Primary and Secondary Study Areas (i.e., the Goethals Bridge Study Area) identified during the Draft Environmental Impact Statement (DEIS) scoping process. These APEs were submitted to the NJHPO and the NYSOPRHP in June 2005 for review and comments as part of the Section 106 consultation process for the Goethals Bridge Replacement EIS.

The NJHPO review of the APE determined that, owing to broader viewshed concerns, the use of a larger APE for historic architecture in New Jersey would be recommended. In October 2005 a field review of the Goethals Bridge Study Area and its environs was conducted to develop an appropriate APE that addressed the potential viewshed concerns. Following further consultation, a revised APE was submitted to the NJHPO on March 10, 2006. The revised APE considered the nature and scale of the proposed project, the existing built environment in which the project will occur, and the various ways the project could reasonably be demonstrated to affect historic properties. On July 20<sup>th</sup>, 2007, the Deputy State Historic Preservation Officer of NJHPO, Dorothy P. Guzzo, concurred with that revised APE.

The revised APE is bounded by the Arthur Kill on the east, the Elizabeth River and Mattano Park on the north, Clifton and Pulaski Streets on the west, Interchange 13 and associated ramps on the southwest, and Morses Creek on the south. All the above-stated correspondences under the Section 106 Consultation with NYSOPRHP, NYCLPC, and NJHPO are actually compiled in Appendix BA. The APEs for both New Jersey and New York are depicted on Figure 1.

The historic resources survey consisted of research, field survey and documentation, and the evaluation of resources identified within the two APEs for National Register eligibility. Much background research was gathered as part of the 1997 Staten Island Bridges Program. This research included an examination and analysis of selected historical maps and secondary histories available at the New York City Public Library in Manhattan, the Staten Island Institute of Arts and Sciences (SIIAS), the St. George Library Center in Staten Island, and the New Jersey State Library in Trenton, New Jersey. Additional historical maps were collected from online-digital historic cartographic clearinghouses such as the American Memory Map Collection of the Library of Congress and the Davis Rumsey Historical Map Collection. Historic architecture site survey files were also reviewed at the NJHPO and the NYSOPRHP. This review revealed that several historic resources within the New Jersey APE had previously been determined eligible for listing in the National Register.

A previously-conducted historic architectural survey, as part of the Staten Island Bridges Program, resulted in the documentation of 24 historic resources that were subsequently submitted to the NJHPO for review and concurrence (USCG 1997). The NJHPO provided concurrence on the submitted documentation via letter dated July 11, 1995 (NJHPO 1995). Based on this earlier documentation, the NJHPO did not require that the original 24 historic resources be resurveyed as part of the current project. The NJHPO did request that any substantial changes to the resources identified in the field should be outlined in the historic resources survey report.

An updated historic resources field survey was conducted in July 2006 and identified 65 additional historic resources within the two APEs. The survey consisted of a pedestrian examination of properties within the respective APEs. Narrative field notes were obtained describing the form, fabric, condition, and integrity of each historic resource identified. Black and white photographs were also taken of each resource. A total of 60 properties were field documented, 47 in the New Jersey APE and 13 in the New York APE. Fieldworkers were denied permission to photograph or otherwise document five of the identified historic resources. The field survey also examined the previously documented historic resources to determine whether any had undergone substantial changes.

Architectural survey forms were prepared for each of the 60 historic resources documented within the APEs (see Appendices BB and BC). The survey forms contain brief narrative descriptions, statements of significance, and evaluations of their National Register eligibility using the National Register eligibility criteria (36 CFR 60.4). The results of the survey and subsequent National Register evaluations are summarized in Chapter 4.0.

## 3.0 HISTORICAL OVERVIEW

### 3.1 New Jersey Section of the APE: Historical Overview

The western terminus of the Goethals Bridge in New Jersey is in Elizabeth, about 1,500 feet south of the Elizabeth River. Elizabeth, formerly called Elizabethtown, was the site of the first permanent English settlement in the state (Figure 2). An association of eastern Long Islanders bought a large tract of land between the Passaic and Raritan rivers from Native Americans living on Staten Island. This tract encompassed Union County and parts of Morris, Somerset, Essex, and Middlesex counties (Heritage Studies 1985:55). The area bordering the Arthur Kill was known as Elizabeth Point, or Governor's Point, as it was the site of Phillip Carteret's landing in 1665. The area at the mouth of the Elizabeth River remained sparsely settled and was noted mainly for the ferry to Staten Island, established in 1697 at the foot of Elizabeth Avenue.

Settlers from eastern Long Island and Connecticut arrived in 1664/1665 on the banks of the Elizabeth River and selected a site 2.5 miles from its confluence with the Arthur Kill. The initial settlement and the encompassing tract of land were named Elizabethtown in honor of the wife of Sir George Carteret, a proprietor of East Jersey. The Elizabethtown colony was organized according to the New England town plan, as a nucleated village containing a meeting house with farm lots surrounding the village. In Elizabeth, 4-acre home lots were surveyed on the first upland beyond the salt marsh along both banks of the Elizabeth River extending for 2 miles upriver (Wacker 1975:249). These "townlots" were surveyed in the form of long lots, with their short sides abutting the river, providing each with river frontage. Surrounding the town were outlying farm lots (Heritage Studies 1985:56). The Elizabeth River provided navigation and hydropower for shipping and milling as well as drinking water (Leo et al. 1979).

Settlement along Newark Bay and the Arthur Kill continued with the founding of Newark (1666) to the north and Woodbridge (1668) to the south, thereby fixing the boundaries of the Elizabethtown settlement and the future Union County. In 1682 the General Assembly of the Province of New Jersey created four counties in East Jersey: Bergen, Middlesex, Monmouth, and Essex. The Elizabethtown territory as well as Newark was included within the bounds of Essex County. In 1693 the Assembly further divided these counties into townships and finally, in 1740, Elizabethtown Township received the status of Borough by Royal Charter. The following year its western boundary was defined with the formation of Somerset County (Snyder 1969).

The centralized New England town plan of Elizabeth soon broke down as dispersed hamlets and clusters of farms appeared in different localities (Ricord 1897). These hamlets, such as Lyons Farms (Hillside), Connecticut Farms (Union), and Rahway, developed westward from Elizabeth along rivers and streams between 1669 and 1738. Communities too far from the civic and religious center of Elizabeth built their own churches and schools and were designated wards of Elizabeth. Such wards were Westfield (1735), New Providence (1738), Connecticut Farms (1740), Rahway (1741), and Springfield (1746).

Elizabeth Point became a strategic location during the Revolutionary War, as it was the location of many crossings by British troops. From 1777 to the end of the war, the British maintained fortifications at the Point to guard their troop crossing. Two battles occurred at the Point, the first on July 21, 1778, and the second on June 8, 1780. After the Revolutionary War, Elizabethtown territory was divided into townships, with the older villages serving as administrative centers (Union County Cultural and Heritage Programs Advisory Board 1982:5-6).

Farming continued to be the mainstay of Union County after the Revolutionary War. The extensive salt meadows adjacent to the Arthur Kill, Rahway River, and Morses Creek provided excellent grazing lands. During the course of the nineteenth century, the farms of the area continued to produce a variety of raw and processed grain and livestock products. Farms located adjacent to navigable waterways thrived, having the most efficient access to the New York, Elizabeth, and Perth Amboy marketplaces. Between 1801 and 1829, 30 turnpikes were built in New Jersey. The improved roads increased the efficiency of overland transportation, and together with an expanding road network facilitated access to fresh farmland (Community Pride Publications 1985:23; Heritage Studies 1979:3; New Jersey Department of Transportation [NJDOT] 1975:5-9; Union County Department of Parks and Recreation 1986).



FIGURE 2: New Jersey and New York APAs in 1690

SOURCE: Lea 1690

Elizabeth Point did not become developed until after the Elizabethtown-Somerville Railroad was built to that location in 1835 (Figure 3). A group of New York City businessmen, drawn by the shipping and transportation facilities, laid out the town of Elizabethport. A large railroad depot was built on the southwest corner of Broadway and Front Street, just north of the APE. The railroad tracks ran down the center of 100-foot-wide Broadway (Rolfe 1835). Expansion of the railroad system occurred in the 1870s. A roundhouse, freight depot, and other railroad structures were built in the vicinity of the APE on the Arthur Kill (Sanborn 1889).

During the mid- and late nineteenth century, as urban areas of New York, Newark, and Elizabeth expanded, settlement focused along the railroad corridors serving the cities (Sanborn Map Company 1886, 1891, 1896a, 1896b, 1901a, 1901b). This pattern of growth was typical of many regions of the country, in which railroads created “metropolitan corridors” along which industrial development and suburban growth took place (Stilgoe 1983, 1988). In 1865 the Pennsylvania Railroad built a branch line from Rahway to Perth Amboy, now part of Conrail’s Perth Amboy Division, located west of the APE. The villages of Colonia and Avenel, promoted as pleasant retreats from New York City and fitting settings for country estates, were established along this line in the 1870s. As railroads wove a tight web of transportation corridors across the region in the later 1800s, the large farm tracts established during the colonial period were subdivided into smaller farms. These small and more specialized farms engaged in truck gardening, emphasizing the production of perishable vegetables, fruit, and berries for the growing urban markets in Elizabeth, Perth Amboy, and New York (Clayton 1882:396; Fridlington 1981; Heritage Studies 1979:3; Ricord 1897:636; Union County Department of Parks and Recreation 1986).

Late nineteenth-century expansion of railroad lines and steamship transportation, combined with industrialization and rapid growth of the ports of Elizabeth and Newark, supplied the forces that transformed the land in the vicinity of the APE from a rural hinterland to a heavily populated extension of urban New York and Newark. In 1871 the Pennsylvania Railroad gained control of the key route between New York and Philadelphia by leasing the several railroad companies, including the Philadelphia & Trenton Railroad, the Camden and Amboy Railroad, and the United Canal & Railroad Companies of New Jersey, for a period of 999 years. By 1876 the Perth Amboy and Elizabethport Railroad was in place, paralleling the earlier New Jersey Railroad to the west, and flanking the APE on the east. This line had become the Long Branch Division of the Central Railroad of New Jersey by 1882. Attracted by the flat land and nearby shipping facilities, industries, including chemical and fertilizer companies and iron works, were established along the shoreline of the Arthur Kill between Perth Amboy and Elizabeth during the 1870s, 1880s, and 1890s (Clayton 1882:396; Drury 1992:255-256; Everts and Stewart 1876; Honeyman 1923:448; Robinson 1882:59; NJDOT 1975:21; USGS 1900, 1905; Wall and Pickersgill 1921:473).

Late nineteenth-century links to the New Jersey Central and Pennsylvania railroads crossed the APE and served developing industrial areas along the Arthur Kill (Everts and Stewart 1876; Robinson 1882; USGS 1900, 1905). The New Jersey Terminal Railroad, built by 1900, connected industrial developments at Chrome and Star Landing with the Pennsylvania Railroad at Rahway. The line is now abandoned. The Port Reading Branch of the Philadelphia and Reading Railroad was built in 1890-1892. Running parallel to the southern end of the APE, this line transported coal and freight to the Port Reading shipping terminus on the shore of the Arthur Kill (Everts and Stewart 1876; Linden Centennial Committee Corporation c.1961:26; Ludewig 1970; USGS 1900, 1905, 1955, 1956, 1981a; Wall and Pickersgill 1921:409-410).

During the early twentieth century the section of the Arthur Kill between Perth Amboy and Elizabeth continued to develop as a center of heavy industry (Sanborn Map Company 1901a, 1901b, 1908, 1909, 1912, 1916a, 1916b, 1920, 1924a, 1924b, 1929, 1931). The iron, fertilizer, and chemical plants of the late nineteenth century were replaced by larger plants focusing on oil refining and the production of steel, chrome, and tin. A steel factory opened at Chrome (now part of Carteret) in 1902. The Standard Oil Company (Exxon) established the first oil refinery on the Arthur Kill in Linden in 1909. Grasselli Chemical Company, now a part of E.I. du Pont de Nemours, was established in Linden during World War I (Heritage Studies 1979:1; Honeyman 1923:448; Union County Department of Parks and Recreation 1986; Wall and Pickersgill 1921:473).

Railroad and industrial expansion continued to stimulate residential and commercial development around the industrial plants and along the railroad lines leading to Newark and New York during the early twentieth century. Some residential developments provided homes for increasing numbers of immigrant laborers seeking work in the burgeoning industrial economy. Pockets of this housing still exist along the north side of Bayway Avenue and Krakow Street.

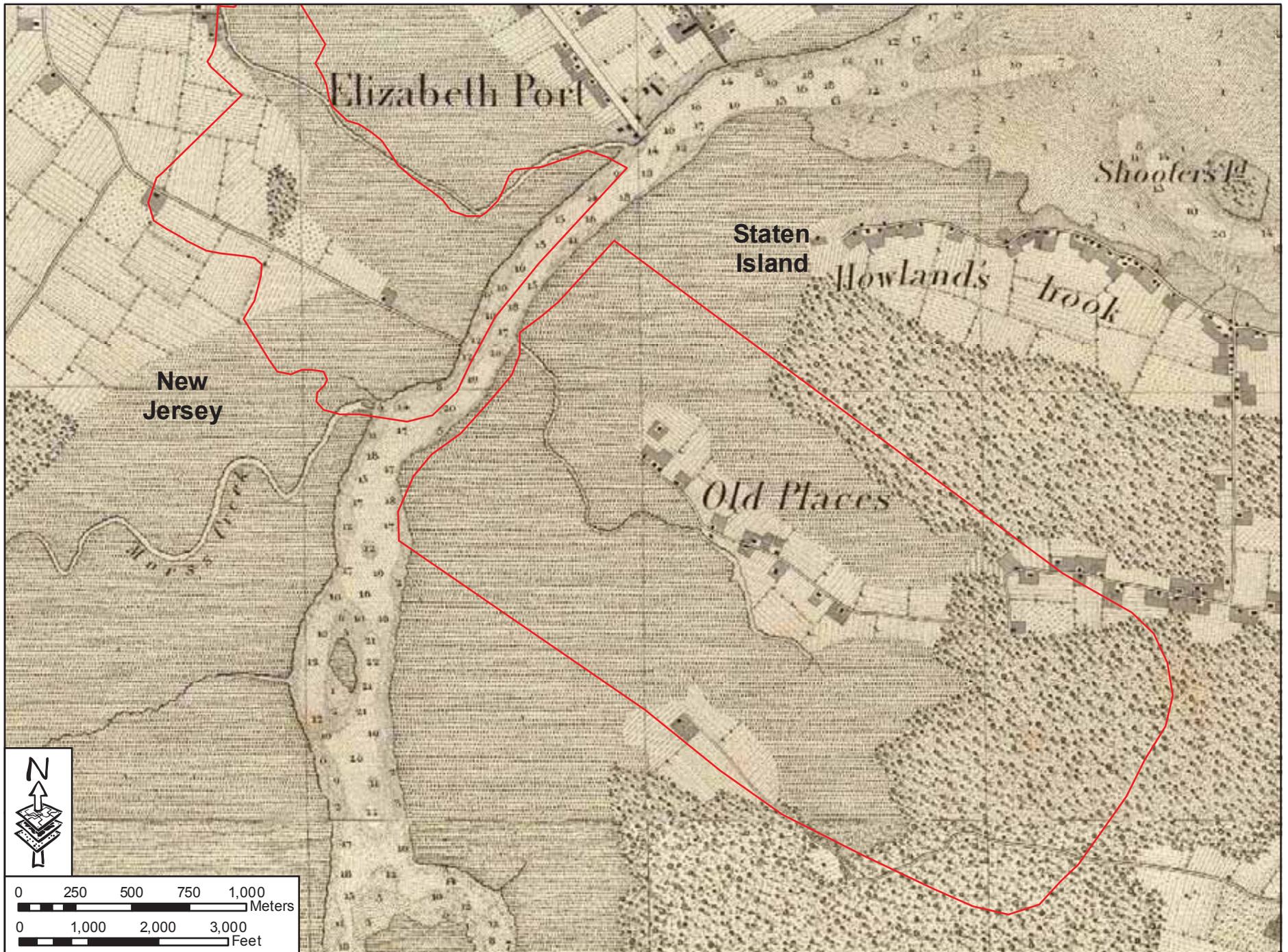


FIGURE 3: New Jersey and New York APES in 1844

SOURCE: U.S. Coastal Survey 1844

The development of electric street cars or trolleys provided efficient mass transportation for northern New Jersey's expanding population. By 1900 trolley lines had appeared in almost all the urban areas in New Jersey, and long distance excursions were common. As trolley lines expanded, electric companies consolidated to help bear the costs of operating the associated power stations. The Public Service Corporation, composed of the Public Service Gas Company and the Public Service Electric Company, was founded in 1903 to provide gas and electric services to the general public and electric power for street railways. In 1907 the corporation created a third division, the Public Service Railway Company, which assumed all trolley-related business and activities, including the operation of the "Fast Line," which made the trip from Jersey City to Trenton in five hours (NJDOT 1975:22-23). The opening of the Goethals Bridge in 1928 expanded the role of the automobile as an important transportation link between New York and New Jersey.

Large-scale development along the Arthur Kill shoreline recommenced during World War II with the establishment of large tank farms providing storage for gas, oil, and chemicals. Between 1947 and 1951 Public Service Electric & Gas completed four of the five generating stations on the Arthur Kill, and by 1970 the fifth and largest, the Sewaren Generating Plant, had been brought on line. This same period saw the construction of the GAF and American Cyanamid chemical plants, the Linden Chlorine Plant, and several petroleum fuel terminals on Tremley Point in Linden (Ludewig 1970:77; Mason 1989:28; USGS 1955, 1956).

With the intent of easing problems of traffic congestion and providing efficient transportation for the growing post-war suburban population, construction of Route 100, the precursor to the New Jersey Turnpike, commenced in Woodbridge and Carteret in 1947. The New Jersey Turnpike itself, running 147 miles from the Delaware River Bridge to the George Washington Bridge, was completed in 1952. The turnpike provided residents of Woodbridge, Carteret, and Linden with easy access to the greater metropolitan area, enabling them to seek employment elsewhere, and enabling workers from outside the area to make the reverse commute (Gillespie and Rockland 1989:23, 37-38; Kraft 1977:9; NJDOT 1975:45; USGS 1955, 1956, 1981a, 1981b).

The far-reaching road improvements of the 1950s and the success of motor buses in mass transportation led to the demise of the Public Service Electric Trolley lines. Abandoned by 1961 as a trolley line, the former electric trolley corridor currently functions as an active utility right-of-way providing passage for electric lines and gas and oil pipelines (Geological Survey of New Jersey 1922, 1930; NJDOT 1975:23; Sanborn Map Company 1920, 1924b, 1931; USGS 1955, 1956).

An analysis of historical maps shows a steady development away from open meadowland to a heavily industrialized area beginning in the second half of the nineteenth century. A map depicting the APE at the time of the American Revolution shows the APE to be open meadow, located south of lots lining either side of the Elizabeth River. The 1862 Meyer and Witzel Topographic Map of Union County shows the city of Elizabeth to be extensively developed and docks and a ferry landing present along the shore north of the APE. The area south of the Elizabeth River appears to be relatively untouched by commercial development. The 1882 Robinson Atlas of Union County depicts the modern street grid largely in place, with paper block lines representing some of the streets. A number of unidentified structures are present along the south side of Bayway Avenue east of South Front Street. The 1889 Sanborn Insurance map illustrates the accelerated pace of the industrialization of the New Jersey section of the APE. A large complex belonging to the Bowker Fertilizer Company, along with a refinery and extraction works, are present at the end of Bayway Avenue at the shoreline of the Arthur Kill. The 1903 Sanborn Insurance map shows the Bowker complex to have been expanded to include additional acid tanks and heavy chemical manufacturing building connected by a rooftop tramway. The Standard Oil Company facility is shown opposite the Bowker complex on the west side of South Front Street. Interestingly, several tenements are pictured on the northwest corner of Bayway Avenue and South Front Street. The 1906 Bauer map shows the network of rail lines leading directly to the large industrial complexes situated along the Arthur Kill (Figure 4). The 1922 Sanborn Insurance map shows the entire shoreline to be occupied by elaborate industrial complexes consisting of refineries, manufacturing plants, and underground tanks. The 1929 map of Union County is one of the earliest maps to depict the Goethals Bridge through the APE (Figure 5). The bridge, along with the numerous railroad lines and street grid, created a labyrinth of features, which by that time had obliterated any vestiges of the early nineteenth-century shoreline meadows. The 1930 Sanborn Insurance map shows a rather narrow approach ramp for the Goethals Bridge just north of Bayway Avenue. The 1922 Sanborn Insurance map updated in 1950 depicts the Phelps Dodge complex and the approach to the Goethals Bridge to be part of the continually changing New Jersey shoreline.

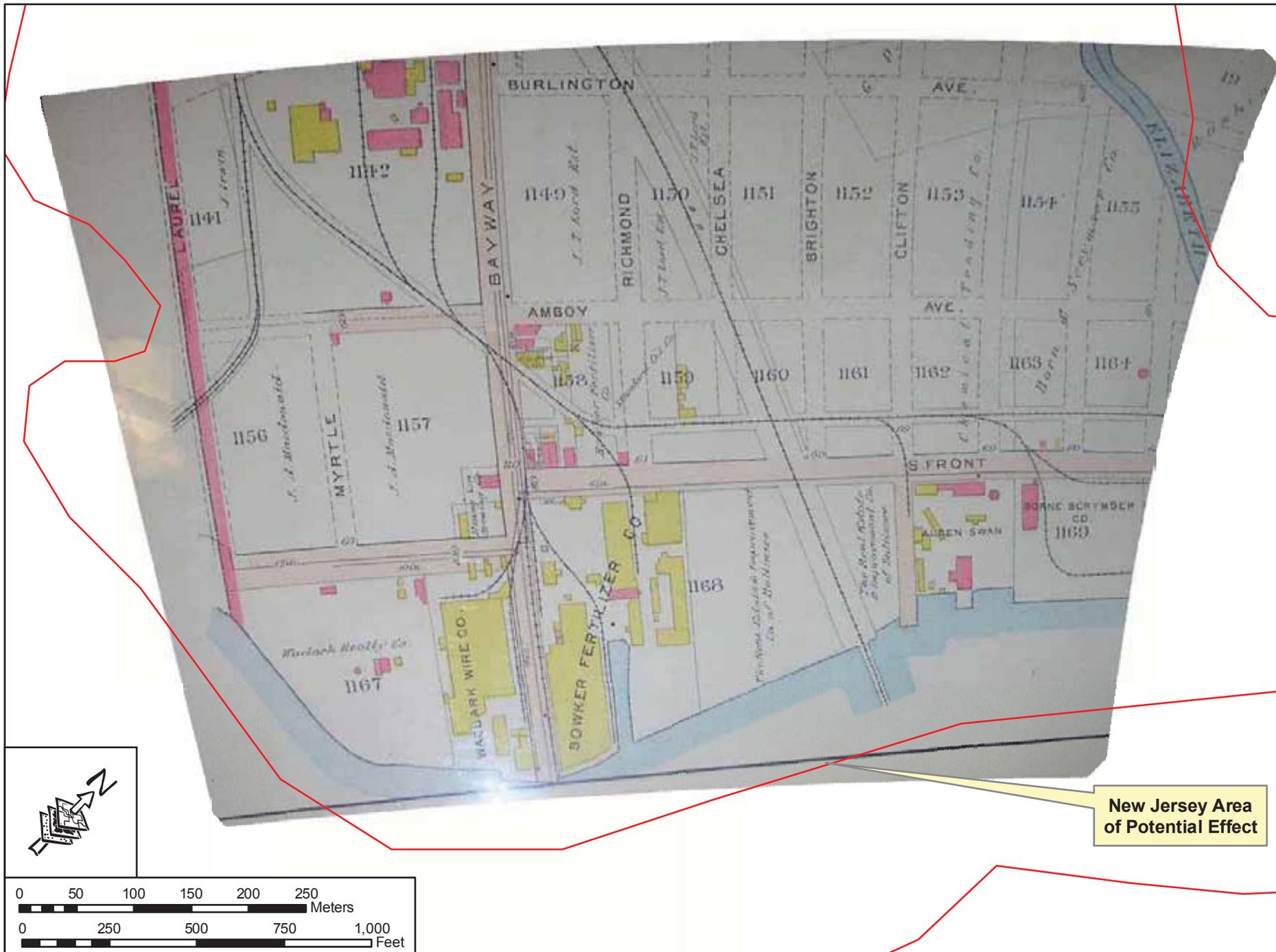


FIGURE 4: Portion of the New Jersey Section of the APE in 1906

SOURCE: Bauer 1906

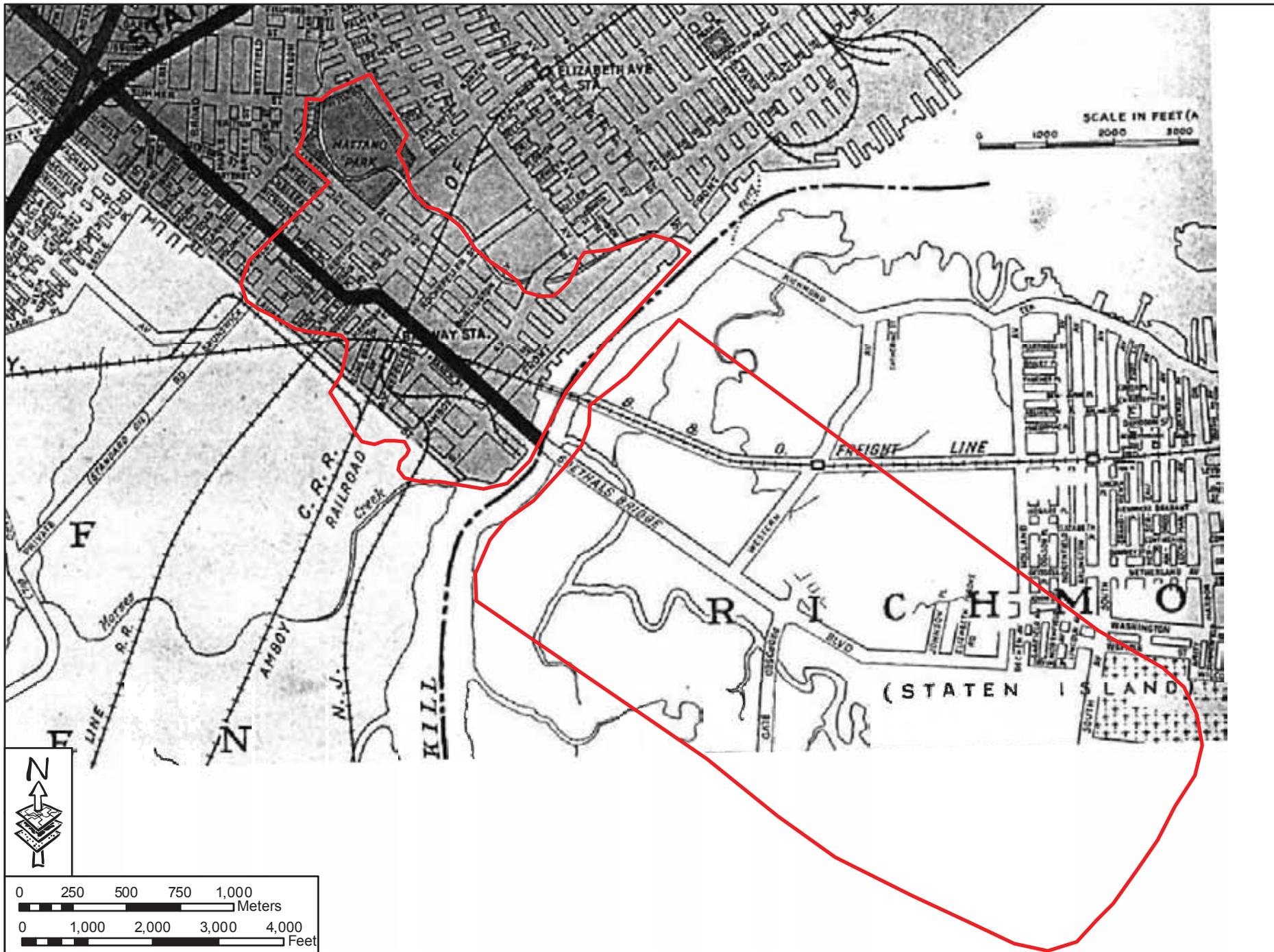


FIGURE 5: New Jersey and New York APEs in 1929

SOURCE: Bauer 1929

### 3.2 New York Section of the APE: Historical Overview

From 1621 to 1664 Staten Island was part of the Province of New Netherland. The province was administered by the Dutch West India Company, under whose jurisdiction the island received its name. The Native American population resisted Dutch settlement, culminating in the Peach War of 1655, which vastly depopulated the island. In 1662 a handful of dwellings and a small blockhouse were erected on a site above Lower New York Bay, a short distance south and west of the high ground at The Narrows. This settlement, known as Oude Dorp (Old Town), consisted chiefly of Dutch and French colonists from the Palatinate.

In 1664 New Netherland, including Staten Island, was taken over by Great Britain. The last Native American claims to Staten Island were extinguished in 1670, and in 1683 the island was organized as the County of Richmond. Settlement continued under the British, with significant numbers of Huguenots arriving in the last years of the seventeenth century. But the entire population of Staten Island was only 727 by the year 1698, 10 percent of which were slaves (Steinmeyer 1950:18). By the mid-eighteenth century Staten Island's population was a mix of people of Dutch, French, Belgian, English, and African descent (Berger 1985:11).

The earliest European colonization of Staten Island occurred in 1639 with a small band of settlers led by Captain Pieterz De Vries. Another of the earliest European settlements on Staten Island was approximately 4 miles southeast of the APE in the Richmondtown area. Richmondtown was first settled by European colonists in 1680, and by 1710 the area was developing as a small crossroads hamlet (Baugher et al. 1989:48). Richmondtown was first known as Cocklestown (or Cuckoldestowne) because of the multitude of oysters harvested on the shores of Staten Island. Richmondtown became the county seat in 1728 and remained so until 1898, when Staten Island became a borough of the City of New York and the seat of government was moved to St. George.

Between 1750 and 1760 two gristmills were constructed on streams south of the APE (Baugher et al. 1989:60). During the eighteenth century Staten Island developed as a primarily agricultural and fishing community with its county seat at Richmondtown, its principal village. Some of the products raised at that time were beef, pork, wheat, rye, and apples. Fish, oysters, and clams were commonly taken from the waters about the Island, and salt hay was gathered from its extensive salt meadows in the towns of Northfield, Southfield, and Westfield (Akerly 1843; Smith 1970). Peter Kalm, a Swedish naturalist who traveled extensively throughout the colonies during the eighteenth century, made this observation upon visiting Staten Island:

Near the inn [in Elizabethtown] where we had passed the night, we were to cross a river and we were brought over, together with our horses, in a wretched, half-rotten ferry....The country was low on both sides of the river, and consisted of meadows. But there was no other hay to be got, than such commonly grows in swampy grounds; for as the tide comes up in this river, these low plains were sometimes overflowed when the water was high. The people hereabouts are said to be troubled in summer with immense swarms of gnats or mosquitoes, which sting them and their cattle. This was ascribed to the low swampy meadows, on which these insects deposit their eggs, which are afterwards hatched by the heat [Kalm, cited in Steinmeyer 1950:19].

This description appears to refer to the New York section of the APE. The river that Kalm mentions is likely the Arthur Kill. Also, the proximity to Elizabethtown and the description of "swampy grounds" indicates that the ferry landing was located somewhere in the vicinity of the present-day Goethals Bridge, although no remains of the ferry have been mentioned in more recent cultural resource literature.

In July 1776 British forces landed on Staten Island and proceeded to establish a military rule that lasted until the close of the Revolutionary War in 1783 (Berger 1985:11). The Island served as a staging area for British attacks into Long Island and New Jersey, and as a source of produce, wood, and fodder for the increasing military and civilian population. The Revolutionary War had profound effects on the citizens of Staten Island. For example, a study of the house of Christopher Billopp, a wealthy naval officer, revealed that few items reflective of his high social status had survived. Baugher and Venables (1987:49-50) attribute the absence of such items to British confiscation and American looting.

During the Revolutionary War the Richmondtown courthouse and church were destroyed, and in 1808 a Dutch Reformed church was built on the site of the former church (Baugher et al. 1989:60). Period maps show little development in the northwest quadrant of Staten Island, with the major roads located to the east and south. British

troops did engage in a skirmish and occupy a site in the vicinity of Western Avenue and Goethals Road North (Payne and Baumgardt 1986). The 1781 Taylor and Skinner map (Figure 6) shows Richmond Terrace, a road that runs along the northern coastline of Staten Island approximately 0.8 miles north of the APE, in place with a number of structures along the south side.

Following the Revolutionary War residents of Staten Island initially re-established the colonial agrarian socioeconomic system. Beginning in the 1830s, Staten Island caught the attention of wealthy New Yorkers as a fashionable bathing resort and summertime retreat. They built large summer houses along the shores and gradually began to remain year round, particularly in communities such as New Brighton, Stapleton, and Clifton. They were accompanied, if not preceded, by developers, such as Daniel Low, who established the Staten Island Association to promote development along the eastern shore. As a result, many large tracts of formerly agricultural land were gradually subdivided, and former farmhouses, such as the Austen House at Clifton, were remodeled as Victorian “cottages.” By the early 1840s, according to a contemporary writer, “the whole eastern shore” down to Fort Richmond had become “almost a continued village. . . occupied by country seats and town plots” (Akerly 1843:199).

The earliest historic settlement of the APE dates to around 1680, although the first structure does not appear on a map until the time of the American Revolution. This structure, known as the Tunnisen House, is believed to have been located in the vicinity of the Howland Hook Marine Terminal west of Western Avenue (Payne and Baumgardt 1986). A tidal mill was also reported to have existed during this time, but does not appear on any maps (Payne and Baumgardt 1986). A second tidal mill, built in 1804 and possibly a successor to the colonial-era mill, was destroyed by fire around 1898 (Figure 7) (Morris 1898:378; Steinmeyer 1950:90). The APE remained largely agricultural after the Revolution until the early twentieth century. The gradual shift from an agricultural/residential community to an industrial/transportation corridor resulted in the removal of the majority of the early domestic buildings and industrial structures by the middle of the twentieth century.

While the New Jersey side of the Arthur Kill saw tremendous industrial growth during the nineteenth century, the west side of Staten Island remained largely undeveloped. The few industrial enterprises begun during the mid-nineteenth century include the Kreisler Brick Works, the Tottenville Copper Company, the American Linoleum Manufacturing Company, and the Atlantic Terra Cotta Company (Brighton 1997:11). Development within the APE during the second half of the nineteenth century was limited to the area along Western and Washington Avenues (Figure 8; see Figure 3). The 1874 Beers Atlas of Richmond County shows six structures along Old Place Road (Washington Avenue) west of Western Avenue (Beers 1874). All appear to be farmhouses situated on lots ranging in size from 8 to 13 acres. A flouring mill was depicted east of Western Avenue along the south side of Old Place Creek in the vicinity of the APE. The residence of the mill proprietor was located opposite the mill on the north side of Old Place Road.

The 1898 *Atlas of the Borough of Richmond County* shows the APE to be largely in the possession of private owners, with corporate ownership represented by several tracts listed as the “New York Transit and Terminal Company” (Figure 9). Western Avenue and Washington Avenue (the current Goethals Bridge service road) are depicted close to their current alignments. The 1907 *Atlas of the Borough of Richmond County* shows a number of new streets extending north from Washington Avenue (Figure 10). These streets, McKinley Street and Elizabeth Avenue, would be short-lived, as they lay directly in the path of the future Goethals Bridge. The flouring mill first depicted on the 1874 atlas was still standing at that time. The 1917 *Bromley Atlas of the City of New York Borough of Richmond* depicts many of the former farms in the possession of industrial corporations or large realty companies (Figure 11). The 1917 Sanborn Insurance map shows a cluster of buildings that were demolished when construction began on the Goethals Bridge in 1925 (Sanborn Map Company 1917). This same map shows the nineteenth-century flouring mill and associated buildings to have been demolished. Other buildings that were present on nineteenth-century atlases were also gone by this time, evidence of the transition from a farming community to an industrial area, and were replaced by Howland Hook Ferry Company complex along the Arthur Kill shoreline.

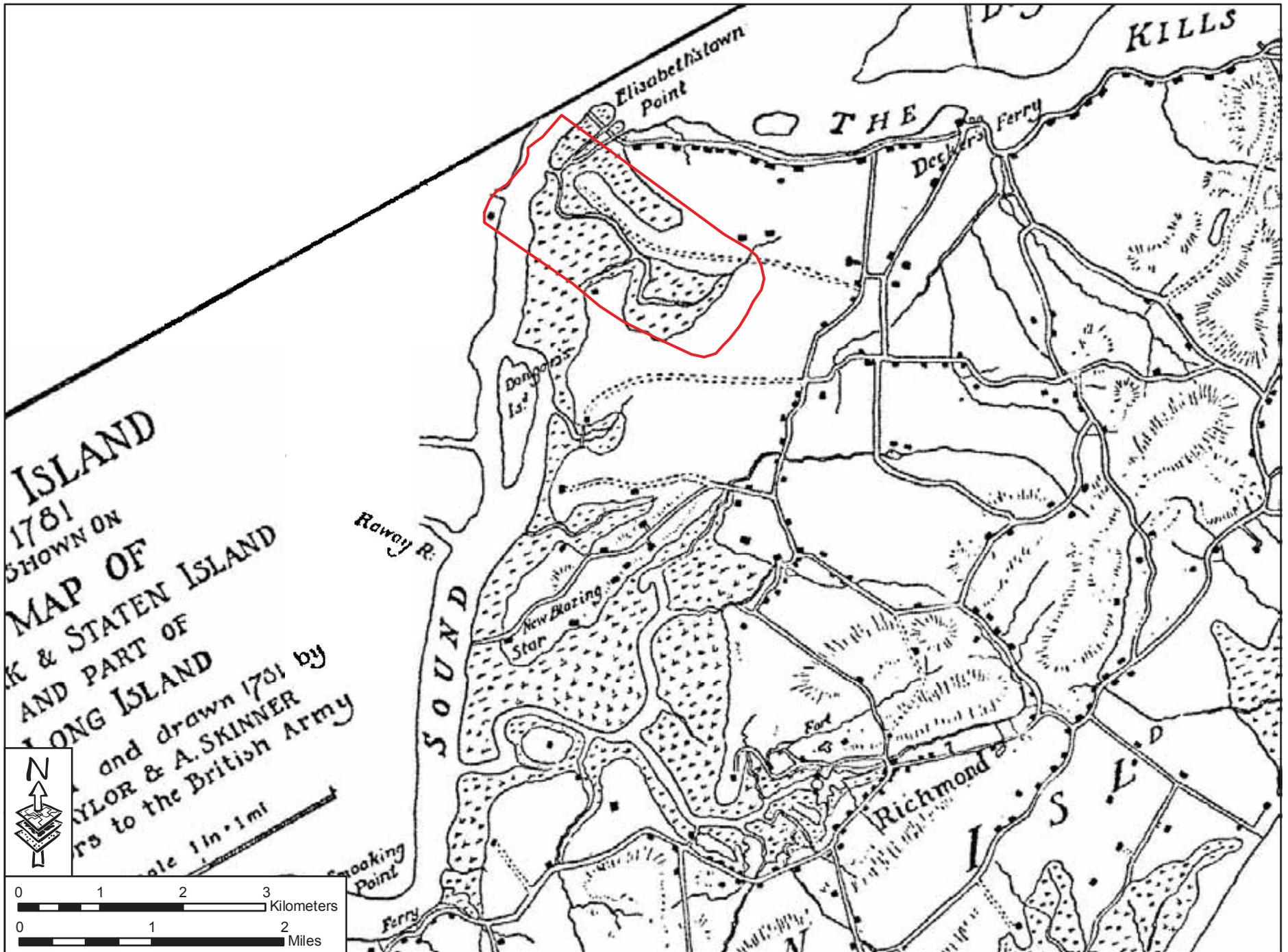
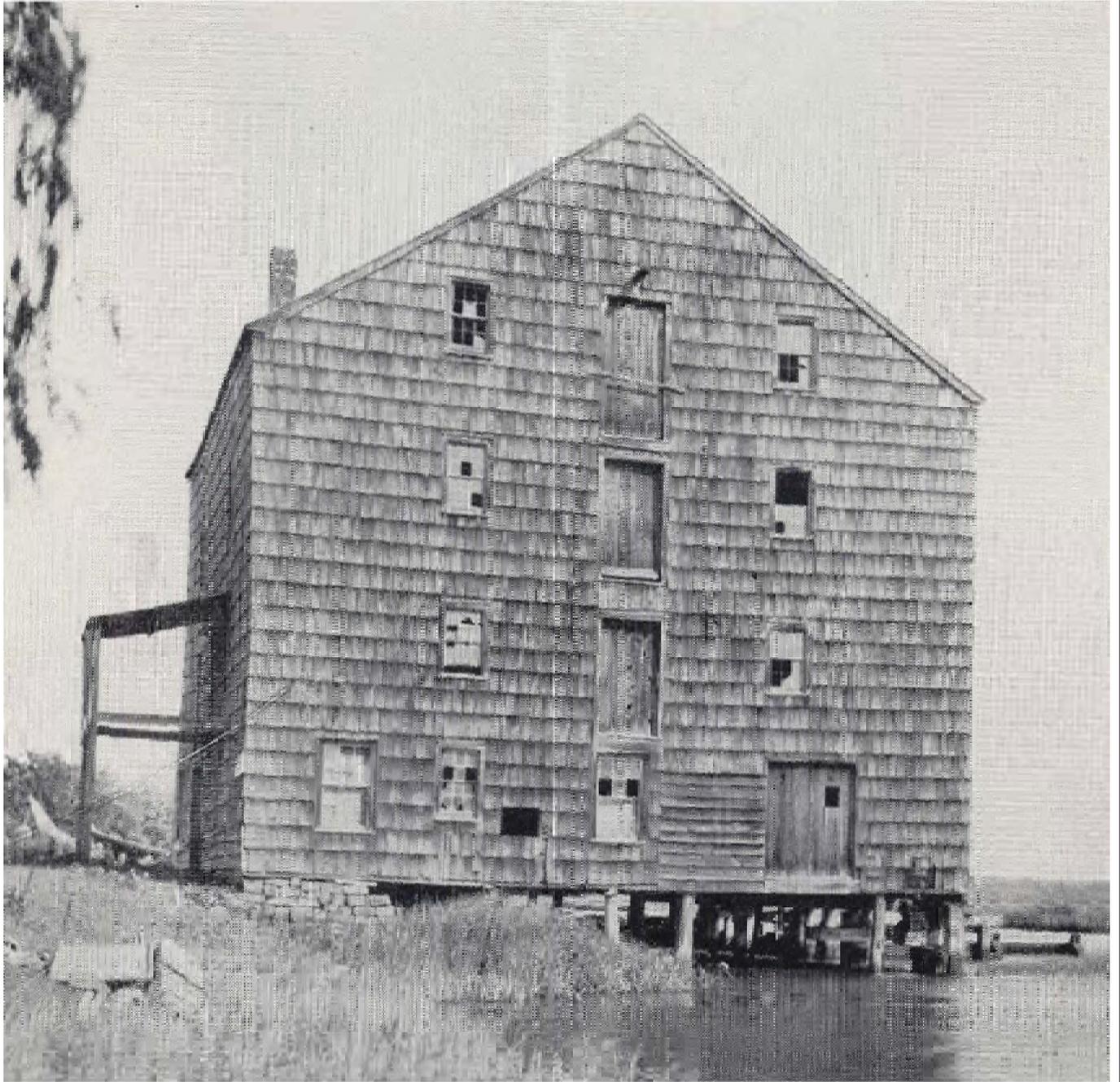


FIGURE 6: New York Section of the APE in 1781

SOURCE: Taylor and Skinner 1781



**FIGURE 7: Circa 1804 Mill Along the Old Place Creek**

*SOURCE: Morris 1898*

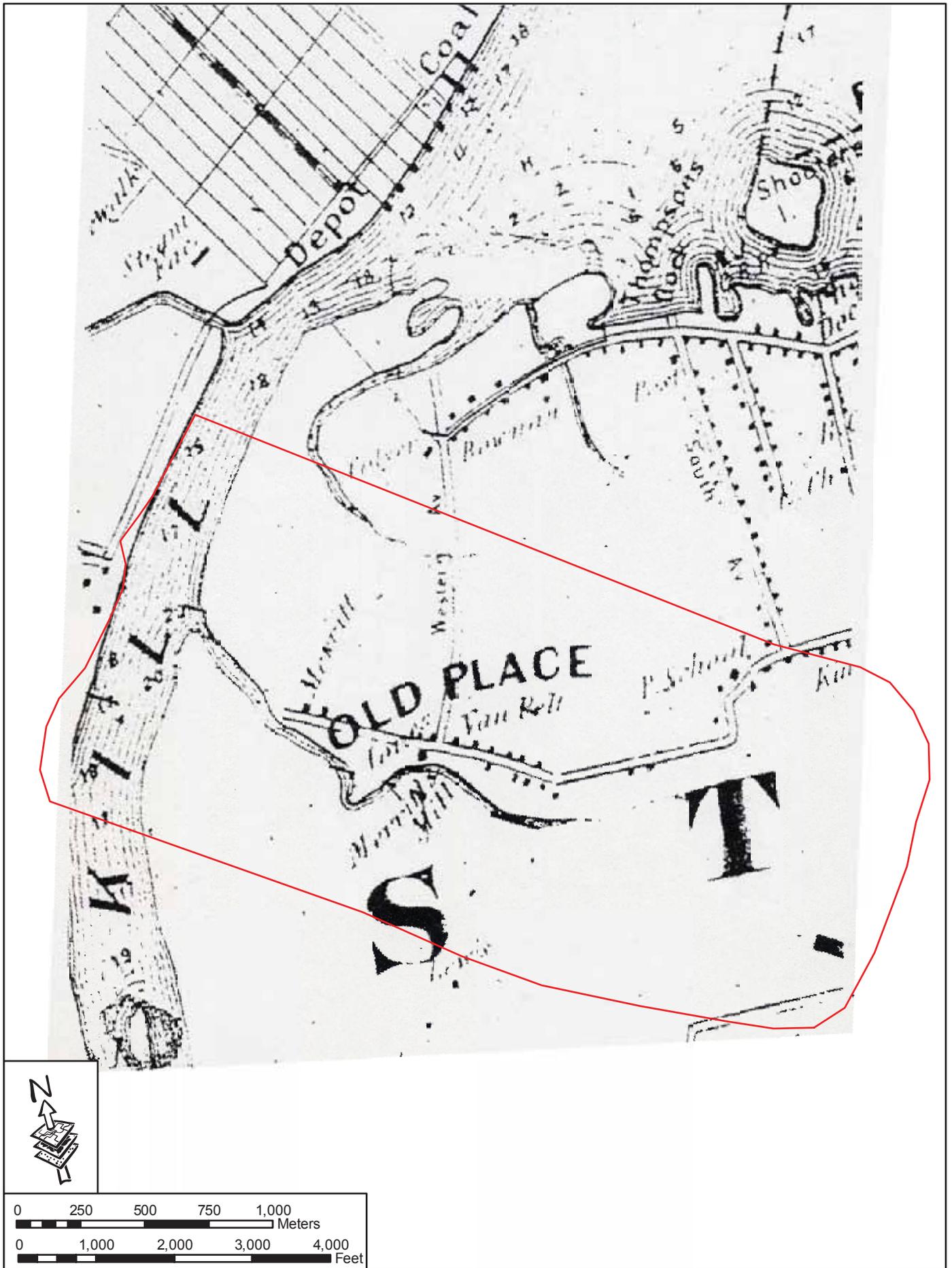


FIGURE 8: New York Section of the APE in 1860

SOURCE: Grover and Baker 1860

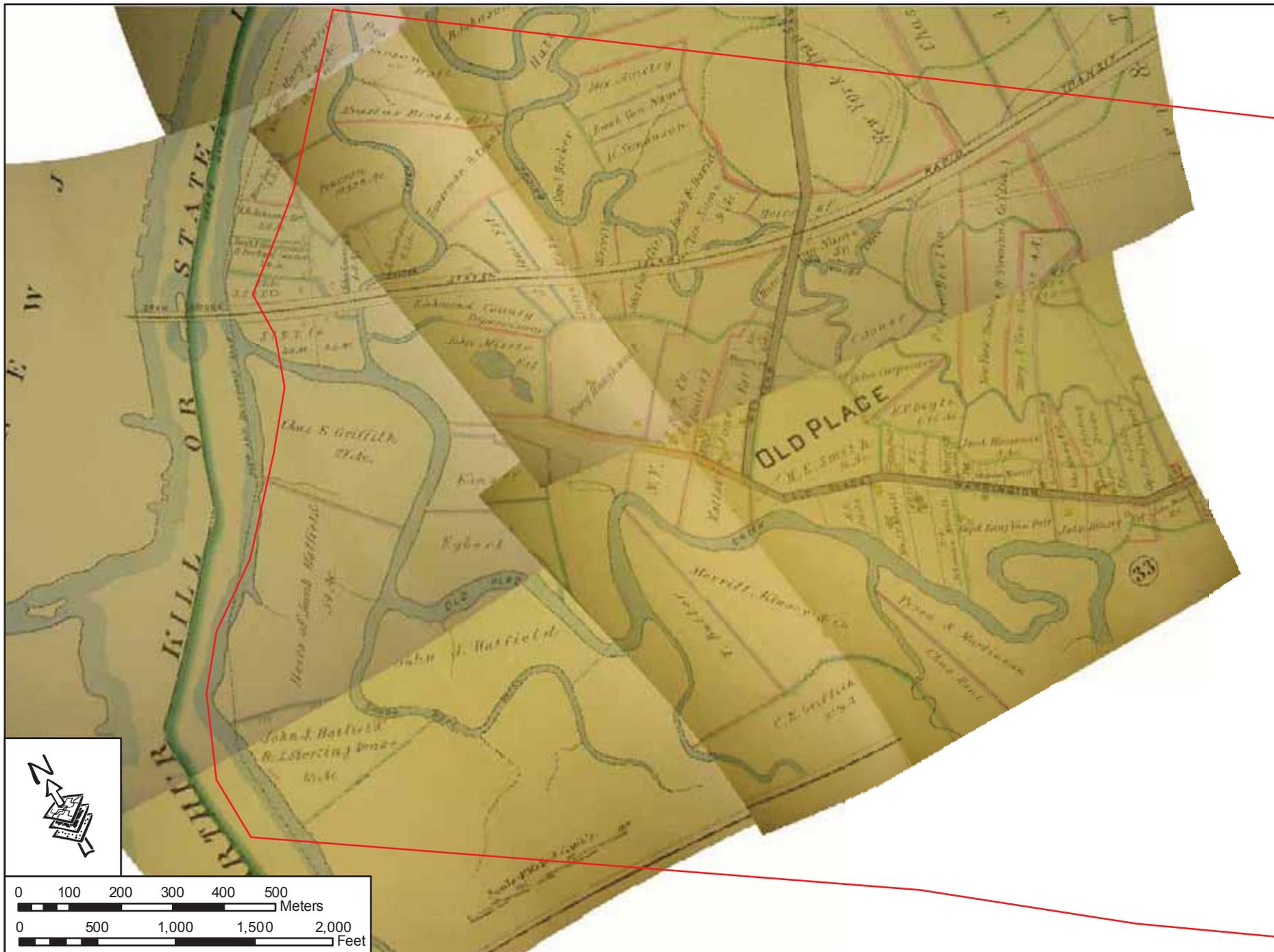
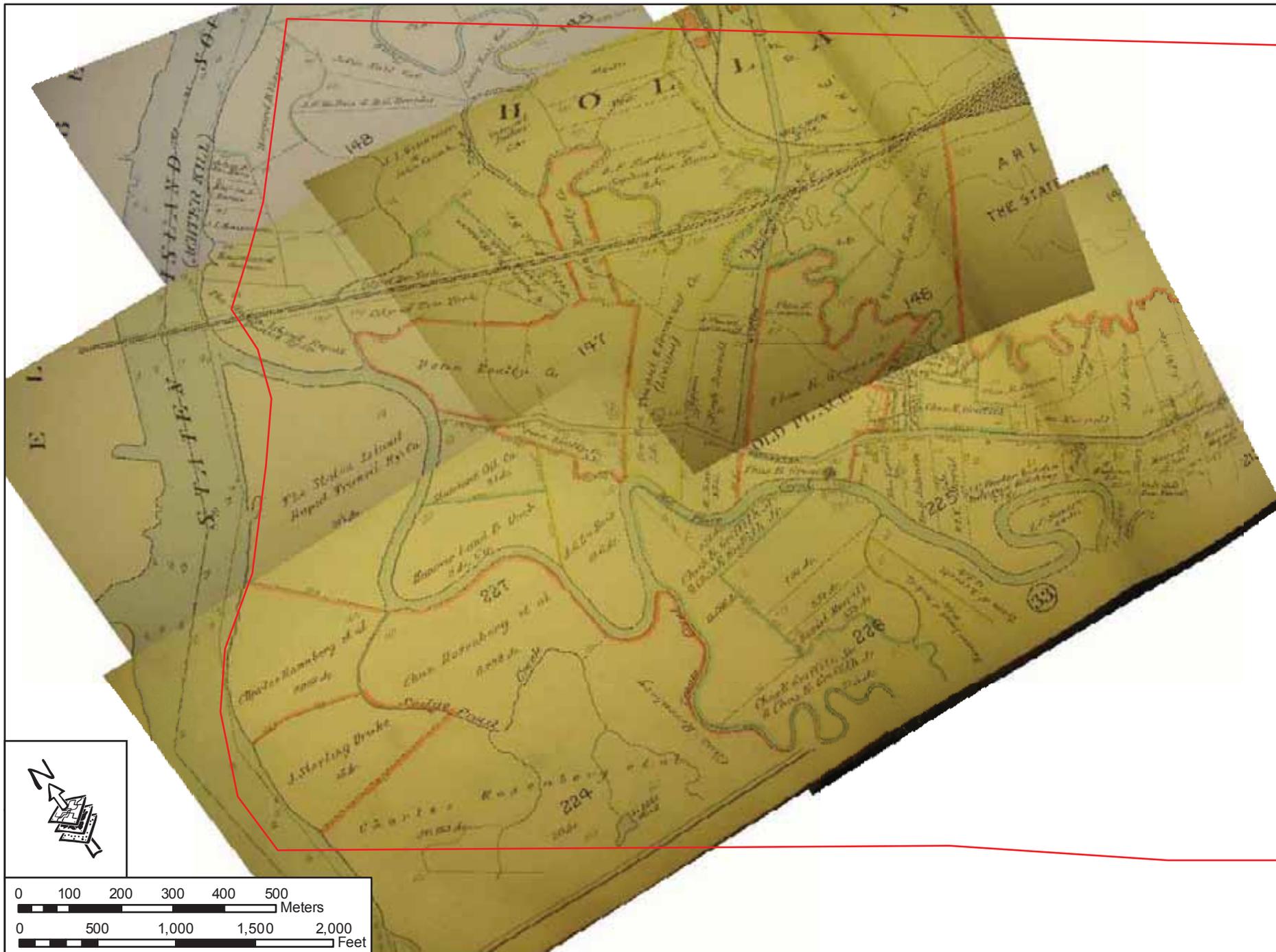


FIGURE 9: New York Section of the APE in 1898

SOURCE: Robison 1898



SOURCE: Robinson 1907

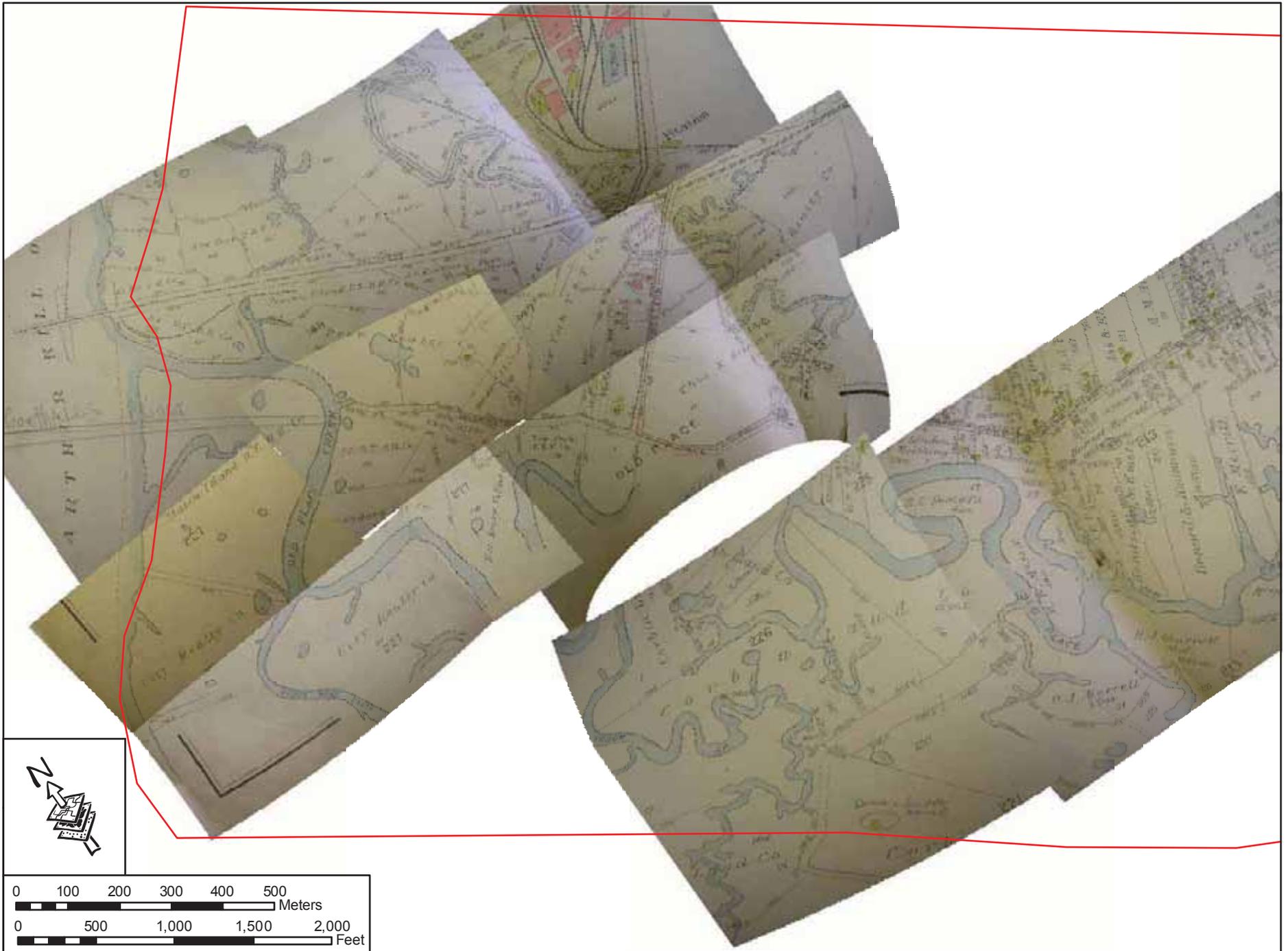


FIGURE 11: Portion of the New York Section of the APE in 1917

SOURCE: Bromley 1917

## 4.0 DATA SUMMARY

### 4.1 Previously Inventoried Historic Resources

In 1994 the United States Coast Guard conducted a historic architectural survey as part of the Staten Island Bridges Program, an earlier planning study undertaken in preparation for the Goethals Bridge Replacement Project (USCG 1997). As part of the study, 24 historic resources were identified within an APE that primarily included properties immediately adjacent to the present bridge. NJHPO Individual Structure Survey Forms were completed for each historic resource and submitted to the NJHPO as part of Section 106 consultation for the Staten Island Bridges Program project. Two of the documented properties, the Goethals Bridge and the Staten Island Railroad Historic District (including the Arthur Kill Lift Bridge), were determined eligible for listing in the National Register by the NJHPO in February 1995 (NJHPO 2006). The remaining 22 historic resources were recommended as not eligible for the National Register (Allee King Rosen & Fleming, Inc. 1994). The NJHPO provided concurrence on the properties recommended as not eligible in July 1995 (NJHPO 1995).

Because of the quality of this earlier documentation, the NJHPO has not required that these 24 historic resources be resurveyed as part of the current project. The NJHPO did request that any substantial changes to the resources identified in the field should be outlined in the historic resources survey report. One property, a house located at 123 Bayway in Elizabeth, has been demolished in the intervening years. None of the other historic resources had been altered significantly enough to warrant additional documentation.

In addition to the 24 previously documented historic resources within the New Jersey APE, background research revealed four additional historic resources within the APE that are eligible for listing in the National Register. These properties are the Perth Amboy and Elizabethport Branch of the Central Railroad of New Jersey (CNJ), the CNJ Bridge over the Elizabeth River, the South Front Street Bridge over the Elizabeth River, and the South First Street Bridge over the Elizabeth River. The Perth Amboy and Elizabethport Branch railroad, built in 1871, linked the CNJ main line with New Jersey's shore communities and was determined eligible for the National Register by the NJHPO in 2000. The CNJ Bridge, a Scherzer-type bascule bridge built in circa 1912, carries the former Perth Amboy and Elizabethport branch line over the Elizabeth River and was determined eligible for listing in the National Register by the NJHPO in 1990. The South Front Street and the South First Street bridges, bascule spans designed by the Strauss Bridge Company and erected in 1920 and 1908, respectively, were determined eligible for the National Register by the New Jersey Department of Transportation in 1995 (NJDOT 2002).

NYSOPRHP files do not contain information on any previously listed or eligible historic resources within the New York portion of the historic architecture APE.

Table 1 lists the previously inventoried historic resources identified within the New Jersey APE, along with a notation on their National Register eligibility status, and provides a key to their location on Figure 12.

### 4.2 Newly Inventoried Historic Resources in the New Jersey APE

As a result of the field investigations, Berger/PB JV identified 50 historic resources within the New Jersey portion of the APE. Most of the historic resources in the New Jersey APE consist of early twentieth-century single or multiple dwellings with very poor architectural integrity. Ten of the New Jersey historic resources consist of large industrial-related or warehouse buildings, including a water pumping station (500-526 Trenton Avenue) and an electrical switching building (530-614 Trenton Avenue). The remaining industrial and warehouse buildings are located along Elizabeth and Linden's South Front Street waterfront area. These buildings have been greatly altered over time in order to serve contemporary manufacturing and warehousing requirements. The Halloran School, built in 1950 at 421-447 Richmond Street, displays modern replacement window units in all of its window bays. Mattano Park, a riverside recreational park designed by the Olmsted Brothers landscape architecture firm in the mid-1920s, has undergone a number of improvements that have removed much of its original design.

After applying the National Register eligibility criteria to the 50 newly identified historic resources within the New Jersey APE, none of the historic resources are recommended as meeting National Register significance and integrity criteria for listing in the National Register. Table 2 summarizes the results of the historic resources survey in the

TABLE 1

## PREVIOUSLY INVENTORIED HISTORIC RESOURCES WITHIN THE APES

FIG. 12 No.	NAME/ADDRESS	BLOCK/LOT	DATE BUILT	NATIONAL REGISTER ELIGIBILITY STATUS
1	Goethals Bridge	N/A	1927	Determined Eligible NJHPO 2/14/1995
2	167 Bayway, Elizabeth	4/179	ca. 1915	Recommended Not Eligible; NJHPO concurrence 7/11/1995
3	147 Bayway, Elizabeth	4/177	ca. 1922	Recommended Not Eligible; NJHPO concurrence 7/11/1995
4	145 Bayway, Elizabeth	4/176	ca. 1922	Recommended Not Eligible; NJHPO concurrence 7/11/1995
5	137-143 Bayway, Elizabeth	4/175	ca. 1917	Recommended Not Eligible; NJHPO concurrence 7/11/1995
6	135 Bayway, Elizabeth	4/174	ca. 1922	Recommended Not Eligible; NJHPO concurrence 7/11/1995
7	133 Bayway, Elizabeth	4/173	ca. 1922	Recommended Not Eligible; NJHPO concurrence 7/11/1995
8	123 Bayway, Elizabeth	4/172	ca. 1910	DEMOLISHED
9	119 Bayway, Elizabeth	4/171	ca. 1950	Recommended Not Eligible; NJHPO concurrence 7/11/1995
10	117 Bayway, Elizabeth	4/170	ca. 1922	Recommended Not Eligible; NJHPO concurrence 7/11/1995
11	109 Bayway, Elizabeth	4/167	ca. 1922	Recommended Not Eligible; NJHPO concurrence 7/11/1995
12	100-103 Bayway, Elizabeth	4/163	ca. 1922	Recommended Not Eligible; NJHPO concurrence 7/11/1995
13	93-95 Bayway, Elizabeth	4/162	ca. 1903	Recommended Not Eligible; NJHPO concurrence 7/11/1995
14	89 Bayway, Elizabeth	4/159	ca. 1922	Recommended Not Eligible; NJHPO concurrence 7/11/1995
15	663 Amboy Avenue, Elizabeth	4/51	ca. 1922	Recommended Not Eligible; NJHPO concurrence 7/11/1995
16	100-106 Krakow Street, Elizabeth	4/51	ca. 1922	Recommended Not Eligible; NJHPO concurrence 7/11/1995
17	112 Krakow Street, Elizabeth	4/833	ca. 1922	Recommended Not Eligible; NJHPO concurrence 7/11/1995
18	114 Krakow Street, Elizabeth	4/834	ca. 1922	Recommended Not Eligible; NJHPO concurrence 7/11/1995
19	118 Krakow Street, Elizabeth	4/835	ca. 1922	Recommended Not Eligible; NJHPO concurrence 7/11/1995
20	120 Krakow Street, Elizabeth	4/836	ca. 1980	Recommended Not Eligible; NJHPO concurrence 7/11/1995
21	Bayway Terminal Storage Warehouse, 666 South Front Street, Elizabeth	4/1471	1927	Recommended Not Eligible; NJHPO concurrence 7/11/1995
22	Phelps Dodge Complex, Elizabeth	4/55 & 4/1457	ca. 1903-1950	Recommended Not Eligible; NJHPO concurrence 7/11/1995
23	Borne Chemical Company Complex, Elizabeth	4/1469	1916-1917	Recommended Not Eligible; NJHPO concurrence 7/11/1995
24	Staten Island Railroad Historic District, Elizabeth	N/A	1889-1959	Determined Eligible NJHPO 2/27/1995
25	Perth Amboy and Elizabethport Branch of the Central Railroad of New Jersey, Elizabeth	N/A	1871	Determined Eligible NJHPO 8/30/2000
26	CNJ over the Elizabeth River Bridge	N/A	ca. 1912	Determined Eligible NJHPO 4/9/1990
27	South Front Street over Elizabeth River, Elizabeth	N/A	1920	Recommended Eligible NJDOT; NJHPO concurrence 1995
28	South First Street over Elizabeth River, Elizabeth	N/A	1908	Recommended Eligible NJDOT; NJHPO concurrence 1995

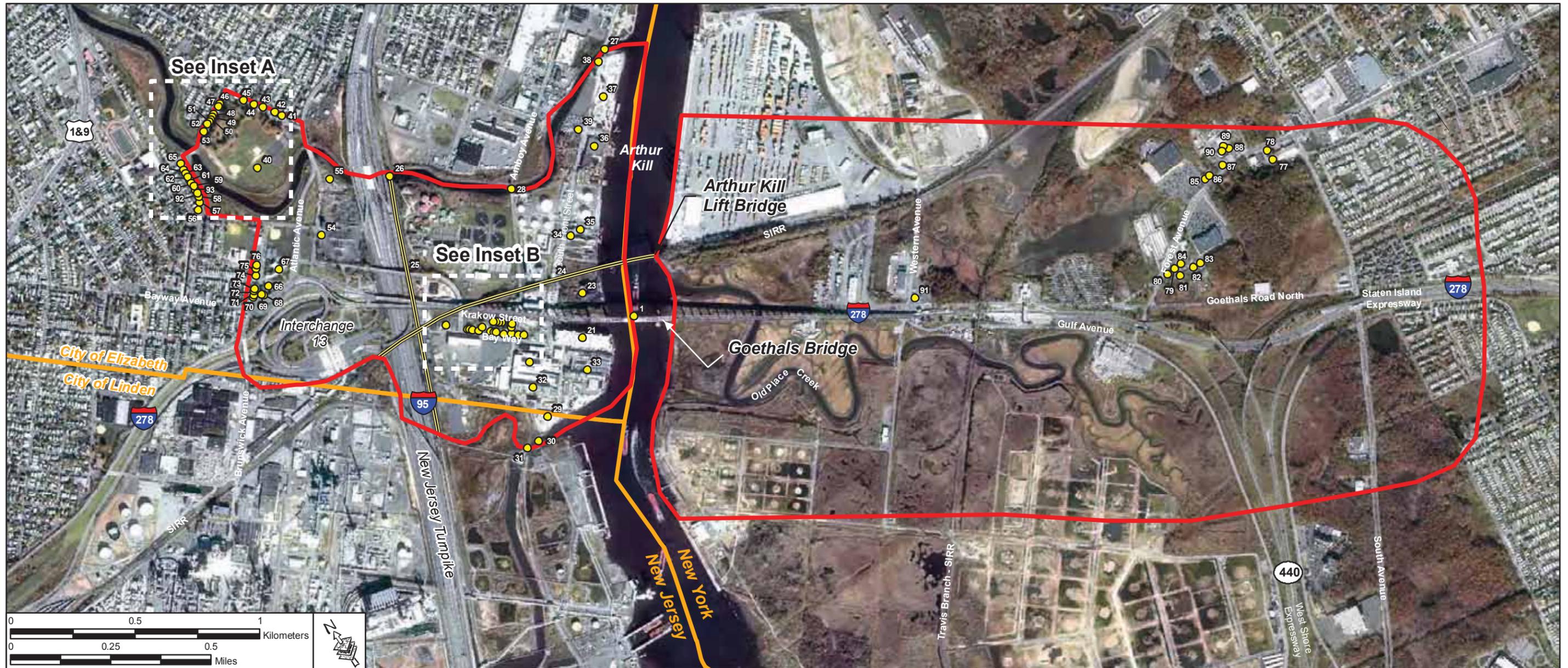
TABLE 2

## NEWLY INVENTORIED HISTORIC RESOURCES IN THE NEW JERSEY APE

FIG. 12 No.	NAME/ADDRESS	BLOCK/LOT	DATE BUILT	PRIMARY RESOURCE DESCRIPTION	RECOMMENDATION
29	2710 Allen Street Extension, Linden	586/4	ca. 1920	3-story concrete block industrial building	Not Eligible
30	Carringer Road over Morses Creek, Linden	N/A	ca. 1950	timber stringer road bridge	Not Eligible
31	RR Bridge over Morses Creek, Linden	N/A	ca. 1920	timber stringer railroad bridge	Not Eligible
32	735-757 South Front Street, Elizabeth	4/1458	ca. 1919	3-story concrete block industrial building	Not Eligible
33	700-758 and 760-766 South Front Street, Elizabeth	4/1472 & 4/1474	ca. 1950	1-story brick industrial building	Not Eligible
34	349-527 and 534-538 South Front Street, Elizabeth	4/1447 & 4/1466	ca. 1924	1-story stuccoed industrial building	Not Eligible
35	346-532 & 349-527 South Front Street, Elizabeth	4/1464 & 4/1445	ca. 1940	3-story concrete block industrial building	Not Eligible
36	15-21, 76-78, 80-312, and 314-344 South Front Street, Elizabeth	4/1438.B, 4/1461, 4/1462, & 4/1463	ca. 1920	2-story hollow tile industrial building	Not Eligible
37	2-74 South Front Street, Elizabeth	4/1459	ca. 1930	1-story hollow tile industrial building	Not Eligible
38	1-13 South Front Street, Elizabeth	4/1436	ca. 1875	2-story frame dwelling	Not Eligible
39	65-85 South Front Street, Elizabeth	4/1438.A	ca. 1950	1-story concrete block industrial building	Not Eligible
40	Mattano Park, Elizabeth	4/59, 5/453.B, 5/1262, 7/968	ca. 1927	municipal park	Not Eligible
41	437 Doyle Street, Elizabeth	5/241.I	ca. 1950	1-story gable-front frame dwelling	Not Eligible
42	436 Redcliffe Street, Elizabeth	5/1153.I	ca. 1950	1-story gable-front frame dwelling	Not Eligible
43	437 Redcliffe Street, Elizabeth	5/1144.D	ca. 1950	1-1/2 story gable-front frame dwelling	Not Eligible
44	436 Loomis Street, Elizabeth	5/859.E	ca. 1950	1-1/2 story gable-front frame dwelling	Not Eligible
45	475 Fifth Street, Elizabeth	5/895	ca. 1950	1-1/2 story side-gable brick dwelling	Not Eligible
46	505 South Fifth Street, Elizabeth	5/1251	ca. 1945	1-story gable-front brick dwelling	Not Eligible
47	507 South Fifth Street, Elizabeth	5/1252	ca. 1930	DENIED ACCESS	Not Eligible
48	511-513 South Fifth Street, Elizabeth	5/1253	ca. 1950	2-story frame double house	Not Eligible
49	515 South Fifth Street, Elizabeth	5/1254	ca. 1950	2-story frame double house	Not Eligible
50	517 South Fifth Street, Elizabeth	5/1256	ca. 1920	2-1/2 story tee-plan frame dwelling	Not Eligible
51	521 South Fifth Street, Elizabeth	5/1257	ca. 1900	2-1/2 story gable-front frame dwelling	Not Eligible
52	525 South Fifth Street, Elizabeth	5/1258	ca. 1900	2-1/2 story gable-front frame dwelling	Not Eligible
53	539 South Fifth Street, Elizabeth	5/1261	ca. 1940	2-story concrete block house and shop	Not Eligible

TABLE 2 (continued)

FIG. 12 No.	NAME/ADDRESS	BLOCK/LOT	DATE BUILT	PRIMARY RESOURCE DESCRIPTION	RECOMMENDATION
54	Bayway Switching Station, 530-614 Trenton Avenue, Elizabeth	4/1582	ca. 1930	3-story brick switching station	Not Eligible
55	City of Elizabeth Pumping Station, 500-526 Trenton Avenue, Elizabeth	4/1582.A	ca. 1920	1-story Art Deco brick pumping station	Not Eligible
56	630 Clarkson Avenue, Elizabeth	4/378.A	ca. 1950	2-story brick apartments and commercial	Not Eligible
57	627 Arnett Street, Elizabeth	4/58	ca. 1955	3-story brick apartment building	Not Eligible
58	625 A/B Arnett Street, Elizabeth	4/57.G & 4/57.H	ca. 1950	2-story side-gable twin dwelling	Not Eligible
59	617 A/B Arnett Street, Elizabeth	4/57 & 4/57.B	ca. 1950	2-story side-gable twin dwelling	Not Eligible
60	615 A/B Arnett Street, Elizabeth	4/56.H & 4/56.I	ca. 1950	2-story side-gable twin dwelling	Not Eligible
61	613 A/B Arnett Street, Elizabeth	4/56.F & 4/56.G	ca. 1950	2-story side-gable twin dwelling	Not Eligible
62	609 A/B Arnett Street, Elizabeth	4/56.D & 4/56.E	ca. 1950	2-story side-gable twin dwelling	Not Eligible
63	607 A/B Arnett Street, Elizabeth	4/56.B & 4/56.C	ca. 1950	2-story side-gable twin dwelling	Not Eligible
64	605 Arnett Street, Elizabeth	4/56.A & 4/56	ca. 1950	2-story side-gable twin dwelling	Not Eligible
65	584-586 Summer Street, Elizabeth	4/1495 & 4/1495.A	ca. 1950	2-story side-gable twin dwelling	Not Eligible
66	442 Richmond Avenue, Elizabeth	4/455	ca. 1940	1-1/2 story gable front frame dwelling	Not Eligible
67	Halloran School, Elizabeth	4/1278	1950	2-story brick school	Not Eligible
68	445 Fern Place, Elizabeth	4/456	ca. 1930	2-story gable-front frame dwelling	Not Eligible
69	447 Fern Place, Elizabeth	4/457	ca. 1910	2-1/2 story gable-front frame dwelling	Not Eligible
70	663 Pulaski Street, Elizabeth	4/300	ca. 1910	2-story gable-front frame dwelling	Not Eligible
71	659 Pulaski Street, Elizabeth	4/299	ca. 1910	2-1/2 story gable-front frame dwelling	Not Eligible
72	655 Pulaski Street, Elizabeth	4/298	ca. 1920	2-1/2 story hipped roof frame dwelling	Not Eligible
73	653 Pulaski Street, Elizabeth	4/297	ca. 1920	1-1/2 story hipped roof frame dwelling	Not Eligible
74	501 Richmond Avenue, Elizabeth	4/1280.A	ca. 1940	1-1/2 story brick and frame gable-front dwelling	Not Eligible
75	641 Pulaski Street, Elizabeth	4/294.K	ca. 1940	DENIED ACCESS	Not Eligible
76	637 Pulaski Street, Elizabeth	4/294.J	ca. 1940	DENIED ACCESS	Not Eligible
92 (58A)	623 A/B Arnett Street, Elizabeth	4/57.E & 4/57.F	ca. 1950	2-story side-gable twin dwelling	Not Eligible
93 (58B)	619 A/B Arnett Street, Elizabeth	4/57.C & 4/57.D	ca. 1950	2-story side-gable twin dwelling	Not Eligible



- Legend**
- Area of Potential Effect
  - Surveyed Historic Resource
  - Surveyed Linear Historic Resource

FIGURE 12: Surveyed Historic Resources in the New Jersey and New York APEs

SOURCE: Berger Field Data 2007; Port Authority of New York and New Jersey 2002

New Jersey APE and Berger/PB JV's National Register eligibility recommendations. Figure 12 depicts the locations of the 50 newly identified historic resources in the New Jersey APE. NJHPO survey forms were completed for 47 of the 50 identified historic resources. The surveyor was denied permission to photograph or otherwise document three of the historic resources. Copies of the survey forms for the 47 historic resources are provided in Appendix BB.

### **4.3 Newly Inventoried Historic Resources in the New York APE**

Berger/PB JV identified 15 historic resources within the New York portion of the APE during the field investigation. Nearly all of these historic resources consist of late nineteenth- or early twentieth-century single dwellings with very poor architectural integrity. Two other historic resources, a pump house at 2949 Goethals Road North and a small yard office building at 2409 Forest Avenue, also display poor integrity.

After applying the National Register eligibility criteria to the 15 newly identified historic resources within the New York APE, none of the historic resources are recommended as meeting National Register significance and integrity criteria for listing in the National Register. Table 3 summarizes the results of the historic resources survey in the New York APE and Berger/PB JV's National Register eligibility recommendations. Figure 12 depicts the locations of the 15 newly identified historic resources in the New York APE. NYSOPRHP survey forms were completed for 13 of the 15 identified historic resources. The surveyor was denied permission to photograph or otherwise document two of the historic resources. Copies of the survey forms for the 13 historic resources are located in Appendix BC.

TABLE 3

## NEWLY INVENTORIED HISTORIC RESOURCES IN THE NEW YORK APE

FIG. 12 No.	NAME/ADDRESS	BLOCK/LOT	DATE BUILT	PRIMARY RESOURCE DESCRIPTION	RECOMMENDATION
77	17 Lilac Court, Staten Island	1707/5	ca. 1915	2-story gable-front frame dwelling	Not Eligible
78	11 Lilac Court, Staten Island	1707/38	ca. 1940	2-story side-gable frame dwelling	Not Eligible
79	881 Morrow Street, Staten Island	1384/1	ca. 1915	DENIED ACCESS	Not Eligible
80	885 Morrow Street, Staten Island	1384/3	ca. 1915	2-story ell-plan frame dwelling	Not Eligible
81	856 Morrow Street, Staten Island	1717/56	ca. 1940	2-story gable-front frame dwelling	Not Eligible
82	834 Morrow Street, Staten Island	1717/67	ca. 1920	2-story gable-front frame dwelling	Not Eligible
83	830 Morrow Street, Staten Island	1717/72	ca. 1920	2-story frame dwelling	Not Eligible
84	851 Morrow Street, Staten Island	1380/41	ca. 1915	2-story gable-front frame dwelling	Not Eligible
85	2437 Forest Avenue, Staten Island	1348/96	ca. 1915	DENIED ACCESS	Not Eligible
86	2431 Forest Avenue, Staten Island	1348/102	ca. 1915	2-1/2 story Four Square dwelling	Not Eligible
87	2409 Forest Avenue, Staten Island	1290/15	ca. 1931	1-story hipped-roof frame office	Not Eligible
88	15 Kinsey Place, Staten Island	1290/66	ca. 1915	1-story side-gable frame dwelling	Not Eligible
89	17 Kinsey Place, Staten Island	1290/64	ca. 1920	1-story gable-front frame dwelling	Not Eligible
90	19 Kinsey Place, Staten Island	1290/63	ca. 1920	1-1/2 story hipped-roof frame dwelling	Not Eligible
91	2949 Goethals Road North, Staten Island	1394/101	ca. 1940	1-story brick pump house	Not Eligible

## **5.0 CONCLUSION**

As part of the Goethals Bridge Replacement Environmental Impact Statement (GBR EIS) for the United States Coast Guard (USCG), The Louis Berger Group, Inc./Parsons Brinckerhoff Joint Venture (Berger/PB JV) undertook a historic resources survey in accordance with Section 106 of the National Historic Preservation Act of 1966, as amended. The survey identified National Register listed and eligible historic properties within the project's area of potential effect (APE), and documented and evaluated the National Register of eligibility of new historic resources identified within the project's APE. Field investigations and historic research for the survey were completed in July and August 2006. As a result of the architectural resources investigations, Berger/PB JV identified 93 resources meeting the National Register's 50-year age criteria within the project's APE for architectural investigations.

Twenty-eight of the 93 resources identified within the APE had been previously identified, documented, and evaluated for National Register eligibility. Six of these 28 resources were determined eligible for listing in the National Register via consensus determinations between the New Jersey Historic Preservation Office (NJHPO) and federal and state agencies. These six resources, all located in the New Jersey portion of the APE, are the Goethals Bridge, the Staten Island Railroad Historic District, the Perth Amboy and Elizabethport Branch of the Central Railroad of New Jersey (CNJ), the CNJ Bridge over the Elizabeth River, the South Front Street Bridge over the Elizabeth River, and the South First Street Bridge over the Elizabeth River. The remaining 22 previously identified resources were determined not eligible via consensus determination.

Berger/PB JV also identified 65 historic resources within the project APEs that had not previously been documented or evaluated for National Register eligibility. The 65 newly identified resources consisted of 50 resources within the New Jersey APE and 15 resources within the New York APE. Berger/PB JV documented 60 of the historic resources (the surveyor was denied permission to document five properties by their owners or residents) through narrative notes and photographs, and completed appropriate state historic preservation office architectural survey forms for each surveyed resource. Berger/PB JV then evaluated the significance and integrity of each property utilizing National Register eligibility criteria (36 CFR 60). As a result of these activities, Berger/PB JV recommends none of the 65 newly identified historic resources within the APE as meeting National Register eligibility criteria because they lack historical and/or architectural significance and have lost integrity.

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# APPENDIX BA

## SECTION 106 CONSULTATION DOCUMENTS/CORRESPONDENCES

1. 07/28/04 - SHPO Archaeology Meeting Briefing Package mailed to both NJHPO and NYSOPRHP.
2. 08/11/04 - Minutes of Archaeology Coordination Meeting with NYSOPRHP.
3. 08/17/04 - Email from Doug Mackey (NYSOPHRP) approving final minutes of the Archaeology Coordination Meeting of August 11<sup>th</sup>, 2004.
4. 08/18/04 - Email Mike Gregg (NJHPO) endorsing decisions made at the Archaeology Coordination Meeting of August 11<sup>th</sup>, 2004.
5. 03/14/05 - NYCLPC Archeology/Historic Environmental Review Form.
6. 03/21/05 - NYCLPC Archeology/Historic Environmental Review Form.
7. 04/14/05 - National Park Service-NHRP Letter.
8. 05/05/05 - Minutes of Coordination Meeting with NJHPO for Historical/Architectural Resources.
9. 06/17/05 - USCG's Project Initiation Letters for Section 106 Consultation with both NJHPO and NYSOPRHP.
10. 07/14/05 - NYSOPRHP Response Letter to USCG for the Initiation of Section 106 Consultation.
11. 07/25/05 - USCG Follow-Up Letter to NYSOPRHP's Letter of July 14<sup>th</sup>, 2005.
12. 10/31/05 - USCG Letter to NJHPO re: Proposed APE for Historical/Architectural Resources and Minutes of the Field Visit with NJHPO on October 17<sup>th</sup>, 2005.
13. 12/07/05 - NJHPO E-mail to USCG with NJHPO's Expanded APE for Historical/Architectural Resources.
14. 03/10/06 - USCG Response Letter to NJHPO with Revised/Final APE and Technical Memorandum on the Consideration of the APE for Historical/Architectural Resources.
15. 07/20/07 - NJHPO Concurrence on Revised/Final APE provided by USCG on March 10<sup>th</sup>, 2006.

**STATE HISTORIC  
PRESERVATION OFFICER  
MEETING  
BRIEFING PACKAGE**

***GOETHALS BRIDGE MODERNIZATION PROGRAM (GBMP)  
ENVIRONMENTAL IMPACT STATEMENT (EIS)***

***The Louis Berger Group, Inc./Parsons Brinckerhoff JV***

***August 11, 2004***



July 28, 2004

NYSOPRHP  
FSB  
Delaware Avenue  
Cohoes, N.Y. 12047

Att: Doug Mackey, (518) 237-8643

Department of Environmental Protection  
Historic Preservation Office  
4<sup>th</sup> Floor  
501 East State Street  
Trenton, N.J. 08625-0578

Att: Mike Gregg, (609) 633-2395

**Re: Goethals Bridge Modernization Program Environmental Impact Statement  
SHPO Coordination Meeting Briefing Package**

Dear Sir:

On behalf of the U.S. Coast Guard, and in cooperation with the Port Authority of New York and New Jersey (PANYNJ), The Louis Berger Group, Inc. / Parsons Brinckerhoff joint venture looks forward to meeting with you at the August 11, 2004 SHPO Coordination Meeting (10:00 AM, 115 Broadway, New York City, 5<sup>th</sup> Floor) to discuss the above-mentioned program. As requested, a briefing package, which details aspects of the program as well as proposed topics for discussion at the meeting, has been enclosed for review in advance of the meeting.

The U.S. Coast Guard has federal regulatory oversight of the Goethals Bridge Modernization Program (GBMP) due to its authority under the General Bridge Act of 1946 as amended. The PANYNJ, the bridge owner and program proponent, has proposed replacement of the Goethals Bridge, which links Elizabeth, NJ with northwestern Staten Island, NY to address the functional limitations of the 76-year old bridge structure.

The Coast Guard is the federal lead agency for the preparation of an Environmental Impact Statement (EIS) in accordance with the requirements of the National Environmental Policy Act (NEPA). In the near future, a Notice of Intent (NOI) to prepare a Draft EIS will be published in the *Federal Register*. In order to set study parameters and begin collecting baseline data, the consultant team is beginning a preliminary data collection effort. In this regard, we are requesting the cooperation and availability of the SHPO to consult with the Coast Guard and members of the consultant team in order to move the NEPA process forward as expeditiously as possible.

At the August 11 meeting, we would like to discuss cultural resource issues aimed to: 1) provide the agencies with information about the project and the proposed schedule for development of the EIS; 2) provide a list of cultural resource data sources to be used in developing the EIS for review and evaluation;



3) determine agency concerns and issues to be addressed under NEPA; 4) solicit relevant data that the agencies may possess; and 5) obtain guidance as to the likely permits or approvals that would be required.

If you have any questions, please contact either Mark Renna at (973) 678-1960, ext. 485 or Gerry Scharfenberger at ext. 770.

Very truly yours,

**The Louis Berger Group, Inc. / Parsons Brinckerhoff Joint Venture**

Kenneth J. Hess, P.P., AICP  
Project Manager  
Berger/PB JV

Distribution:

# **ENVIRONMENTAL IMPACT STATEMENT FOR THE GOETHALS BRIDGE MODERNIZATION PROGRAM**

## **SHPO COORDINATION MEETING AUGUST 11, 2004 AGENDA TOPICS**

### 1.0 Introductions

#### 1.1 Purpose of the Meeting

### 2.0 Project Overview

#### 2.1 Project Description

#### 2.2 NEPA Process and Schedule

#### 2.3 Seasonal Field Investigation Schedule

### 3.0 Cultural Resource Data Sources

#### 3.1 Data Compiled

#### 3.2 Additional Data Agencies May Possess

### 4.0 Data Collection Plan of Study

### 5.0 SHPO Agency NEPA Issues

### 6.0 Permit Guidance

### 7.0 Next Steps

#### 7.1 Agency Scoping – September

#### 7.2 Environmental Task Force – October

**ENVIRONMENTAL IMPACT STATEMENT  
FOR THE GOETHALS BRIDGE MODERNIZATION PROGRAM  
FEDERAL INTERAGENCY COORDINATION MEETING  
JULY 29, 2004  
BRIEFING PACKAGE**

## **1.0 Introduction**

The U.S. Coast Guard, as the Federal lead agency, and in cooperation with the Port Authority of New York and New Jersey (PANYNJ), intends to prepare and circulate a Draft Environmental Impact Statement (DEIS) for a proposed new bridge to replace the existing Goethals Bridge crossing the Arthur Kill and connecting Staten Island, New York and Elizabeth, New Jersey. This proposed action is designated as the PANYNJ's Goethals Bridge Modernization Program (GBMP). A Coast Guard bridge permit authorizing the location and plans for the project, which crosses navigable waters of the United States, is required before construction can begin.

The Goethals Bridge provides a direct connection between Staten Island, New York and Elizabeth, New Jersey (see Figure 1). It facilitates mobility between the two States as part of the Port Authority's Interstate Network, comprised of the George Washington Bridge, the Holland and Lincoln Tunnels and the three Staten Island Bridges (i.e., Goethals Bridge, Outerbridge Crossing and Bayonne Bridge). In addition, the Goethals Bridge serves as a primary route for traffic traveling along the Interstate 95 corridor between north and central New Jersey into Staten Island. The bridge is considered a primary path of travel within the Southern Corridor connecting Interstate 278 (the Staten Island Expressway) near Staten Island's north shore, with the New Jersey Turnpike (Interstate 95) and U.S. Routes 1 and 9 in New Jersey.

The project proposes to replace the existing Goethals Bridge, which has substandard geometrics and is experiencing escalating deterioration that has resulted in safety and reliability concerns. The design of a proposed new facility would reflect current traffic design standards, modern structural and seismic codes, national-security safeguards and technology enhancements. It would also add the operational flexibility to facilitate future transit-service opportunities.

Based on the information currently available, the Coast Guard has determined that an EIS would be the appropriate level of environmental documentation for assessing the potential impacts of the proposed project under Section 102(2)(C) of the National Environmental Policy Act (NEPA) of 1969, as amended.



**Goethals Bridge  
Modernization Program EIS  
REGIONAL LOCATION**

Figure 1

In addition to the no-build alternative (no-action), the selection of alternatives may include alternative alignments within the existing bridge corridor; alternative bridge designs; provision of high-occupancy vehicle or express bus lanes; intelligent vehicular highway system options; congestion pricing options; consideration of transit alternatives such as potential light rail, commuter rail, bus and/or ferry routes and services; as well as all other reasonable alternatives identified by the public.

Potentially significant issues to be evaluated include: impact on existing/future land use within the proposed project right-of-way; traffic patterns; threatened and endangered species, and critical habitat; historic and archeological resources; wetlands; water quality; noise; air quality; navigation; construction impacts; and cumulative impacts.

A formal interagency scoping meeting with federal, state and local agencies is proposed in September 2004. In addition, public scoping meetings in both Staten Island and Elizabeth are proposed in October 2004. The dates for the scoping meetings will be announced locally.

## 2.0 Bibliography of Data Sources

Data have been cataloged and assessed for use in developing the EIS environmental baseline conditions reflective of existing conditions within the GBMP study area. These data, along with proposed technical studies associated with the GBMP EIS data collection efforts, will be used to establish study area environmental conditions and serve as the basis for assessment of potential project impacts. The initial bibliography of data collected for this study is presented below.

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Sidney, J. C. Map of Staten Island or Richmond County. M. Dripps, New York. 1850.

U.S. Geological Survey (USGS) 7.5-min quadrangle maps: Perth Amboy, NJ-NY, 1956, photorevised 1981; Arthur Kill, NY-NJ, 1966, photorevised 1981; and Elizabeth, NJ-NY, 1967, photorevised 1981.

\_\_\_\_\_. Staten Island 15' Topographic Quadrangle. 1900/1909.

Walling, H. F. Map of Staten Island, Richmond County, New York. D. A. Fox, New York. 1859.

#### ▪ **PHOTOGRAPHS**

Aerial photographs taken by Robinson Aerial Surveys, Inc.

Current aerial photographs of the study area from the Port Authority of New York & New Jersey

Current topographic and planimetric maps of the study area from the Port Authority of New York & New Jersey

Historical aerials of the study area circa mid-1990's: NJDEP, Port Authority.

Historical aerial photographs of the study area, circa mid-1990's from Port Authority of New York & New Jersey and the New Jersey Department of Environmental Protection

# MEETING MINUTES

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## GOETHALS BRIDGE MODERNIZATION PROGRAM EIS

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**DATE:** August 17, 2004  
**TO:** Gary Kassof, Ernie Feemster (USCG)  
**FROM:** Gerry Scharfenberger/Mark Renna (Berger/PB JV)  
**SUBJECT:** Minutes of the NYOPRHP Coordination Meeting of August 11, 2004  
**CC:** Meeting Attendees: Douglas Mackey (NYSHPO), Gary Kassof, Jim Blackmore, Jay Shuffield, Camille Gonzalez, Gerry Scharfenberger, Mark Renna.  
Not in Attendance: Mike Gregg (NJHPO)

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### 1.0 Introductions

#### 1.1 Purpose of the Meeting

The Goethals Bridge Modernization Program (GBMP) was introduced as a new project distinct from the 1997 Staten Island Bridges (SIB) EIS. The project proponent is the PANYNJ and the USCG is the lead federal agency pursuant to NEPA. The USCG is directing the Consultant Team Joint Venture (JV). The purpose of this meeting was to address the issue of cultural resources, in particular archaeology to expedite fieldwork that is seasonally dependent in a timely manner. In addition, a discussion of the field investigation strategy was undertaken between the JV archaeologist and the New York State Office of Parks, Recreation and Historic Preservation (NYOPRHP) archaeologist to resolve any discrepancies in the JV field plan and allow for the input of the review agencies from both states prior to the commencement of fieldwork.

The New Jersey State Historic Preservation Office (NJHPO) did not send a representative, however, in a personal communication; Mr. Mike Gregg of NJHPO indicated that NJHPO would abide by direction provided by the NYOPRHP.

### 2.0 Project Overview

#### 2.1 Project Description

#### 2.2 NEPA Process and Schedule

#### 2.3 Seasonal Field Investigation Schedule

The JV described the GBMP as a proposal by the PANYNJ to replace the 76-year old Goethals Bridge. Scoping was described to begin in September and conclude in December. The scoping schedule necessitated the need to coordinate with the NYOPRHP so as to permit the initiation of field studies this summer and fall prior to the completion of scoping.

Mr. Mackey indicated that cultural resource issues regarding the existing Goethals Bridge structure would be an impact issue as the bridge is eligible for listing under the National Register of Historic Places. Mr. Mackey advised he would defer to the NYOPRHP lead in this regard, Ms. Beth Cummings, but outlined

# MEETING MINUTES

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## GOETHALS BRIDGE MODERNIZATION PROGRAM EIS

the issues to be addressed including consultations, impact assessment (likely to be assessed as adverse), and mitigation options including preservation of the structure or recordation of the bridge structure all in accordance with Section 106 of the Historic Preservation Act. Mr. Kassof indicated that if the bridge were to be decommissioned and not operational, the USCG would require that the bridge be removed and preservation would not be an option.

### **3.0 Cultural Resource Data Sources**

#### **3.1 Data Compiled**

#### **3.2 Additional Data Agencies May Possess**

The JV provided an overview of cultural resource data compiled for the study area. A large body of data exists from years past, most notably, the 1997 FEIS for the Staten Island Bridges Program, along with a number of related environmental and cultural resource studies. A general request for additional data from the NYOPRHP was made followed by an agreement with JV staff to examine the files of the NYSM and the NJHPO to review and collect information from any studies that were undertaken since the 1997 FEIS. In addition, the newly created online website of archaeological sites maintained by the NYOPRHP will be reviewed to create a predictive model for cultural resource sensitivity within the Area of Potential Effect (APE). Doug Mackey advised that a password was needed to enter the site and would assist if the JV did not have one.

Mr. Mackey provided copies of data from the immediate study area identifying known cultural resource locations and geotechnical soil boring data.

### **4.0 Data Collection Plan of Study**

The JV archaeologist outlined the Phase IB field investigation plan designed to determine the presence/absence of buried cultural resources within the current APE. This plan calls for the creation of a grid for shovel testing of all areas not covered by buildings, structures, or other impervious surfaces or in any of the two sites designated as Superfund sites. Shovel testing will occur at set intervals of 50-feet. Shovel testing will be avoided in the front yards of private residence, except in cases where the common area closest to the curb has been disturbed by utilities, road construction, etc. Georeferencing of historic maps over present site plans and the examination of geotech boring data may further narrow the areas eligible for investigation by virtue of excessive disturbance or filling in the past. Doug Mackey also recommended checking the files of the NYSM for evidence of burials. If there is the potential to encounter burials in a particular area, Doug Mackey suggested decreasing the shovel test interval to 25-feet. At this time, it was agreed that there is no need to examine soils under paved surfaces. However, Doug Mackey would want the JV to examine the areas under the paved surfaces if:

- There is fill that would have capped the original ground surface
- There is a defined alternative that calls for impacting that area

### **5.0 SHPO Agency NEPA Issues**

Doug Mackey advised that the JV should follow the Section 106 process to answer NEPA questions.

# MEETING MINUTES

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## GOETHALS BRIDGE MODERNIZATION PROGRAM EIS

### 6.0 Permit Guidance

The JV inquired of the agencies their advice regarding applicable permits. The JV proposed to collect data in the EIS at a level sufficient for permit application preparation. Again, Mr. Mackey advised that data collection sufficient to address Section 106 would suffice.

### 7.0 Next Steps

#### 7.1 Agency Scoping – September

#### 7.2 Environmental Task Force – October

The scoping meeting will be held on September 14, 2004. A scoping document will be distributed to all parties 3-4 weeks prior to the meeting.

### 8.0 General Notes

It was agreed that all correspondence, summaries, reports related to the project will be sent to both Doug Mackey of the NYOPRHP and Mike Gregg of the NJHPO for review.

# ATTENDANCE SHEET

## GOETHALS BRIDGE MODERNIZATION PROGRAM EIS

**MEETING PURPOSE:** State Historic Preservation Officer Coordination Meeting  
**DATE:** August 11, 2004  
**LOCATION:** Port Authority of New York & New Jersey Offices, 115 Broadway, NY, NY  
**TIME:** 10:00 am

NAME	AFFILIATION	PHONE	EMAIL
Gerry Scharfenberger	The Louis Berger Group	(973) 678-1960 X770	gsharf@louisberger.com
Mark Renna	The Louis Berger Group	(973) 678-1960 X485	mrenna@louisberger.com
Jay Shuffield	PANYNJ	(212) 435-4845	jshuffield@panynj.gov
CAMILLE GONZALEZ	URS / PANYNJ	(212) 435-5286	CGONZALEZ@panynj.gov
Douglas Mackey	SHPO	(518) 237-8643x3091	douglas.mackey@oprhp.state.ny.us
GARY KASSOFF	USCG	212 668 7021	gkassoff@batferry.uscg.mil
Jim Blackmore	PANYNJ	212 435-5290	jblackmo@panynj.gov

## Magron, Jean Philippe

---

**From:** Mackey, Douglas (PEB) [Douglas.Mackey@oprhp.state.ny.us]  
**Sent:** Tuesday, August 17, 2004 2:30 PM  
**To:** Renna, Mark; Mike.Gregg@dep.state.nj.us  
**Cc:** Jim Blackmore; Ernie Feemster; cgonzalez@panynj.gov; Marc Helman; Hess, Kenneth; Gary Kassof; Ed Lopez; Magron, Jean Philippe; Jeff Reidenauer; Judith Versenyi; Scharfenberger, Gerard; Cumming, Beth (PEB)  
**Subject:** RE: Goethals Bridge Modernization Program EIS SHPO Meeting Minutes

Mark,

Thanks for pulling this together. It looks accurate to me, however our tech reviewer for the project will be Beth Cumming (no s at the end) and her e-mail is Beth.Cumming@oprhp.state.ny.us and her extension is 3282.

Beth and Mike G. This is a fairly complete and accurate summary of the meeting. I did indicate the Project Archaeologist should contact Mike to work out testing strategies for the New Jersey side to be sure their concerns are met. If either of you have any questions, let me know.

Doug

Douglas Mackey

New York State Office of Parks, Recreation and Historic Preservation Peebles Island PO Box 189 Waterford, NY 12188  
(518) 237-8643 x 3291

Douglas.Mackey@oprhp.state.ny.us  
<mailto:Douglas.Mackey@oprhp.state.ny.us>

-----Original Message-----

From: Renna, Mark [mailto:mrenna@louisberger.com]  
Sent: Tuesday, August 17, 2004 12:35 PM  
To: Mike.Gregg@dep.state.nj.us; Mackey, Douglas (PEB)  
Cc: Jim Blackmore; Ernie Feemster; cgonzalez@panynj.gov; Marc Helman; Hess, Kenneth; Gary Kassof; Ed Lopez; Magron, Jean Philippe; Jeff Reidenauer; Judith Versenyi; Scharfenberger, Gerard  
Subject: Goethals Bridge Modernization Program EIS SHPO Meeting Minutes

Doug:

Attached please find minutes of our meeting of August 11, 2004. We appreciate your assistance and look forward to working with you on this project.

Mike: we know you were unable to attend, but indicated that NJ SHPO would concur with direction provided by NY. We look forward to working with NJ SHPO on this project and would welcome your comments and participation in the upcoming Agency Scoping Meeting scheduled for September 14, 2004 at 10:30 am at the US Coast Guard Offices in Battery Park, NYC. The NOI is also attached for your records.

Thanks, Mark

Mark Renna  
Vice President of Environmental Sciences The Louis Berger Group, Inc.  
100 Halsted Street  
East Orange, New Jersey 07018  
800/323-4098 ext 485  
973/678-1960 ext 485  
fax 973/672-4284

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## Magron, Jean Philippe

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**From:** Renna, Mark  
**Sent:** Wednesday, August 18, 2004 11:22 AM  
**To:** Bach, James; 'Jim Blackmore'; 'Ernie Feemster'; 'cgonzalez@panynj.gov'; 'Marc Helman'; Hess, Kenneth; 'Gary Kassof'; 'Ed Lopez'; Magron, Jean Philippe; 'Jeff Reidenauer'; 'Judith Versenyi'  
**Subject:** FW: Goethals Bridge archaeology meeting

NJSHPO seems to indicate concurrence with our field plan of study. A couple specific points of contact have been mentioned. I suggest we add them to our mailing list. Mark

-----Original Message-----

**From:** Mike Gregg [mailto:Mike.Gregg@dep.state.nj.us]  
**Sent:** Wednesday, August 18, 2004 11:07 AM  
**To:** Gscharf@louisberger.com; Renna, Mark; Douglas.Mackey@oprhp.state.ny.us  
**Subject:** Goethals Bridge archaeology meeting

Thank you all for adequately representing NJ's interests at the meeting last Wednesday, and thanks for the DEIS Notice of Intent from FR, meeting minutes, and meeting sign-up sheet. I have forwarded this information to our Deputy SHPO Dorothy Guzzo, head of our transportation unit Charles Scott, and bridge specialist Andrea Tingey.

Good Luck,

This transmission is neither privileged nor confidential. If the reader of this transmission is not the intended recipient, you are hereby notified that although you have received this document in error, you may review, disseminate, distribute or copy this transmission however you wish or may imagine. If you have received this transmission in error, no problemo! Please do not bother notifying me or anybody, and certainly do not be concerned with deleting, altering, forwarding, or flushing it.

Michael L. Gregg  
Historic Preservation Specialist  
Historic Preservation Office  
PO Box 404  
Trenton NJ 08625-0404  
phone 609 633 2395, fax 609 984 0578, Mike.Gregg@dep.state.nj.us  
<http://www.state.nj.us/dep/hpo/>

## ENVIRONMENTAL REVIEW

USCG /ER.R

03/08/05

PROJECT NUMBER

DATE RECEIVED

### PROJECT

GOETHALS BRDG MODERNIZ'TN:

- No architectural significance
- No archaeological significance
- Designated New York City Landmark or Within Designated Historic District
- Listed on National Register of Historic Places
- Appears to be eligible for National Register Listing ~~and/or New York City Landmark Designation~~
- May be archaeologically significant; requesting additional materials

### COMMENTS

The LPC is in receipt of: "Task G: Scoping Summary Report Version 2.0", 1/31/05; "Task I- Alternative Actions and Screening, Technical Memorandum, Preliminary Alternatives", 2/05; and "Task I-Alternative Actions and Screening, Technical Memorandum: Alternatives Screening Evaluation Methodology" dated 2/05. The documents are acceptable for architectural properties.

cc: SHPO, *Libberger*

*Guia Santucci*  
SIGNATURE

03/14/05

DATE

## ENVIRONMENTAL REVIEW

USCG/ER.R

03/08/05

PROJECT NUMBER

DATE RECEIVED

### PROJECT

GOETHALS BRDG MODERNIZ'TN:

- No architectural significance
- No archaeological significance
- Designated New York City Landmark or Within Designated Historic District
- Listed on National Register of Historic Places
- Appears to be eligible for National Register Listing and/or New York City Landmark Designation
- May be archaeologically significant; requesting additional materials

### COMMENTS

The LPC is in receipt of: "Task G: Scoping Summary Report Version 2.0," 1/31/05, "Task 1- Alternative Actions and Screening, Technical Memorandum, Preliminary Alternatives," 2/05, and "Task 1- Alternative Actions and Screening, Technical Memorandum" Alternatives Screening Evaluation Methodology" dated 2/05. The work being undertaken to identify the potential impact of the proposed project upon potentially significant archaeological resources is appropriate.

cc: SHPO, Louis Berger

  
\_\_\_\_\_  
SIGNATURE

03/21/05

DATE



# United States Department of the Interior

NATIONAL PARK SERVICE

1849 C Street, N.W.

Washington, D.C. 20240

IN REPLY REFER TO:

April 14, 2005

2280

Dear Sarah Moss,

Thank you for your inquiry. According to our records, Goethals Bridge, NY/NJ is not listed in the National Register of Historic Places.

Thank you for your interest in the preservation programs of the National Park Service.

Sincerely,

Edson H. Beall

Historian

National Register of Historic Places

Phone: (202) 354-2255

Web: <http://www.cr.nps.gov/nr>

E-mail: [Edson\\_Beall@nps.gov](mailto:Edson_Beall@nps.gov)

EXPERIENCE YOUR AMERICA

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# MEETING MINUTES

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## GOETHALS BRIDGE REPLACEMENT EIS

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**DATE:** May 17, 2005  
Andrea Tingey, Michelle Hughes (NJSHPO)  
**To:** Gary Kassof, Ernie Feemster (USCG)  
Coleen Hopson, Gary Mason, Camille Gonzalez, Rosalie Siegel (PANYNJ)  
Judith Versenyi, Esther Schwalb (Berger/); Barbara Thayer, Sara Moss (BTA)  
**FROM:** Esther Schwalb, Sara Moss  
**SUBJECT:** Minutes of May 5, 2005, Meeting with NJSHPO  
**CC:** James Warren (NYSHPO), Jim Blackmore, Ed Lopez, Paul Crist, Phil Dinh, Lou Venech, Ter Benczik, Joann Papageorgis, Steve Coleman

---

**DATE/LOCATION:** Thursday, May 5, 2005; 1:30 – 3:00 PM  
USCG office, 3<sup>rd</sup> fl. Conference Room,  
Battery Building, One South Street, NY

**ATTENDEES:** Attendance sheet attached.

**PURPOSE OF MEETING:** Agenda attached.

**ACTION ITEMS:**

Item #	Description	Responsibility (follow up/action)
1	Send Section 106 initiation letter to NJSHPO.	USCG
2	Begin work on Alternatives Analysis Report.	Berger/PB (BTA)
3	Send copy of past Alternative Analysis Report considered good examples of such documentation to USCG.	NJSHPO
4	Speak with Jim Warren, NYSHPO to determine whether to hold meeting, or submit Section 106 initiation letter first.	USCG
5	Determine if NYSHPO requires a report comparable to the NJSHPO Alternatives Analysis Report.	Berger/PB (BTA)
6	Email link to existing Goethals Bridge HAER photography to meeting attendees.	BTA
7	Review Historic Resource Inventory prepared for SIBP EIS and determine whether suitable for GBR EIS documentation and consultation purposes.	USCG Berger/PB, BTA

# MEETING MINUTES

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## GOETHALS BRIDGE REPLACEMENT EIS

### 1. Project Background:

- a. USCG welcomed NJSHPO and other attendees, and defined purpose of meeting was to ask NJSHPO for input.
- b. Berger/PB provided an overview of the defined project purpose and need, and the related project goals, and noted that documents provided to NJSHPO (Draft Scoping Document, Scoping Summary Report, Technical Memoranda on Preliminary Alternatives and Alternatives Screening Methodology) contain more detail on topics to be discussed at the meeting.
- c. Berger/PB noted the key differences of the current proposed project from the 1997 SIBP EIS as follows:
  - i. Physical deterioration of the existing bridge has accelerated in the past 10 years, beyond what was anticipated when previous EIS was prepared; current major repair of bridge will have to be followed by full deck replacement in next 7 to 10 years, with subsequent significant repairs and rehabilitation of superstructure and substructure elements required every 20 to 25 years after that.
  - ii. Post 9/11 security concerns - among measures recommended to increase security at bridges, per an FHWA Blue Ribbon Panel, is creating standoff distances from primary structural components; as existing protective dolphin is already in the Arthur Kill's navigation channel, additional in-water protection would likely encroach further on navigation in the waterway.
  - iii. E-Z Pass system has been introduced at the Goethals and in the regional network since the SIBP EIS.
  - iv. New transit services have been introduced in area served by the Staten Island bridges, including Hudson-Bergen LRT, express bus lanes currently under construction on the Staten Island Expressway, which connects to Goethals Bridge approach on the east
  - v. Howland Hook Marine Terminal (operated by New York Container Terminal) re-opened in late 1990's and is expanding, resulting in increased truck traffic to/from the facility and across the Goethals Bridge; with the closure of the Military Ocean Terminal at Bayonne (MOTBY), Howland Hook is now the military facility for the Port of NY & NJ. The former MOTBY, now The Peninsula at Bayonne Harbor, has acreage set aside for port use, yet to be developed.
- d. NJSHPO responded that additional information would be required concerning project purpose and need, and potential solutions, including:
  - i. Security measures: NJSHPO has looked at non-structural security enhancements at other bridges (e.g., cameras and signage) and would like more information on types of security methods under consideration for Goethals.
  - ii. E-Z Pass technology – NJSHPO will need more specific explanation of how E-Z Pass affects bridge traffic and what potential solutions are.
  - iii. Structural integrity - NJSHPO will want to see a bridge inspection report, AADT and other traffic data as further explanation of problem.

# MEETING MINUTES

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## GOETHALS BRIDGE REPLACEMENT EIS

- iv. Functional obsolescence – NJSHPO will want to see details on problem related to sub-standard alignment/approach and what potential solutions are.
  - e. Non-historic factors considered in NJSHPO’s decision-making
    - i. NJSHPO will consider stated need for wider lanes and standard shoulder width; given truck traffic, need may be compelling. NJSHPO considers AASHTO standards, but finds them flexible.
    - ii. Navigation in Arthur Kill – NJSHPO view is that maintenance of existing navigation channel is typical action, while improving horizontal clearance is not. USCG noted that changing marine traffic would benefit from wider navigation channel.
2. **Preliminary Alternatives:**
- a. Review of Preliminary Alternatives - Berger/PB provided a summary of the preliminary alternatives defined, based on the project purpose and need, and goals.
    - i. In response to NJSHPO question regarding whether reactivation of the Staten Island Railway was investigated, Berger/PB stated that it had been considered, but that dispersed origins and destinations in Goethals Bridge service area would likely be better served by transit that is not fixed-rail. Bus rapid transit and ferry system preliminary alternatives have been defined.
    - ii. Considerations related to potential Goethals Bridge rehabilitation
      - 1. NJSHPO noted that additional information is required before can make determination regarding whether rehabilitation of existing bridge should be dismissed and demolition considered.
      - 2. PANYNJ noted that if the Goethals Bridge were to be “twinning,” the number of lanes would be reduced from four lanes to three on the existing bridge, without an emergency shoulder.
    - iii. Proposed Project –
      - 1. NJSHPO cautioned against project “creep,” meaning expansion of transportation infrastructure improvements beyond what initially proposed. USCG responded that project expansion is not anticipated, but that physical mitigation of any significant impacts may be required.
      - 2. NJSHPO noted City of Elizabeth concerns regarding increased traffic and inability of local roads to handle it. Berger/PB responded that USCG has written to City of Elizabeth, and invited City to have representatives on the study’s Technical Advisory Committee, Environmental Task Force, and Stakeholder Committee, for ongoing opportunity for input to the EIS process.
      - 3. PANYNJ noted that, at the behest of the Mayor of Elizabeth, they met with representatives from the Cities of Elizabeth and Linden, Union County, NJDOT and NJ Turnpike Authority twice since the DEIS public scoping process, and expect to continue to meet over the course of the project, to address some of their concerns that are not related to the proposed project.

# MEETING MINUTES

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## GOETHALS BRIDGE REPLACEMENT EIS

- b. Alternatives Analysis Report
    - i. Alternatives Analysis Report (NJSHPO previously provided outline to Berger/PB) needs to be completed before alternatives are eliminated. If USCG intends to conduct NEPA and Section 106 process together, Alternatives Analysis Report needs to be prepared now. NJSHPO offered to provide examples of effective Alternatives Analysis Reports to the Study Team. NJSHPO will review bridge inspection reports, traffic data, justification of substandard alignment/approach claim, cost, etc.
    - ii. Guidelines for architectural and archaeological surveys are available on NJSHPO website.
    - iii. NJSHPO noted their view that a 77-year-old bridge is a “new bridge.”
    - iv. NJSHPO stated that the character-defining features of the bridge that make it notable for designation under “Criterion C: construction methods” are no different than if the bridge had been noted for its design and type.
  - c. Area of Potential Effect (APE)
    - i. The APE may need to be larger than in the SIBP FEIS, since the APE is based on the potential direct and indirect effects (such as visibility) of the bridge alternatives on other historic resources identified.
    - ii. NJSHPO mentioned new NJDEP stormwater regulations, which have changed, should be considered in archaeology study.
3. **Possible Mitigation options if replacement bridge/demolition of existing bridge is identified as Preferred Alternative:**
- a. Magnitude of mitigation will depend on the number of historic resources affected, and NJSHPO would want to coordinate with NYSHPO regarding necessary mitigation.
  - b. Design of Replacement Bridge
    - i. NJSHPO suggested that if the Goethals Bridge is replaced, the design of a new bridge (structure type) could affect the size of the APE.
    - ii. In response to NJSHPO question about what type of bridge is under consideration, Berger/PB responded that bridge design has not yet been considered this early in process.
    - iii. NJSHPO suggested that feasible bridge design types be considered, at “thumbnail sketch” level, including worst-case design scenario.
    - iv. NJSHPO would be looking for a ‘signature bridge’ as replacement.
  - c. Documentation of Goethals Bridge, should it be demolished:
    - i. Need photo documentation of existing bridge – to be determined whether formal HAER documentation would be required. *Note: Some HAER documentation (photographs from October 1991) is available online.*
    - ii. Curation & archiving that is available to public through historic societies, libraries.
    - iii. Creative ways of reaching public through education, e.g. school lesson plans, film documentary.

# MEETING MINUTES

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## GOETHALS BRIDGE REPLACEMENT EIS

- iv. Website showing history of bridge (in consultation with interested parties) and bridge demolition/construction.

#### 4. **Initiation of Section 106 Consultation:**

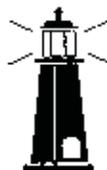
- a. Formal Section 106 consultation initiation letter from lead agency (USCG) (§803 of Section 106 regulations) should be sent to NJ (and NY) SHPO. Letter will include project description, purpose & need, definition of area of potential effect for all alternatives under study, public involvement plan, and list of consulting parties (which may differ by State).
- b. Adequacy of historic resource inventory conducted for 1997 SIBP EIS
  - i. NJ Policy is that everything 50 yrs old must be inventoried; information has a 10-year life span. Data from the 1995 DEIS/1997 FEIS is beyond this term, but the data may be re-certified if the USCG deems the previous inventory of sufficient quality and thoroughness. As Guidelines for Architectural Survey changed last year, the 1995 inventory needs to be re-evaluated. NJSHPO offered to accompany Study staff for field visit to confirm validity of previous inventory and identify any new resources.
- c. Goethals Bridge's National Register status – Bridge is still eligible for listing.
- d. Project review process
  - i. NJSHPO requested that information be sent to her office in substantial amounts to facilitate efficiency of her reviews.
  - ii. Study Team should copy NJSHPO when corresponding with NYSHPO.
  - iii. NJSHPO usually responds within 30 days from when material is received.
  - iv. NJSHPO noted that she will coordinate with NYSHPO.



COMMANDER (OBR)  
 FIRST COAST GUARD DISTRICT  
 BRIDGE BRANCH  
 ONE SOUTH STREET  
 BATTERY PARK BUILDING  
 NEW YORK, NY 10004-1466  
 TEL: (212) 668-7165  
 FAX: (212) 668-7967

DATE: 5 MAY 2005  
 LOCATION: 1 SOUTH STREET  
 GOETHALS BRIDGE REPLACEMENT PROJECT  
 COORDINATION WITH NJSHPO  
 TIME: 1330

NAME	AGENCY	TELEPHONE	E-mail
Judith Versenji	Berger/PB	212-465-5239	versenji@pbworld.com
Sara Moss	BTA (Berger/PB)	212-564-2750	smoss@btanyc.com
Francesca Thayer	F Thayer Assoc	212-564-2750	thayer@btanyc.com
Michelle Thayer	NJDEP-HPO	609-984-6018	Michelle.Thayer@dep.state.nj.us
Andrea Tingey	NJDEP-HPO	609-984-0539	Andrea.Tingey@dep.state.nj.us
GARY KASSOFF	USCG Bridge	212-668-7072	gkassoff@battery.uscg.mil
ERNE FEEMSTER	U.S. Coast Guard	(212) 668-7994	efeemster@battery.uscg.mil
Rosalee Simpson	PANY/NJ	(212) 435-4401	rsimpson@panynj.gov
Chopson	PANY/NJ	212-435-5520	chopson@panynj.gov
CAMILLE GONZALEZ	PANYNS/URS	212-435-5286	cgonzalez@panynj.gov
ESTHER SCHWALB	Berger/PB	212-465-5240	schwalb@pbworld.com
GARY KASSOFF	PANY/NJ	973-992-4117	gkassoff@panynj.gov



U.S. Department of  
Homeland Security

United States  
Coast Guard



Commander  
First Coast Guard District

One South Street  
Battery Building  
New York, NY 10004

Staff Symbol: obr  
Phone: 212 668-7165  
Fax: 212 668-7967

June 17, 2005

Ms. Andrea Tingey  
Architectural Historian  
New Jersey Department of Environmental Protection  
Historic Preservation Office  
P.O. Box 404  
Trenton, NJ 08625-0404

**Re: Initiation of Section 106 Consultation for the Goethals Bridge Replacement EIS**

Dear Ms. Tingey:

This letter is to formally initiate the Section 106 consultation process (pursuant to the National Historic Preservation Act of 1966) for the proposed Goethals Bridge Replacement (GBR) project, for which the Port Authority of New York and New Jersey (PANYNJ) is the project sponsor. The United States Coast Guard (USCG), federal lead agency for preparation of an environmental impact statement (EIS), in accordance with the National Environmental Policy Act (NEPA) of 1969, as amended, requests your consultation in the Section 106 review.

The USCG has regulatory oversight of the bridge project due to its authority under the General Bridge Act of 1946, as amended. As such, the project is subject to Section 106 of the National Historic Preservation Act and associated implementing regulations found at Title 36 CFR 800, which mandates review of a federal undertaking's effects on historic resources.

The USCG authorizes the Louis Berger Group, Inc./Parsons Brinckerhoff, Inc. Joint Venture, the environmental consultant team assisting the USCG with preparation of the GBR EIS, to prepare information, analyses, and recommendations supporting this effort.

Attached is the following information:

- Project Description, Purpose and Need for the Proposed Project, and Project Goals;
- Definition of Area of Potential Effect;
- List of Proposed Consulting Parties; and
- Public Participation Plan.

We seek your concurrence on the APE and the consulting parties.

Initiation of Section 106 Consultation for GBR EIS

Thank you for your assistance in this undertaking; we look forward to working with you further. We will provide you with additional project-related information, following your outline for a Historic Bridge Alternatives Analysis Report, in coming weeks.

In the meantime, please call me, at 212-668-7021, or Sara Moss, B. Thayer Associates (a member of the Berger/PB team), at (212) 564-2750, if you have any comments or questions concerning the enclosed information.

Sincerely,  
  
Gary Kassof  
Bridge Program Manager  
First Coast Guard District  
By direction of the District Commander

Enclosures:

Copy: Michelle Hughes (NJSHPO); James Warren (NYSHPO); Ernie Feemster (USCG); J. Blackmore, Coleen Hopson (PANYNJ); Ken Hess, Judith Versenyi, Esther Schwalb (Berger/PB); Sara Moss (BTA)

U.S. Department of  
Homeland Security

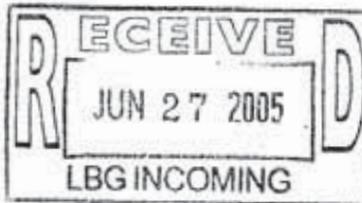
United States  
Coast Guard



Commander  
First Coast Guard District

One South Street  
Battery Building  
New York, NY 10004

Staff Symbol: obr  
Phone: 212 668-7165  
Fax: 212 668-7967



June 17, 2005

Mr. James Warren  
New York State Office of Parks, Recreation  
and Historic Preservation  
P.O. Box 189  
Peebles Island  
Waterford, NY 12188

**Re: Initiation of Section 106 Consultation for the Goethals Bridge Replacement EIS**

Dear Mr. Warren:

This letter is to formally initiate the Section 106 consultation process (pursuant to the National Historic Preservation Act of 1966) for the proposed Goethals Bridge Replacement (GBR) project, for which the Port Authority of New York and New Jersey (PANYNJ) is the project sponsor. The United States Coast Guard (USCG), federal lead agency for preparation of an environmental impact statement (EIS), in accordance with the National Environmental Policy Act (NEPA) of 1969, as amended, requests your consultation in the Section 106 review.

The USCG has regulatory oversight of the bridge project due to its authority under the General Bridge Act of 1946, as amended. As such, the project is subject to Section 106 of the National Historic Preservation Act and associated implementing regulations found at Title 36 CFR 800, which mandates review of a federal undertaking's effects on historic resources.

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- Definition of Area of Potential Effect;
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- Public Participation Plan.

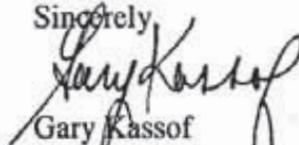
We seek your concurrence on the APE and the consulting parties.

Initiation of Section 106 Consultation for GBR EIS

Thank you for your assistance in this undertaking; we look forward to working with you further. We will provide you with additional project-related information, and seek your guidance in relation to NYSHPO's preference regarding the necessary documentation, in coming weeks.

In the meantime, please call me, at 212-668-7021, or Sara Moss, B. Thayer Associates (a member of the Berger/PB team), at (212) 564-2750, if you have any comments or questions concerning the enclosed information.

Sincerely,



Gary Kassof  
Bridge Program Manager  
First Coast Guard District  
By direction of the District Commander

Enclosures:

Copy: Andrea Tingey (NJSHPO); Michelle Hughes (NJSHPO); Ernie Feemster (USCG); J. Blackmore, Coleen Hopson (PANYNJ); Ken Hess, Judith Versenyi, Esther Schwalb (Berger/PB); Sara Moss (BTA)

**Request to Initiate Section 106 Process:**

**Project Description**

The PANYNJ has proposed construction of a new bridge to replace the existing Goethals Bridge, which provides a direct connection over the Arthur Kill between Staten Island, New York, and Elizabeth, New Jersey. It facilitates mobility between the two states as part of the PANYNJ's interstate transportation network, comprised of the George Washington Bridge, the Holland and Lincoln Tunnels, and the three Staten Island Bridges (the other two being the Outerbridge Crossing and the Bayonne Bridge). The Goethals Bridge is considered a primary path of travel within the Southern Corridor, connecting Interstate 278 (the Staten Island Expressway) near Staten Island's north shore with the New Jersey Turnpike (Interstate 95) and U.S. Routes 1 and 9 in New Jersey.

The PANYNJ notified the USCG by letter dated June 3, 2004, of its intent to submit a formal application for a Bridge Permit, under the General Bridge Act of 1946. A Bridge Permit is required before construction could begin, since the proposed replacement bridge would cross navigable waters of the United States. The USCG assumed the role of the Federal lead agency for preparation and issuance of an EIS, pursuant to NEPA. The EIS will examine the potential social, economic, and environmental impacts of reasonable and feasible alternatives for replacement of the Goethals Bridge. The principal elements of the Goethals Bridge Replacement EIS process include definition and analysis of alternatives, environmental documentation, and public outreach and interagency coordination.

**Purpose and Need**

The Staten Island Bridges Program (SIBP) Modernization and Capacity Enhancement Final EIS, published in 1997, included environmental, historic, and visual resource analyses for the Goethals Bridge corridor. The purpose and need for the current, proposed GBR project is different from and broader than the purpose and need articulated for the previous SIBP study, as it takes into account conditions that have changed since 1997, including the bridge's deteriorating structural integrity and escalating maintenance requirements; emergence of E-Z Pass use at the bridge (and consequent changes in traffic conditions on the bridge and in its environs); post-9/11 security needs at critical links, such as the Goethals Bridge, in the region's transportation network; reactivation and expansion of the area's port facilities, notably the New York Container Terminal at Howland Hook, and consequent increases in truck traffic; and other transportation projects in the bridge's vicinity and in the region.

The existing Goethals Bridge has substandard 10-foot-wide lanes, no emergency shoulders, and escalating repair and maintenance costs. The functional and physical obsolescence of the 77-year-old bridge impedes efforts to: improve safety and reliability on this interstate crossing; adequately accommodate modern vehicles and trucks, as the bridge was designed for vehicles that were significantly smaller than those in use today; and maintain efficient traffic operations, particularly during peak periods of travel, and improve incident response.

## Initiation of Section 106 Consultation for GBR EIS

The design of the proposed new facility would reflect current traffic design standards, modern structural and seismic codes, national-security safeguards, and technology enhancements. A proposed new crossing would also incorporate operational flexibility, which is precluded by the existing span, to facilitate future transit-service opportunities. By ensuring the ability to meet current and future interstate travel demand, the proposed bridge replacement would support long-term economic growth and improved mobility for the local communities that are dependent on the crossing, as well as enhance the overall performance, flexibility, and reliability of the transportation network serving the greater New York/New Jersey metropolitan area.

### **Project Goals**

Project goals have been defined on the basis of the stated purpose and need for the proposed project, and reviewed through the Draft EIS scoping process. The project goals, in turn, serve as the basis for: 1) identifying potential project alternatives; and 2) defining criteria and related performance measures that will be used to select reasonable and feasible alternatives that may best satisfy the project goals, address the project purpose and need, and, therefore, warrant detailed evaluation in the EIS.

Based on the purpose and need for the Goethals Bridge Replacement project, the following project goals have been defined:

- Address the functional obsolescence of the existing Goethals Bridge.
- Address structural integrity issues associated with the aging bridge.
- Reduce roadway congestion and delays and enhance mobility on the Goethals Bridge.
- Improve the flow of goods to and from Staten Island and New Jersey and in the New York/New Jersey region.
- Correct the inability of the existing bridge to physically accommodate transit services and other single-occupant-vehicle commuting alternatives.
- Restore and enhance pedestrian access and provide for bicycle access.
- Implement measures to improve bridge structural security.
- Minimize environmental consequences of the improvement.

### **Definition of Area of Potential Effect**

In the 1997 SIBP FEIS, the Area of Potential Effect (APE) was defined as one-half mile in all directions from the Goethals Bridge corridor. The APE for the GBR EIS has been initially defined in the same fashion, as it is anticipated that project alternatives will be within the Goethals Bridge corridor and assumed to be designed within the same overall envelope as the alternatives considered in the SIBP FEIS. The APE comprises the combined primary and secondary study areas defined during the GBR DEIS scoping process, and as shown on the attached maps. Once the alternatives are defined, the APE will be further refined, as necessary, in consultation with the New Jersey and New York State Historic Preservation Offices (SHPO)

## Initiation of Section 106 Consultation for GBR EIS

to ascertain its sufficiency for determination of potential direct and indirect effects on identified historic resources when the project alternatives have been identified.

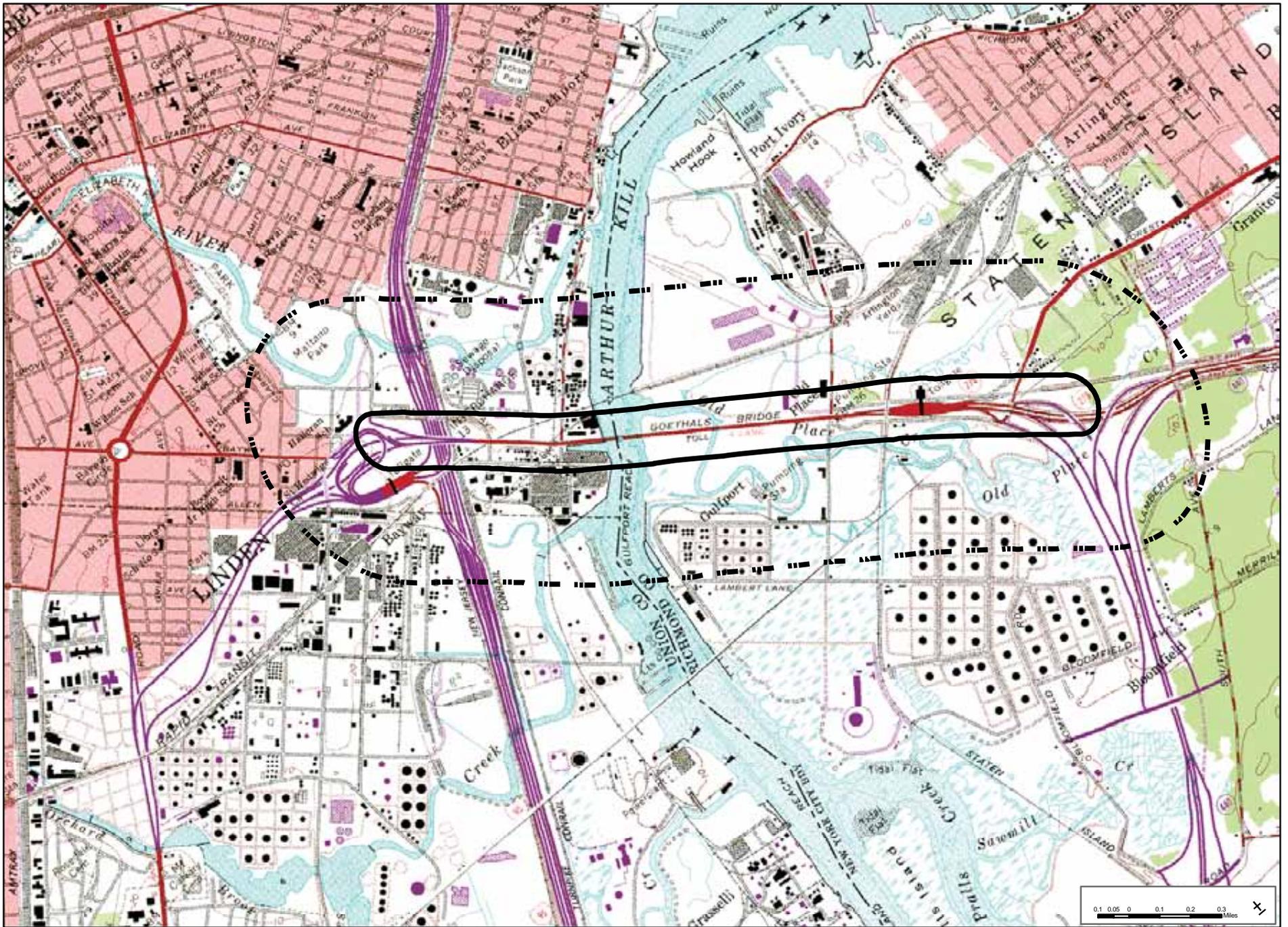
### **Public Participation Plan**

The USCG has developed and implemented a public participation program that will continue throughout the EIS process. The program's purpose is to inform, educate, and directly engage all those with an interest in the GBR EIS. The Public Participation Plan, which conforms to and satisfies the public participation requirements of NEPA, is attached.

### **List of Consulting Parties**

The following consulting parties may have an interest in this project:

- New Jersey State Historic Preservation Office
- Union County Division of Cultural and Heritage Affairs
- Historical Society of Elizabeth
- New York State Historic Preservation Office
- New York City Landmarks Preservation Commission
- New York City Economic Development Corporation
- Preservation League of Staten Island



- Primary Study Area
- Secondary Study Area / Area of Potential Effect

*Area of Potential Effect*

# PLAN OF STUDY

## **Final Public Participation Plan** Addendum to Version 2.0, dated May 24, 2004

### ***GOETHALS BRIDGE MODERNIZATION PROGRAM (GBMP)*** ***ENVIRONMENTAL IMPACT STATEMENT (EIS)***

*The Louis Berger Group, Inc./Parsons Brinckerhoff JV*

*July 23, 2004*



# PLAN OF STUDY

## **TASK D - PUBLIC OUTREACH AND STAKEHOLDER PARTICIPATION**

### **APPROACH**

Conduct Public Participation Program:

Design and conduct a public and community participation program throughout the EIS process, that seeks to inform, educate, and directly engage all those with an interest in the Goethals Bridge Modernization Program. The Public Participation Program will conform to and satisfy the public participation requirements of NEPA.

- Develop and implement, in a manner consistent with NEPA, a public participation program which draws on multi-media approaches, including, but not limited to: scheduling stakeholders meetings and public open houses; preparing materials, handouts, periodic newsletters and displays for ongoing public participation; and developing and maintaining a project website.
- Maintain a database of all interested persons and organizations. The database will include all stakeholders, and will be updated as needed.

### **WORK PLAN**

The principal activities for public participation and agency coordination and consultation will be detailed in the Public Participation Program. Outreach activities, which may be refined following consultation with the USCG and the Port Authority, follows.

- The following activities are proposed to be conducted prior to and/or coincident with Tasks B - Field Verification/Inspection; E – Applicable Regulatory Initiatives, Public Law, Permits, and Other Approvals; F - Purpose and Need; G - Public Scoping; H - Identify Environmental Criteria; and I - Alternative Actions and Screening.
  - Prepare Draft Public Participation Program;
  - Develop initial database (i.e., mailing list) of interested persons/organizations, to be maintained, updated, and supplemented throughout course of the GBMP EIS, as warranted;
  - Prepare first newsletter to introduce the GBMP EIS and notify the public of upcoming public scoping meetings;
  - Create issues log for subsequent recording of all public comments and GBMP EIS disposition of comments;
  - Prepare press releases and announcements for public notification of public scoping meetings;
  - Create Technical Advisory Committee (TAC) and Environmental Task Force (ETF), in coordination with USCG and the Port Authority pertaining to agencies/parties on each task force; and
  - Develop GBMP EIS website
  - Initiate development of Stakeholder Committee.

- The following activities are proposed to be conducted coincident with Tasks E - Applicable Regulatory Initiatives, Public Law, Permits, and Other Approvals; I - Alternative Actions and Screening; J - Evaluation of Design Options/Alternatives; K - Existing Conditions; L - Environmental Consequences; and M - Prepare Preliminary DEIS.
  - Revise Public Participation Program, if and as necessary, based on scoping and other public input;
  - Conduct Stakeholder Committee, TAC, and ETF meetings;
  - Second newsletter, focused on the alternatives screening activities;
  - One round of public open houses (one each in Elizabeth and Staten Island for each round);
  - Draft periodic press releases about the GBMP EIS status and findings, and to announce public open houses;
  - Update website, maintain database/mailling list, maintain issues log; and
  - Conduct other targeted outreach, as project issues and public interest warrant.
  
- The following activities are proposed to be conducted coincident with Tasks E - Applicable Regulatory Initiatives, Public Law, Permits, and Other Approvals, F - (refinement of) Purpose and Need, L - Environmental Consequences, M - Prepare Preliminary DEIS, N - Prepare DEIS, O - Facilitate All Public Hearings, and P - Prepare Final EIS.
  - Hold Stakeholder, TAC, and ETF meetings;
  - Prepare third and fourth newsletters, timed with completion of the DEIS and FEIS, respectively;
  - Second round of public open houses;
  - Draft periodic press releases about GBMP EIS status, findings, conclusions and to announce public open houses;
  - Update website, maintain database/mailling list, maintain issues log;
  - Conduct other targeted outreach, as project issues and public interest warrant, and;
  - Hold public hearings to gather comments on the DEIS; and
  - Prepare Technical Memorandum documenting the GBMP EIS public participation program.
  
- Coordinate Interagency Services

In furtherance of the NEPA EIS process, establish and coordinate, subject to the USCG concurrence, the following:

- An Inter-Agency Technical Advisory Committee (TAC) including PANYNJ, NJDOT, NYSDOT, NYCDOT, NJ Turnpike Authority, NJ Transit, MTA, the North Jersey Transportation Planning Authority, the New York Metropolitan Transportation Council, and other agencies as required.
- An Environmental Task Force (ETF).
- Assist in the preparation of presentation materials, evaluate the comments received, recommend courses of action to address the comments, and prepare draft and final minutes of all interagency meetings.
- If requested by the USCG or the Port Authority, access to and review of all procedures and underlying data used in developing submitted sections of the EIS will be provided, including, but not limited to, field reports, subcontractor reports, and interviews with concerned private and public parties, whether or not such information may be contained in the draft or final EIS.
- Notify the agencies of any substantive meetings that are scheduled and of their purpose and provide an opportunity for other parties to attend, if requested by the agencies.

## **DELIVERABLES**

- Conduct Public Participation Program

- A draft Technical Memorandum, outlining a “Public Participation Program.” Incorporate work product comments as directed and resubmit as Final. A Final draft is presented below.
  - Monthly summaries of public participation efforts and outcomes. Incorporate work product comments as directed and resubmit drafts as Final.
  - A database of the interested persons and organizations participating in the EIS process.
- Coordinate Interagency Services
    - A summary of all matters relating to the EIS discussed in any meetings or communications between the Berger/PB JV and inter-agencies will be included in each formal monthly report submitted to the USCG and the Port Authority.

## **TASK G - PUBLIC SCOPING**

### **APPROACH**

The USCG anticipates an early and open process for determining the scope of issues to be addressed in the Draft EIS and for identifying the significant issues related to this project, including the range of actions, alternatives and impacts to be considered.

### **WORK PLAN**

Develop, publish and distribute the notice(s) of meeting(s); organize the meeting location and facilities; make provisions for hearing officers and stenographers, if required; present the proposed; develop draft and final minutes of the meetings; and make recommendations for addressing issues raised during the meetings. All scoping meetings will be conducted in compliance with the requirements of NEPA. In support of the above:

- Develop a draft scoping package outline that includes meetings with the involved agencies and the public.
- After approval of the scoping package outline, develop scoping presentation materials necessary to solicit input from interested agencies, organizations, and individuals. These materials may include, but may not be limited to:
  - Scoping meeting agenda.
  - Scoping presentation
  - Scoping document
  - Scoping document summary
- Establish dates and locations for three (3) meetings related to this task, one (1) all-agency scoping meeting (open to the public), and two (2) public scoping meetings, one (1) each in New York and New Jersey (with afternoon and evening sessions at each location). Set the same agenda for each of the meetings. Determine appropriate mailing lists for notice of meetings and the distribution of scoping materials.
- Attend all scoping meetings and provide administrative support. Provide digital, video and audio recordings of each scoping meeting.
- Provide input to the design team during revision of the goals and objectives based on information gathered from the scoping meetings.

### **DELIVERABLES**

- A draft scoping package outline. Incorporate work product comments and resubmit as Final.

- Draft Scoping Document
- Scoping Summary Report
- A matrix listing all of the comments received at the agency and public scoping meetings or via other means, highlighting significant issues.

## **TASK O - FACILITATE ALL PUBLIC HEARINGS**

### **WORK PLAN**

- Facilitate all public hearings held in conjunction with the EIS process.
- Utilize digital video and audio recording and a court stenographer for all public hearings. Assume two (2) public hearings, with one (1) in New Jersey and one (1) in New York.

### **DELIVERABLES**

- Draft copy of the minutes of the public hearings held for the DEIS and submit for review. Incorporate all comments and resubmit as Final. For estimating purposes, assume the same number of copies as indicated in the Plan of Study section.

# FINAL PUBLIC PARTICIPATION PROGRAM

## Introduction

The Goethals Bridge Modernization Program (GBMP) Environmental Impact Statement (EIS) is being conducted under the direction of the United States Coast Guard (USCG) as the lead federal agency, in coordination with the Port Authority of New York and New Jersey (Port Authority), the project sponsor. The Goethals Bridge spans the Arthur Kill between Staten Island, New York, and Elizabeth, New Jersey, providing direct connections between the Staten Island Expressway/West Shore Expressway on the east of the Kill, and the New Jersey Turnpike/Routes 1/9 on the west. The GBMP EIS will comprise:

- an alternatives analysis of potential options for replacement of the Goethals Bridge and addressing traffic and safety needs in the Goethals Bridge corridor;
- detailed social, economic, and environmental analysis of a short list of alternatives that appear most reasonable and feasible for satisfying the purpose and need for the project;
- and a program of public participation and interagency coordination throughout development of the GBMP EIS.

It is vital that those who are interested in or potentially affected by this study have an opportunity to share their concerns and provide input regarding the GBMP EIS. This Public Participation Program outlines the objectives, strategies, and tools that will be used to engage stakeholders and the general public throughout the GBMP EIS.

## The Environmental Review Process

The GBMP EIS will be performed in accordance with the requirements of the National Environmental Policy Act (NEPA). NEPA is a procedural act aimed at ensuring that environmental information is available to the public and public officials before decisions are made and actions are undertaken. Public participation is a requirement of the environmental review process. In addition to dealing with the public, NEPA regulations require that there be thorough and complete documentation of participation by all involved government agencies and other interested parties.

Since 1969, NEPA has been amended, regulations have been promulgated by the Council on Environmental Quality (CEQ) and other federal agencies, and a whole body of EIS “best practices” literature has been established. Regulations and best practices cover many technical issues, as well as public participation efforts. The best practice for accomplishing this is to have a public participation program that is viewed as objective. This means that:

- The action under environmental review cannot be perceived as a foregone conclusion.
- All reasonable alternatives to the proposed action, including no action, need to be considered as well.
- All social, economic and environmental impacts of the project, both adverse and beneficial, must be identified and analyzed.
- Pro-active, early, and continuous efforts need to be made to involve a broad spectrum of the public in this process. This includes study area residents and businesses, as well as a wide range of stakeholders and groups who may be affected by impacts of the action.

Throughout the NEPA process, the public participation effort focuses on gathering input and dispersing information about the following key areas:

- The purpose and need for the proposed action and goals and objectives of the action.
- The potential set of reasonable alternative actions, including not implementing the action at all.
- Methodologies that will be used to assess impacts. This typically includes such items as models that will be employed to estimate such impacts as traffic conditions, air quality and/or noise impacts, as well as methods used to assess environmental, socioeconomic, cultural resource and/or hazardous material impacts.

- Potential impacts and associated mitigation.

There are two distinct points in the NEPA process where public participation is focused: Scoping and publication of the Draft Environmental Impact Statement (known as the Draft EIS, or DEIS). However, it is valuable to engage the public during the period after scoping and prior to the Draft EIS publication, and doing so is encouraged as a good practice under NEPA.

During scoping, the plan for how the environmental review is going to be conducted is issued in draft form. It is known as the draft scoping document. The public (and all relevant agencies) are invited to offer comments on this plan, both orally at publicized meetings and via written submittals. The draft scoping document includes the project purpose and need, the range of anticipated impacts to be analyzed, the methodologies to be employed to assess impacts, and may include, at least, a preliminary range of alternatives to be considered (though these may be developed in more detail later on in the process).

When the environmental analysis is nearing completion, a Draft EIS is published for public (and agency) review. Review comments can be provided both orally at publicized hearings and via written submittals. Following completion of the comment period, a Final EIS (or FEIS) document is published and made available.

The scoping and Draft EIS review stages are formally announced via notifications in the Federal Register. Public scoping is announced by the issuance of a Notice of Intent (to prepare an EIS), while a Notice of Availability announces the publication of the Draft EIS, kicks off the comment period, and announces public hearing dates and locations.

Other public participation techniques are used throughout the NEPA process to gather and disperse important information. Federal Register notices announcing scoping meetings, public hearings and formal comment periods are typically supplemented by media releases, flyers, newsletters, website announcements, briefings and public notifications. Following scoping, the public (and agencies) are provided with opportunities to offer input to the alternatives development and analysis steps through such means as public open houses and advisory committees. Information about the status of the NEPA process is typically dispersed through newsletters and a project website.

### **Goals and Objectives of the Public Participation Program**

The public participation program is one that will require outreach to commuters, the general public, local businesses, associations, stakeholders, affected government agencies and others on both sides of the Arthur Kill to effectively engage the public in the planning and impact assessment process.

The overriding goal of the public participation program is to engage a diverse group of public and agency participants to solicit relevant input and provide timely information throughout the environmental review process. In order to best accomplish this, the following objectives will be pursued:

- Establish ongoing, inclusive and meaningful two-way communication with stakeholders, agencies and the general public.
- Educate the public about the environmental review process and the role of government, stakeholders and the general public.
- Coordinate outreach efforts with the USCG's internal protocols and policies for timely and relevant outreach activities.
- Evaluate the effectiveness of outreach activities on a continual basis in order to refine this Plan, as necessary, and utilize the most effective techniques throughout this study.

As part of this process, this public participation program will meaningfully engage minority, low-income, and traditionally under-represented populations in the GBMP EIS. As a general rule, the following principles will be adopted to support involvement of "environmental justice" (EJ) populations:

- Documents, notices and meetings will be made concise, understandable and readily accessible to the public.
- When appropriate, notices and meetings deemed will also be provided in Spanish for targeted public audiences and stakeholders.
- Informational material will be made available through a variety of outlets.
- All public events will be scheduled at convenient, accessible locations.
- Various community leaders and groups will be contacted to increase public participation of constituent communities.

## **Public Participation Techniques**

### Basic Support Mechanisms:

**Study Team Communication Protocols** – The study team will establish communication protocols early in the process to facilitate information sharing with the public and agencies in a timely and efficient manner, to comply with NEPA requirements for preparation of the GBMP EIS.

**Stakeholder Identification** – Relying on a variety of sources, including earlier environmental studies of the bridge and the corridor, stakeholders will be identified to meet in group interviews, as appropriate, and to become members of the Stakeholder Committee, which will meet at milestones throughout the EIS. These Stakeholders will represent an array of local and regional perspectives, and include representatives of environmental justice areas.

**Project Branding** – In order to assist the public in identifying project-related materials that will be produced and disseminated by the study team, a banner and readily recognizable “look” will be established and used on all project materials including meeting announcements, flyers, the website, newsletters, etc.

**Mailing List** – A mailing list will be developed for the purpose of publicizing public meeting opportunities via meeting flyers, and for keeping interested parties apprised of study developments through periodic newsletters. The list will be comprised of area residents, businesses, civic associations, shippers, commuters, community groups, schools, health care facilities, etc. Multiple copies of meeting notices and newsletters will be distributed to libraries and community centers in the study area.

**Issues and Media Log** – A log of comments received from the public and media articles relating to the project will be kept for informational and study purposes.

### Meetings:

**Interagency Technical Advisory Committee (TAC):** The TAC will include the necessary federal, state, local and regional agencies to address traffic and transportation issues, mobile-source air quality (and noise) issues related to changes in traffic volumes and patterns, and transit-related air quality issues if transit survives as either an alternative or a component of a multimodal alternative. As there are no stationary sources involved with this project, it can be assumed that all air quality (and noise) issues will be addressed in the TAC, with no overlap with any other committees formed for this project. The TAC will meet several times during the course of the GBMP EIS, for discussion among their respective agencies about the same EIS topics, but focusing on their respective jurisdictions and expertise. The first formal meeting of the TAC will not occur until after the formal agency scoping meeting.

The agencies invited to join this committee will include:

- US Environmental Protection Agency

- Federal Highway Administration
- New York State Metropolitan Transportation Authority (Bridges & Tunnels, NYC Transit)
- New York State Department of Transportation
- New York State Department of Environmental Conservation
- New Jersey Department of Transportation
- New Jersey Department Environmental Protection
- North Jersey Transportation Planning Authority
- New Jersey Turnpike Authority
- New Jersey TRANSIT
- New York Metropolitan Transportation Council
- New York City Department of Transportation
- New York City Department Environmental Protection
- New York City Economic Development Corporation
- New York City Department of City Planning
- Union County Department of Economic Development
- City of Elizabeth Traffic Engineer

**Environmental Task Force:** One of the mechanisms for eliciting participation of involved agencies in developing an EIS is formation of an Environmental Task Force (ETF). ETFs provide an opportunity for concerned agencies to interact and discuss issues and areas of potential concern, as well as provide comments on the development of the EIS. This group will comprise agencies with jurisdiction and special expertise in a wide-range of environmental categories other than traffic/transportation, air quality, and noise (which will be the focus of the TAC, discussed above) social, economic, and environmental impact categories, with the principal issues likely to be related to natural resources. The ETF will be convened several times over the course of the EIS process. Meetings will take place after formal agency and public scoping meetings. A list of potential agencies that will be invited to participate in the ETF follows.

- US Army Corps of Engineers
- US Fish and Wildlife Service
- National Marine Fisheries Service
- US Environmental Protection Agency
- Federal Highway Administration
- NYS Department of Environmental Conservation
- NYS Office of Parks, Recreation, and Historic Preservation
- NYS Department of State
- NYC Mayor's Office of Environmental Coordination
- NYC Department of City Planning
- NYC Department of Parks and Recreation
- NYC Department of Environmental Protection
- Staten Island Borough President's Environmental Representative
- NJ Department of Environmental Protection
- NJ State Historic Preservation Office
- City of Elizabeth Environmental Representative
- Union County Environmental Representative

**Elected Official Briefings:** Briefings will be held with elected officials, as requested. These briefings will be arranged in concert with the Intergovernmental Relations officers at USCG. Among the officials that will be contacted are the Staten Island Borough President, the Mayor of Elizabeth, and the Union County Freeholders. Meetings with members of the federal, state and local legislative bodies serving the study area will be held upon request.

**Agency and Public Scoping Meetings:** Following the publication of the Notice of Intent (NOI) by the

USCG, the USCG will conduct scoping meetings for agencies and for the public. The purpose of these meetings is to gather input and feedback on the study's draft purpose and need statement, and potential alternatives for consideration; issues to be addressed in the EIS; methodologies to be used to evaluate impacts; and the public participation program. One agency scoping meeting will be held at the USCG offices, and public scoping meetings will be held in Staten Island, NY and in Elizabeth, NJ spanning both afternoon and evening hours to gather as broad participation as possible. Meeting participants may make statements orally, which will be transcribed by a stenographer, or submit comments in writing either at a scoping meeting or subsequently by mail during the scoping comment period. Meeting announcements will be mailed to the GBMP EIS mailing list, posted at libraries and community centers, announced through media press releases, through paid advertisements in newspapers, and posted on the GBMP EIS website. Upon request by a prescribed date prior to the public scoping meetings, Spanish translators and/or assistance to individuals with hearing or sight impairment will be provided at meetings for which such services are requested. The public scoping meetings will take place in central, convenient locations, and the facilities will be fully accessible to those with disabilities.

**Stakeholder Committee Meetings:** The Stakeholder Committee will provide an open forum for discussion and encourage interaction among key stakeholders, who represent a cross-section of organizations and interests. Organizations that join the Stakeholder Committee will be invited to assign a representative to the Committee. The Committee will update its membership as additional interested organizations are identified. Stakeholder Committee members will agree to bring their members' concerns to the attention of the project team, and bring project information back to their membership. The Stakeholder Committee will meet several times in the EIS process. The first meeting will be held shortly after Public Scoping to review the methodology and criteria by which the alternatives will be screened, as well as to review the long list of alternatives. Below are the types of organizations expected to be represented on the Stakeholder Committee:

- TransOptions (TMA)
- Tri-State Transportation Campaign (TSTC)
- Regional Plan Association
- Environmental Defense
- NRDC
- Alliance for Action
- Local Emergency Services
- CSX
- Shipping (UPS/FedEx)
- Private Bus Operators
- Trucking Associations
- AAA (NY & NJ)
- Chambers of Commerce
- American Lung Association/Other Health Groups
- Hospitals
- Schools & Colleges
- Large Employers in SI & NJ
- Utilities (PSE&G, ConEd, KeySpan)
- Service/Community Groups who serve low-income and and/or minority populations
- Civic Associations
- Brooklyn-based Groups (Gowanus Area)
- Staten Island Borough President
- Mayor of Elizabeth

If deemed appropriate, non-PANYNJ personnel from the following facilities:

- Newark Liberty International Airport

- Port of Elizabeth
- Port Newark
- Howland Hook Marine Terminal

**Public Open Houses:** Between Public Scoping and the Public Hearings upon completion of the Draft EIS, there will be two rounds of public open houses. Each round will include a meeting in Staten Island, NY and Elizabeth, NJ. These meetings will include both static displays and informal discussions with EIS team members and meeting attendees, as well as presentations. These meetings will be publicized in a similar manner to the Public Scoping meetings. The first open houses will follow the first Stakeholder Committee to review the EIS process, the screening criteria and methodology for reducing the list of alternatives to the short list. The second round will review the impacts of the alternatives undergoing detailed analysis.

**DEIS Public Hearings:** After circulation of the Draft EIS, public hearings will be held both in Staten Island, NY and in Elizabeth, NJ to gather comments on the document. The document will be available for review 30 days prior to these hearings, and the public comment period will be open for an appropriate period of time following the hearings. The comments received during the hearing process will be addressed in the Final EIS. The method for publicizing these hearings, and the format of the hearings themselves will be similar to the public scoping meetings described above.

Materials:

**Newsletters:** Four newsletters will be produced and disseminated to the project mailing list for the purpose of educating the general public about the EIS process, providing information on the study as it progresses, announcing public participation opportunities, and providing project team contact information. These newsletters will be written in straightforward language. Graphics will be used to assist in communicating the appropriate information. The first newsletter will announce the public scoping meetings, the second will review the results of the alternatives analysis, the third will announce the Draft EIS completion, and the fourth will announce the completion of the Final EIS.

**Meeting Announcements:** Meeting flyers will be used to publicize all public meetings. These flyers will be in English and Spanish, and will be mailed to the project mailing list, and distributed in bulk to libraries and community centers.

**Press Releases:** Press releases will be drafted in advance of public meeting opportunities and to announce the availability of project materials. These releases will be submitted to USCG for their review and release. It is anticipated that there will be at least 5 releases to announce public scoping, the two rounds of public open houses, the DEIS public hearings, and the availability of the DEIS and FEIS.

**Website:** A public website will be developed for the GBMP EIS and will include information on the EIS process, project activities and progress, public participation opportunities and project contact information; and will have downloadable documents (pdf format) for information and/or review.

**Meeting Materials:** Handouts will be available at all public meetings for attendees. Public meeting presentations will be available as handouts, as appropriate, and on the website for review by those unable to attend meetings.

**Implementation of the Public Participation Plan**

A three-phase public participation effort is envisioned for the GBMP EIS, as listed below:

- Scoping – during scoping, agencies and the public will comment on the study purpose and need statement, analysis methodologies and the alternatives that will be considered in the Alternatives Analysis.

- Alternatives Analysis – during the Alternatives Analysis phase, the long list of alternatives gathered in scoping will be reduced through a process by which selection criteria are applied to the alternatives. The selection criteria and methodology for reducing the number of alternatives will be shared with the TAC, ETF, Stakeholder Committee and the public. Alternatives will be evaluated based on their transportation performance, environmental impacts and costs.
- Draft EIS and Final EIS – the final short list of alternatives will be put through a rigorous evaluation of impacts, mitigation, and costs prior to selecting the final alternative as part of the Final EIS. The USCG will, upon completion of the Final EIS, publish a Record of Decision for the action that has been agreed to through this process.

A discussion of outreach activities that are anticipated under each phase follows.

- Scoping: During this phase, the following activities will be undertaken:
  - Complete and publish the first newsletter
  - Launch public website
  - Hold briefings with local municipal officials, other elected officials, as requested
  - Conduct Agency Scoping meeting
  - Conduct Public Scoping meetings
  - Update mailing list
  - Present issues log information to USCG and the Port Authority and utilize as input into the scoping process
- Alternatives Analysis: During this phase, the following activities will be undertaken:
  - Update website materials
  - Meet with TAC and ETF and Stakeholder Committee on Screening Criteria and Long list of alternatives, and the Short List of Alternatives and their impacts
  - Draft press releases to announce public meetings
  - Conduct two rounds of Public Open houses on Screening Criteria and Long list of alternatives, and the Short List of Alternatives and their impacts
  - Prepare draft and publish second newsletter
- DEIS and FEIS: During this phase, the following activities will be undertaken:
  - Prepare drafts and publish third and fourth newsletters
  - Update website materials
  - Meet with TAC and ETF and Stakeholder Committee
  - Conduct Elected Official briefings, as requested
  - Draft press releases to announce public hearings and availability of the FEIS
  - Conduct public hearings for DEIS
  - Present issues log information to USCG and the Port Authority and utilize as input into the DEIS review process
  - Categorize agency and public comments on the DEIS, and prepare responses.

### **Evaluation of the Public Participation Program**

Evaluation of the public participation program is important to the EIS process. The purpose of carrying out this program review is:

- To get valuable input that can make the whole public participation process more effective as well as increasing the chance of its successful completion.
- To ensure the public and concerned parties are reached and engaged in the process.

Some examples of critical questions and techniques that will be considered to assess the public participation program include:

- How many hits does the project website receive?
- Are EJ populations and typically under-represented groups involved?
- Are input and comments pertinent and substantive, showing understanding of project information disseminated to the public?
- Conduct brief survey/questionnaire at public meetings for participants to judge the value of the activities.
- Review content of issues log to judge the value of the overall outreach effort.

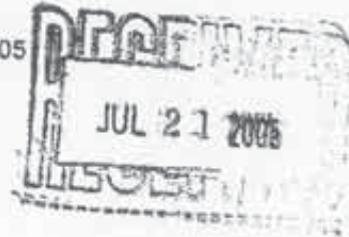
The results of ongoing evaluation will be discussed with USCG officials, with the intent of making mid-course refinements to the public participation program, as appropriate.



New York State Office of Parks, Recreation and Historic Preservation  
 Historic Preservation Field Services Bureau  
 Peebles Island, PO Box 189, Waterford, New York 12188-0189

518-237-8643

July 14, 2005



Gary Kassoff  
 Bridge Program Manager  
 First Coast Guard District  
 One South Street  
 Battery Building  
 New York, New York 10004

Re: USCG/PANYNJ  
 Goethals Bridge Replacement  
 Staten Island, Richmond Co., NY  
 04PR03162

Dear Mr. Kassoff:

Thank you for your letter of June 17, 2005, by which you initiated consultation in accordance with Section 106 of the National Historic Preservation Act of 1966 with regard to the proposed replacement of the Goethals Bridge.

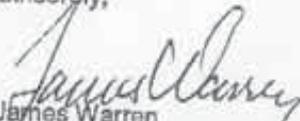
New York State Historic Preservation Office (NYSHPO) accepts the proposed Area of Potential Effect indicated by attachments to your June 17<sup>th</sup> letter as the combined "Primary Study Area" and "Secondary Study Area," subject to refinement as preferred alternatives are defined during the exploration of alternatives phase of the study.

We note your designation of the Louis Berger Group/ Parsons Brinckerhoff, Inc. Joint Venture as environmental consultants assisting USCG in evaluating project alternatives and potential environmental impacts of this undertaking; NYSHPO will communicate directly with you or with Ms. Sara Moss of the Berger/PB team unless directed otherwise.

Although the prior Staten Island Bridges Program study (EIS 1997) referred to the "Modernization and Capacity Enhancement" of Staten Island Bridges, and despite assurances that all alternatives are being explored, including reuse of the existing Goethals Bridge, the NYSHPO is not encouraged by the consistent identification of the current project as the "Goethals Bridge Replacement" project. We recommend the continuing "good faith" exploration of alternatives that include "modernization and capacity enhancement" of the Goethals Bridge.

If you have any questions or comments regarding this response, please call me at (518) 237-8643, extension 3283 or email me at james.warren@oprhp.state.ny.us.

Sincerely,

  
 James Warren  
 Historic Preservation Program Analyst

Copy: Andrea Tingey, NJSHPO  
 Sara Moss, BTA

U.S. Department of  
Homeland Security

United States  
Coast Guard



Commander  
First Coast Guard District

One South Street  
Battery Building  
New York, NY 10004

Staff Symbol: obr  
Phone: 212 668-7165  
Fax: 212 668-7967

July 25, 2005

Mr. James Warren  
Historic Preservation Program Analyst  
New York State Office of Parks, Recreation and Historic Preservation  
Historic Preservation Field Services Bureau  
Peebles Island, PO Box 189  
Waterford, New York 12188-0189

Re: **Goethals Bridge Replacement Environmental Impact Statement (GBR EIS),  
Staten Island, Richmond County, NY – 04PR03162  
Section 106 Consultation**

Dear Mr. Warren,

Thank you for your letter of July 14, 2005, in which you accepted the proposed Area of Potential Effect for the study of historic resources, project alternatives, and potential project-related impact as part of the GBR EIS, and indicated that NYSHPO will communicate directly with me or with the environmental consultant team that is assisting this office with preparation of the GBR EIS.

In response to your reference to the previous (1997) EIS of the Staten Island Bridges, I assure you that the current project designation, the Goethals Bridge Replacement, does not preclude consideration of all reasonable alternatives, including exploration of the potential reuse of the existing bridge. The alternatives screening analysis for identification of a short list of alternatives to be evaluated in detail in the EIS is still in progress.

Again, thank you for your response and, in advance, for your continued consultation with the U.S. Coast Guard on the referenced study.

Sincerely,

  
Gary Kassof  
Bridge Program Manager  
First Coast Guard District  
By Direction of the District Commander

Copy: Andrea Tingey (NJSHPO); Michelle Hughes (NJSHPO); Ernie Feemster (USCG); J. Blackmore, Coleen Hopson (PANYNJ); Ken Hess, Judith Versenyi, Esther Schwalb (Berger/PB); Sara Moss (BTA)

U.S. Department of  
Homeland Security

United States  
Coast Guard



Commander  
First Coast Guard District

One South Street  
Battery Building  
New York, NY 10004

Staff Symbol: obr  
Phone: 212 668-7165  
Fax: 212 668-7967

October 31, 2005

Ms. Dorothy Guzzo  
Deputy State Historic Preservation Officer  
New Jersey Department of Environmental Protection  
Historic Preservation Office  
P.O. Box 404  
Trenton, NJ 08625-0404

**Re: Goethals Bridge EIS/ Field Visit of 10-17-05**

Dear Ms. Guzzo:

As the lead federal agency for the referenced project, the U.S. Coast Guard thanks you, Andrea Tingey and Michelle Hughes for their participation in the October 17<sup>th</sup> field trip to the Goethals Bridge and environs. It was valuable for us to visit the bridge and vicinity with them in order to better understand their concerns, and also to be able to see the bridge firsthand and witness the extent and progress of the ongoing rehabilitation work.

As was requested, we have included the following information as attachments to this letter:

- A set of alignment concept drawings for the four build alternatives that are currently being advanced through the comparative screening process (i.e., 6-Lane Replacement Bridge South; 6-Lane Replacement Bridge North; Twin Replacement Bridges South; and Twin Replacement Bridges North) in order to identify those that will be studied in greater detail in the Draft EIS;
- Notes from the October 17 Field Visit.

In addition, digital photos taken of the study area, including those locations of historic interest that have views of the bridge, will be forwarded to you early next week by the Berger/PB consulting team working with us to prepare the GBR EIS.

It is our understanding that the above defined information will assist you and your staff in further refining the previously-identified limits of the Area of Potential Effect (APE) for this project. We are specifically focusing on the APE issue in this letter in order to finalize the limits of the APE for our study, thereby allowing us to continue addressing existing conditions in a timely fashion. We look forward to receiving your proposed refinements to the APE in the near future. In the interim, we will hold your 25 July 2005 letter in abeyance pending receipt of your final recommendations on the APE.

Goethals Bridge EIS/Field Visit

I have considered Andrea's suggestion to include NJ Turnpike Interchange 13 in the GBR APE. For the following reasons it is the Coast Guard's position that this would neither be practical nor required to ensure a comprehensive environmental investigation under the National Environmental Policy Act (NEPA):

- a) The NJ Turnpike Authority is not currently studying Interchange 13 as part of its ongoing Toll Plaza Improvement Studies north of Interchange 9. They have no specific plan to study it until after the GBR EIS studies have been completed. As a result, the Coast Guard does not consider it within our environmental mandate or responsibility to consider Interchange 13 within this project's APE.
- b) All the GBR alternative alignments currently under consideration to be carried forward to the DEIS are assumed to connect directly with the existing NJ Turnpike interchange ramp configurations since there is no basis to assume otherwise at this stage.

Therefore, I am requesting that any refinements that the NJHPO proposes to the APE be based on the existing configuration of Interchange 13 and its toll plaza.

Once again, thank you for your staff's participation and consideration in this undertaking. The USCG looks forward to your continued involvement in the GBR EIS process and associated Section 106 process.

Please call me at 212-668-7021, or Sara Moss of B. Thayer Associates (a member of the Berger/PB team) at (212) 564-2750, if you have any comments or questions concerning the enclosed information.

Sincerely,

Gary Kassof  
Bridge Program Manager  
First Coast Guard District  
By direction of the District Commander

Enclosures:

Copy: Andrea Tingey (NJHPO);  
Michelle Hughes (NJHPO);  
James Warren (NYSHPO);  
Ken Hess (Berger/PB)  
Judith Versenyi (Berger/PB)  
Esther Schwalb (Berger/PB);  
Sara Moss (BTA)

# FIELD MINUTES

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## GOETHALS BRIDGE REPLACEMENT EIS

**DATE:** October 17, 2005  
Andrea Tingey, Michelle Hughes (NJHPO)  
**To:** Gary Kassof, Ernie Feemster (USCG)  
Coleen Hopson, Camille Gonzalez (PANYNJ)  
Judith Versenyi, Esther Schwalb, Ken Hess (Berger/PB); Sara Moss (BTA)  
**FROM:** Esther Schwalb, Sara Moss  
**SUBJECT:** Field Review of 1994 Historic Resources Inventory with NJHPO  
**CC:** James Warren (NYSHPO), Jim Blackmore, Ed Lopez, Paul Crist, Phil Dinh, Lou Venech, Joann Papageorgis (PANYNJ)

---

**DATE/LOCATION:** Monday, October 17, 2005; 9AM-1 PM  
Goethals Bridge Administration Building, Staten Island, New York

**ATTENDEES:** Attendance sheet attached.

**PURPOSE OF MEETING:** Agenda attached.

**ACTION ITEMS:**

Item #	Description	Responsibility (follow up/action)
1	Send alignment concept drawings to NJHPO.	Berger/PB and USCG
2	Send notes from 10/17 field visit.	Berger/PB and USCG
3	Send photos from project area to NJHPO.	Berger/PB
4	Refine APE based on field visit and visibility from other areas.	NJHPO

# FIELD MINUTES

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## GOETHALS BRIDGE REPLACEMENT EIS

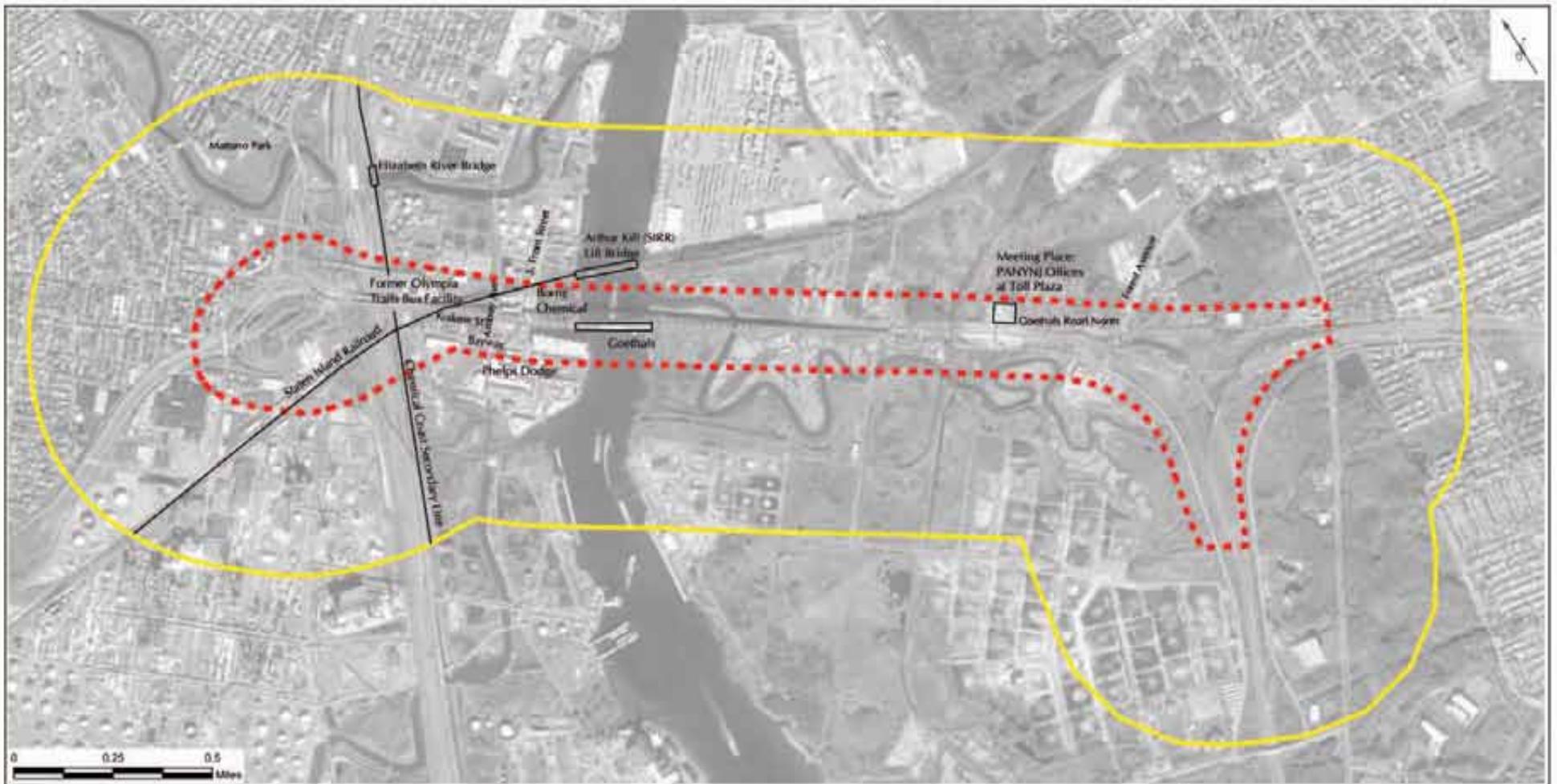
1. At the meeting held in the Goethals Bridge Administration Building prior to the field trip, the Area of Potential Effect (APE) was discussed and it became clear that there had been a previous misunderstanding regarding the level of identification and inventory required for properties located within it. The NJHPO stated that all potentially historic properties within the APE should be inventoried, and entire buildings and complexes should be investigated for historic association even if they partially extend beyond the APE line. Since the APE had recently been accepted, via the NJHPO's letter dated July 25, 2005, at ½ mile around the project area, the NJHPO indicated that they would consider refinement of the APE after reviewing alignment details (e.g., touchdown locations), heights, and indirect visual impacts. The NJHPO felt that a revised APE could extend beyond the proposed APE in some places but could be reduced elsewhere.
2. The height of the replacement bridge is not yet defined, but it is expected to be between 280 to 310 feet in elevation.
3. The existing GB has a 135-foot vertical clearance above the mean high water and the proposed bridge is expected to be approximately the same.
4. The NJHPO said that the APE should be drawn to include possible views with worst-case (tallest potential) heights.
5. The NJHPO felt that rehabilitation of the existing GB should be carried through the EIS process as an alternative even if it is not preferred.
6. The NJHPO asked for a photosimulation of the Arthur Kill Lift Bridge in the lowered position with the new GB in place.
7. The NJHPO requested a description of the methodology used in the 1994 survey and suggested that it didn't meet today's NJ standards (1999).
8. The NJHPO will sketch out an APE based on the field visit and suggest methodologies to conduct the survey. The NJHPO office has historic aerial photography that (together with historic Sanborns) would help determine whether a property is 50 years old or more. NJHPO expressed interest in including the industrial area south on the GB bordering on the Arthur Kill in the APE but was concerned about access limitations and resulting difficulty in conducting research.
9. The NJHPO suggested that Berger/PB review the Route 1/9 Tonnelle Avenue Circle EA since a multi-disciplinary district assessed in that study was similar to the houses on Bayway Ave.
10. Photos were taken during the field trip of three buildings that had been modified since the last survey: 100-103 Bayway Ave. (new windows); 114 Krakow Street (boarded up); 120 Krakow St. (new addition made to rear of building). Two buildings had been demolished in the intervening years: 123 Bayway Ave. and 58-70 Bayway Avenue (Phelps Dodge Building #8).
11. The NJHPO requested that the historic resource review include any proposed Turnpike ramp connecting to the GB. It was agreed that the USCG would provide a response to this request.

# FIELD MINUTES

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## GOETHALS BRIDGE REPLACEMENT EIS

12. The field visit included the opportunity to view the ongoing Goethals Bridge rehabilitation from up close. This opportunity provided the ability to better understand the magnitude of the project and its structural / rehabilitation needs.



**Legend**

- Secondary Study Area
- Primary Study Area

Goethals Bridge Replacement EIS
<b>Primary and Secondary Study Area</b>

**Feemster, Ernest**

---

**From:** Andrea.Tingey@dep.state.nj.us on behalf of Andrea Tingey [Andrea.Tingey@dep.state.nj.us]  
**Sent:** Wednesday, December 07, 2005 3:09 PM  
**To:** Gkassof@batteryry.uscg.mil  
**Cc:** efeemster@batteryry.uscg.mil; smoss@bthaayerassociates.com; Michelle Hughes; jmagron@louisberger.com; khess@louisberger.com; James.Warren@oprhp.state.ny.us; cgonzalez@panynj.gov; chopson@panynj.gov; Versenyi@pbworld.com  
**Subject:** Revised Goethals APE  
**Attachments:** Goethals Bridge APE.pdf

Gary,

I have the revised APE for NJ as a pdf (attached). I will be sending a hard copy along with a formal letter under DSHPO signature, but thought you (and the Port Authority's consultants) would appreciate an electronic version.

Best,

Andrea Tingey  
Principal Historic Preservation Specialist  
Historic Preservation Office  
P.O. Box 404  
Trenton, NJ 08625-0404  
(p) 609-984-0539  
(f) 609-984-0578  
(e) [Andrea.Tingey@dep.state.nj.us](mailto:Andrea.Tingey@dep.state.nj.us)

1/5/2006



0 0.125 0.25 0.5 0.75 Miles

 Goethals Bridge APE 12/07/2005



16591//Goethals Bridge  
March 10, 2006

Ms. Dorothy Guzzo  
Deputy State Historic Preservation Officer  
New Jersey Department of Environmental Protection  
Historic Preservation Office  
P.O. Box 404  
Trenton, NJ 08625-0404

Re: Goethals Bridge EIS/Area of Potential Effect

Dear Ms. Guzzo:

The U.S. Coast Guard thanks you and Andrea Tingey for her recent input via e-mail on December 7, 2005, in which she defined suggested limits of the Area of Potential Effect (APE) for the above-ground cultural (historical/architectural) resources of subject project on the New Jersey side of the project corridor.

Ms. Tingey's transmittal followed our joint field trip to the Goethals Bridge and its environs on October 17, 2005, and my subsequent transmittal to your office of a set of alignment concept drawings for the four build alternatives that are currently being advanced through the alternatives screening process, to identify those that will be studied in greater detail in the Draft Environmental Impact Statement (DEIS). Berger/PB, our environmental consultant team, also forwarded to Ms. Tingey, digital photos taken of the study area during our joint field trip, including those locations of historic interest that have views of the bridge.

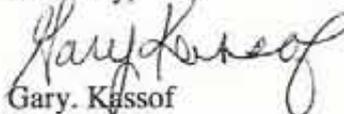
Since the suggested APE defined by Ms. Tingey differed somewhat from our initial submittal to your office, we enlisted the assistance of an architectural historian to assist us in better defining the limits of an appropriate APE. A windshield survey of the project area, including areas within the APE suggested by your office was conducted with the historian and with other Berger/PB cultural resources staff. Based upon the survey and previous documentation, the enclosed *Technical Memorandum on Consideration of the Area of Potential Effect for Historical/Architectural Resources* was developed. The memorandum addresses considerations of the nature and scale of the proposed project, various ways in which the proposed project could reasonably be demonstrated to affect historic properties, and the existing built environment in which the project would be implemented.

On this basis, we feel that the limits of the APE should be defined, as shown on the aerial map in the enclosed memorandum as it represents a reasonable area of potential effect for this undertaking. I have directed Berger/PB to undertake the identification of historic/architectural

properties within this APE and to prepare the inventory documentation for subsequent transmittal to your office for review.

The U.S. Coast Guard looks forward to your continued involvement in this EIS process and associated Section 106 consultation process. Please call me at 212-668-7021 if you have any questions or comments concerning the enclosed information and our determination of the limits of the APE for this project.

Sincerely,



Gary. Kassof  
Bridge Program Manager  
First Coast Guard District  
By direction of the District Commander

Enclosures:

*Technical Memorandum, Consideration of the Area of Potential Effect for  
Historical/Architectural Resources*

Copy: Andrea Tingey (NJHPO)  
Michelle Hughes (NJHPO)  
James Warren (NYSHPO)  
Ken Hess (Berger/PB)  
Judith Versenyi (Berger/PB)  
Esther Schwalb (Berger/PB)  
Marty Bowers (Berger/PB)  
Sara Moss (Berger/PB)

# TECHNICAL MEMORANDUM

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## GOETHALS BRIDGE REPLACEMENT EIS

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**DATE:** March 10, 2006  
**TO:** G. Kassof, E. Feemster  
**FROM:** K. Hess, J. Versenyi, M. Bowers  
**SUBJECT:** Consideration of the Area of Potential Effect for Historical/Architectural Resources  
**CC:** J. Blackmore, C. Hopson, P. Dinh, E. Lopez, C. Gonzalez, E. Schwalb

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### 1. INTRODUCTION

This memorandum summarizes a review of information and issues regarding the Area of Potential Effect (APE) for above-ground (historical/architectural) cultural resources on the New Jersey side of the project corridor for the proposed replacement of the Goethals Bridge. Based on this review, this memorandum also recommends revised APE boundaries for U.S. Coast Guard (USCG) consideration and consultation with the NJHPO.

In June 2005, the USCG initiated consultation with the New Jersey Historic Preservation Office (NJHPO), pursuant to Section 106 of the National Historic Preservation Act. At that time, the USCG requested NJHPO's concurrence with a preliminary APE (to be refined once project alternatives were identified), which was based on the combined primary and secondary study areas previously delineated for the 1997 Staten Island Bridges Program (SIBP) FEIS. In the previous EIS, the historic resources inventory was conducted within 400 feet of all project alternatives' alignments, and impacts to historic resources were assessed within the combined primary and secondary study areas, which extended one-half mile in all directions from the project corridor. The NJHPO initially concurred with an APE comprising the primary and secondary study areas; however, following a site visit by NJHPO representatives and project staff in October, 2005, the NJHPO recommended a substantially different APE, apparently based on potential impacts, including visual effects.

### 2. DEFINITIONS OF THE ADVISORY COUNCIL ON HISTORIC PRESERVATION (ACHP)

The following definitions (in 36 CFR Part 800, Protection of Historic Properties) guide consideration and delineation of an APE:

- **Area of Potential Effect** (36 CFR 800.16(d)): *Area of potential effect* means the geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties, if any such properties exist. The area of potential effect is influenced by the nature and scale of an undertaking and may be different for different kinds of effects caused by the undertaking.
- **Effect** (36 CFR 800.16(i)): *Effect* means alteration to the characteristics of a historic property that qualifies it for inclusion in or eligibility for the National Register.

# TECHNICAL MEMORANDUM

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## GOETHALS BRIDGE REPLACEMENT EIS

There are no uniform guidelines for federal agencies (nor, therefore, for SHPOs) to use in determining an APE for Section 106 purposes. The Council's guidance in the matter is limited to its statement in 36 CFR 800.16(d), as noted above, that "The area of potential effect is influenced by the nature and scale of an undertaking and may be different for different kinds of effects caused by the undertaking."

### 3. PROJECT CHARACTERISTICS

The following information regarding the proposed Goethals Bridge replacement is pertinent to the consideration of an appropriate APE:

- The proposed replacement bridge(s) would be sited immediately up- or downstream from the existing bridge, which would remain in service until the replacement was completed.
- The overall design envelope of the replacement bridge(s) would be similar to that of the existing bridge, involving long elevated approaches to each end of a channel span.
- The replacement would have a wider "footprint" than the existing bridge (potentially ranging from 167 to 198 feet wide, depending on the alternative's configuration; the existing width is 62 feet) to accommodate six, rather than the existing four, travel lanes and sufficient width for a 10-foot-wide sidewalk/bikeway and potential mass transit use.
- The replacement bridge(s) would have more widely spaced piers designed to be entirely outside the Arthur Kill's navigable channel. Similar to the existing bridge, the replacement bridge(s) would have a minimum vertical clearance of approximately 135 feet above mean high water.
- At the west end, the replacement structure(s) would tie directly into the existing NJ Turnpike/I-278 interchange ramp configurations, as the existing Goethals Bridge currently does.

### 4. CHARACTERISTICS OF EXISTING BUILT ENVIRONMENT

The Goethals Bridge rises out of a dynamic urban/industrial environment. The existing land use patterns were basically established in the 19<sup>th</sup> century, predicated in large measure on maritime and railroad transportation and the access both provided to raw materials and markets. Twentieth-century developments in transportation followed 19<sup>th</sup> century alignments: the auto road across the Arthur Kill to Staten Island (Goethals Bridge) beside the much earlier Baltimore and New York Railroad (Arthur Kill) crossing, and the NJ Turnpike beside the much earlier Central Railroad of New Jersey (now Conrail's Chemical Coast Line). (See Figure 1: Key to Photo Locations and Photo 1 for an aerial view of the Goethals Bridge and the Arthur Kill waterfront).

The area immediately around the bridge approach (roughly between the Elizabeth River and Morses Creek) is intensively developed. This development began in the mid- to late 19<sup>th</sup> century at what was probably then a neck of fast land providing access to the Arthur Kill (at a relatively narrow point) and buildable ground for industry. East of the NJ Turnpike, the Goethals and Arthur Kill Lift bridges and approaches are by far the most dominant features. Below and close to either side of the approaches are found closely spaced late 19<sup>th</sup> to mid-20<sup>th</sup> century industrial buildings, varying from one to several stories,

# TECHNICAL MEMORANDUM

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## GOETHALS BRIDGE REPLACEMENT EIS

with brick, concrete, concrete block, or metal-clad exteriors. Here also are brick and wood frame remnants of the residential neighborhood that grew up in response to the industrial development here. Toward the Elizabeth River, the more open reclaimed marshland features industrial buildings and small tank farms.

Immediately west of NJ Turnpike Interchange 13 (north of the long elevated access ramps between Route 1 and the NJ Turnpike) is a densely built-up urban neighborhood fanning out from Bayway, consisting primarily of low-scale (2.5 stories generally being the maximum height), wood frame and brick-masonry residences and small mixed-use blocks dating to the late 19<sup>th</sup> to early 20<sup>th</sup> centuries, terminating at the interchange in service stations from late 20<sup>th</sup> century. To the north are the Halloran School, Mattano Park (containing a channelized stretch of the Elizabeth River), and a large PSE&G electrical substation, from which emanate lines of tall steel transmission towers.

Downstream from Morses Creek, the environment is characterized by an almost abstract landscape of large-scale late 20<sup>th</sup> century infrastructure and industry that are rather widely scattered across flat, partially reclaimed marshland transected by the former Central Railroad of New Jersey alignment and the NJ Turnpike, with a PSE&G generating station on the waterfront on the north side of Piles Creek.

### 5. POTENTIAL EFFECTS TO CONSIDER IN DELINEATING THE APE

The following considerations are pertinent to delineation of the APE.

*a. Potential effects involving physical destruction of or damage to all or part of a historic property:*

The area in which these kinds of effects could occur would encompass the existing bridge and approach corridor, as well as the corridors of proposed new alignments up- or downstream, including:

- All locations where buildings or structures are to be removed (demolished);
- All locations where buildings or structures could suffer damage during demolition of adjacent buildings (e.g., shared party walls or foundations, or proximity that could place them in the way of construction equipment);
- All locations contiguous to and within a defined lateral distance from the outer limits of construction/demolition (as an example, the 90 feet specified in New York City Department of Buildings Technical Policy and Procedure Notice #10/88 regarding “fragile” buildings (including historic buildings and structures)), in which construction-induced ground vibration could damage foundations or structural systems; and
- Locations where the operation of construction equipment could inadvertently damage historic buildings or structures.

# TECHNICAL MEMORANDUM

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## GOETHALS BRIDGE REPLACEMENT EIS

*b. Potential effects involving changes in use or changes to physical features within a property's setting (including introduction of incompatible visual, atmospheric or audible elements) that contribute to its historical significance:*

The project proposes to replace an existing bridge with another in essentially the same location to maintain this important transportation artery connecting New Jersey and New York. The project will not introduce any new features inconsistent with those already present in the built environment, or out of character with this built environment's historical development.

The potential of the project to diminish the integrity of a property's significant historic features is limited to those locations suggested for inclusion in the APE, as described below.

**Elizabeth, East of the NJ Turnpike between the Elizabeth River and Morses Creek:** The intent of the project is to continue using the area east of the NJ Turnpike between the Elizabeth River and Morses Creek as a transportation corridor, albeit with a replacement structure. However, demolition of the existing bridge and construction of a replacement structure(s) within an expanded right-of-way could prompt changes in adjacent land use that could involve historic properties.

Because this area immediately around the Goethals Bridge approach is relatively confined, the setting is relatively intimate (even given the large scale of many elements within it). Both the Goethals Bridge and the adjacent Arthur Kill Lift Bridge are dominant features of this old industrial area. Removal and replacement of the existing Goethals Bridge and approach would thus transform the character of the built environment here and, as a result, transform the integral setting of any other historic properties in this area (Photos 2, 3, and 4).

The area between the Elizabeth River and Morses Creek east of NJ Turnpike should, therefore, be included within the APE.

**Elizabeth, North and Northwest of NJ Turnpike Interchange 13:** The NJ Turnpike Interchange 13 and associated toll plaza, and the NJ Turnpike itself (four lanes plus exit/entrance ramps), plus the double line of electrical transmission towers emanating from the PSE&G substation together constitute a physical and substantial visual barrier between the residential Elizabeth neighborhood north and northwest of the interchange and the Goethals Bridge. Due to the density of the built environment, the bridge is not visible from most locations within this neighborhood. The open, slightly sloping ground of Mattano Park affords the most "immediate" views of the bridge (and also of the Arthur Kill Lift Bridge and a PSE&G substation) both from the park itself and from the turn-of-the-20<sup>th</sup> century, closely spaced dwellings that overlook the park from Fifth Avenue (Photo 5). Limited views of the bridge are also available from locations along Pulaski Street near the northern edge of the interchange ramps (Photos 6 and 7). Therefore, these areas should be included in the APE.

Due to the flat topography and the visual barrier presented by the interchange, replacement of the Goethals Bridge would have no demonstrable potential to effect changes to historic properties (should any historic properties exist) in the residential neighborhood west of Pulaski Street, nor to any contributing attributes of such properties' settings or historical associations. Absent future project information to the

# TECHNICAL MEMORANDUM

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## GOETHALS BRIDGE REPLACEMENT EIS

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contrary, the residential neighborhood west of Pulaski Street appears to lie outside the area of potential effect for this project.

**Elizabeth, West of NJ Turnpike Interchange 13 and South of I-278:** Due to the flat topography and the visual barrier presented by the interchange, replacement of the Goethals Bridge would have no demonstrable potential to effect changes to historic properties (should any historic properties exist) in the residential area directly west of the interchange and the industrial area southwest of the interchange and south of I-278, nor to any contributing attributes of such properties' settings or historical associations. Absent future project information to the contrary, these residential and industrial areas west/southwest of the interchange and south of I-278 appear to lie outside the area of potential effect for this project.

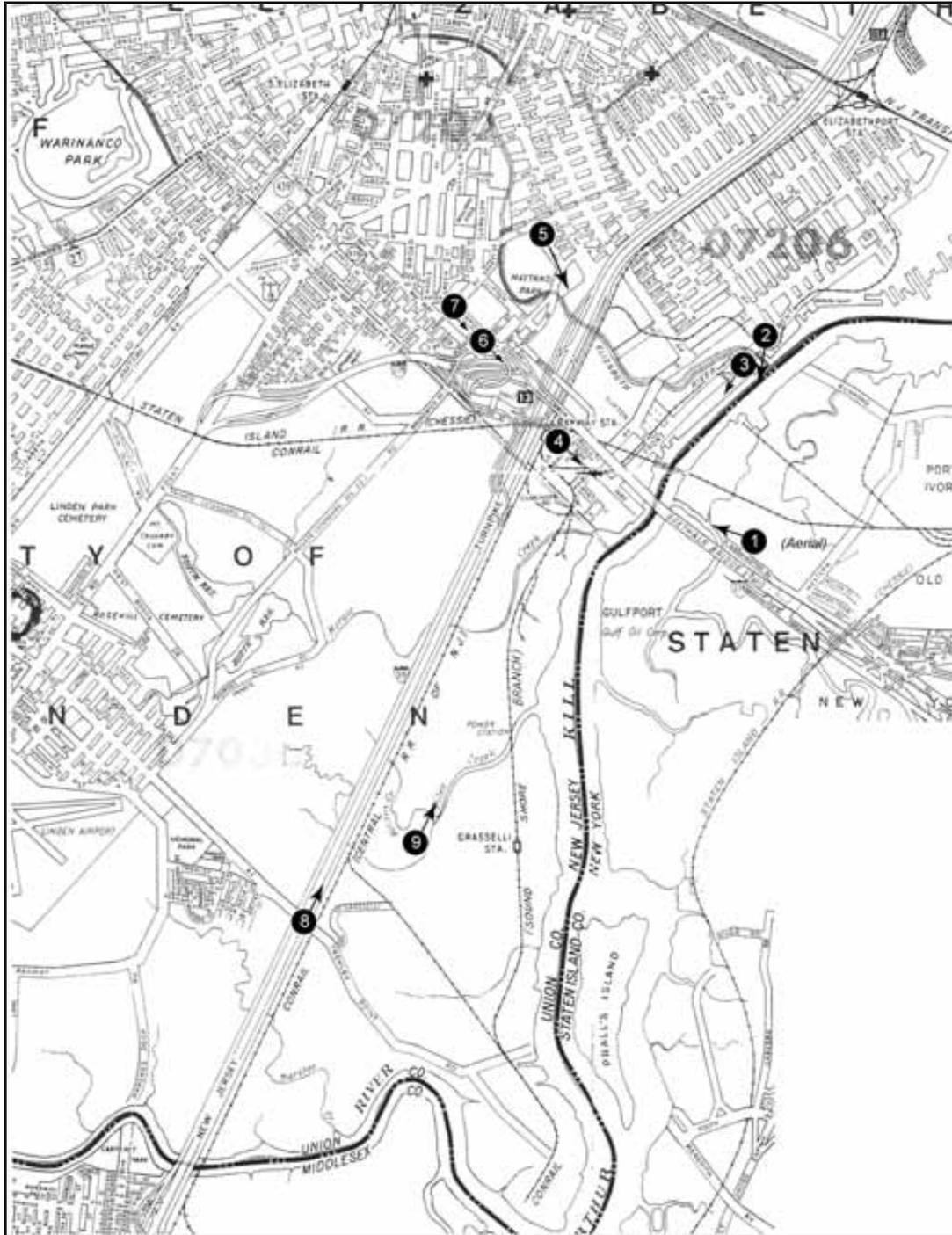
**Linden, East of NJ Turnpike and South of Morses Creek:** Despite the scale of the Goethals Bridge, the structure's prominence in the built environment diminishes rapidly with distance. This may be due to the visual "lightness" of the channel truss and the attenuated character of the long deck approaches. However, it is also due to the proximity of other very large-scale features, among them the Arthur Kill Lift Bridge, the NJ Turnpike and Interchange 13, PSE&G transmission towers, and the sprawling Standard Oil refining and storage facilities just over the city line in Linden. Built by the Port Authority of New York and New Jersey primarily to improve access to Staten Island, the Goethals Bridge appears to have had little discernable influence on the already established industrialization of New Jersey's Arthur Kill waterfront. Maps of Union County from 1923 (pre-Goethals Bridge) and 1951 (post-Goethals Bridge) (Figures 2 and 3) offer clear evidence of the development of the original, rather peripheral area of Elizabeth between Bayway and the Elizabeth River over this period (whether attributable to the bridge or not) but essentially no change in the patterns of land use and transportation below Morses Creek in Linden. This review concludes that replacement of the Goethals Bridge would have no demonstrable potential to effect changes to historic properties (should any exist) in this area of Linden east of the NJ Turnpike nor to any contributing attributes of such properties' settings or historical associations. Therefore, the area of Linden east of the NJ Turnpike south of Morses Creek appears to lie outside the area of potential effect for this project (Photos 8 and 9).

## 6. CONCLUSION

The APE recommended for delineation in this memorandum considers the nature and scale of the proposed project, the existing built environment in which it will occur, and various ways in which the project could reasonably be demonstrated to affect historic properties. The APE provides an appropriate basis for taking the effects of the proposed Goethals Bridge Replacement Project on historic properties into account. The recommended APE boundary is shown on Figure 4.

# TECHNICAL MEMORANDUM

## GOETHALS BRIDGE REPLACEMENT EIS



**FIGURE 1: KEY TO PHOTO LOCATIONS**

# TECHNICAL MEMORANDUM

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## GOETHALS BRIDGE REPLACEMENT EIS



**PHOTO 1:  
Aerial View of the Goethals Bridge Looking Southwest toward  
Elizabeth-Linden**

# TECHNICAL MEMORANDUM

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## GOETHALS BRIDGE REPLACEMENT EIS



**PHOTO 2:  
View Southeast from the Elizabeth River toward the Goethals Bridge  
and Arthur Kill Lift Bridge**

# TECHNICAL MEMORANDUM

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## GOETHALS BRIDGE REPLACEMENT EIS



**PHOTO 3:**  
**View South on Front Street south of Elizabeth River--**  
**the Goethals Bridge and Arthur Kill Lift Bridge are partially visible in**  
**left background**

# TECHNICAL MEMORANDUM

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## GOETHALS BRIDGE REPLACEMENT EIS



**PHOTO 4:  
View Southeast on Bayway Avenue east of Burlington Avenue  
toward Goethals Bridge (partially visible)**

# TECHNICAL MEMORANDUM

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## GOETHALS BRIDGE REPLACEMENT EIS



**PHOTO 5:  
Looking Southeast from the 5th Avenue side of Mattano Park toward  
the Goethals Bridge and Arthur Kill Lift Bridge**

# TECHNICAL MEMORANDUM

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## GOETHALS BRIDGE REPLACEMENT EIS



**PHOTO 6:**  
**Looking Southeast from a Citgo Station on Bayway Avenue near  
Pulaski Street, looking toward Goethals Bridge**

# TECHNICAL MEMORANDUM

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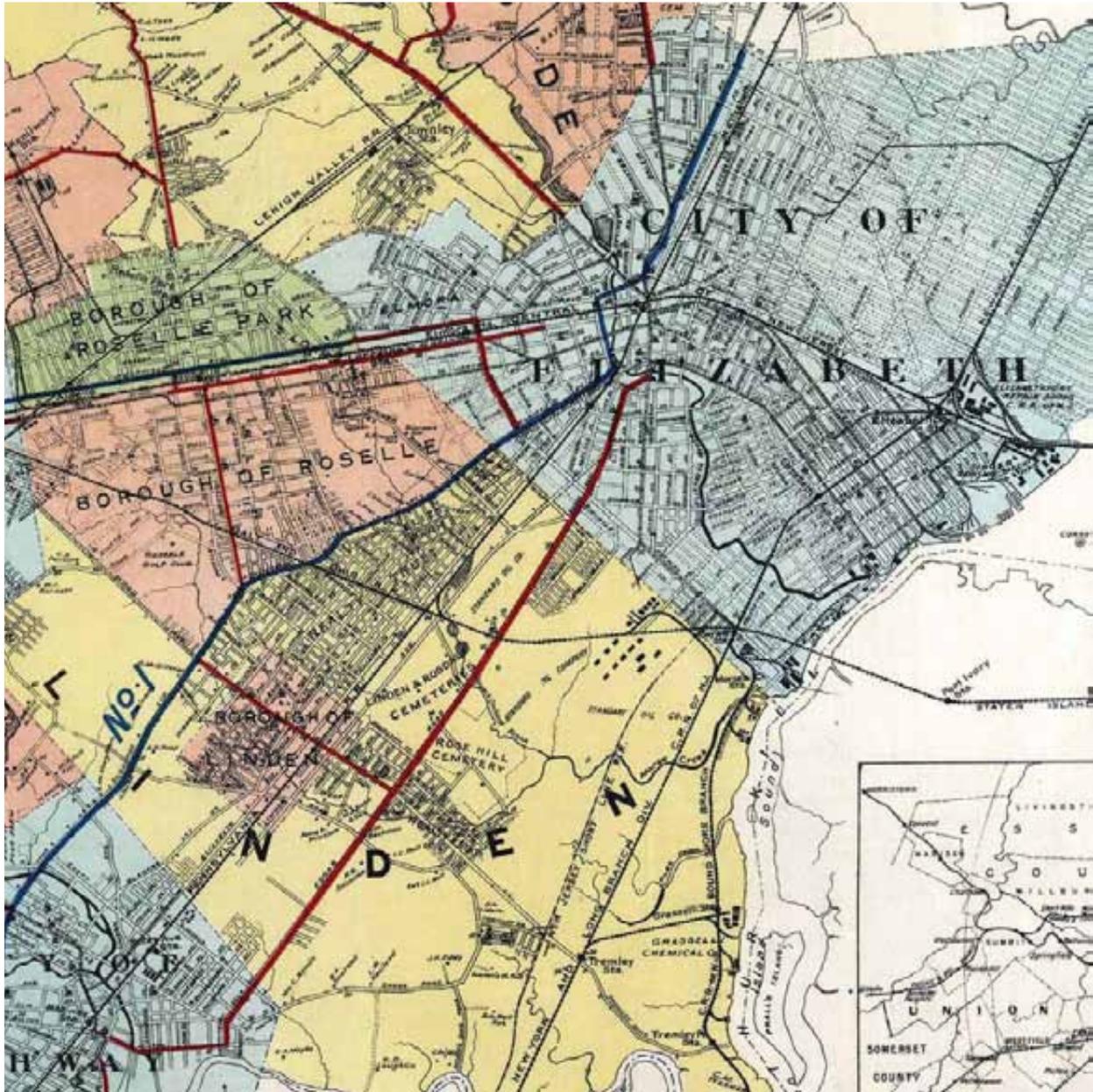
## GOETHALS BRIDGE REPLACEMENT EIS



**PHOTO 7:  
View Southeast along Richmond Street from Pulaski Street  
looking toward the Goethals Bridge**

# TECHNICAL MEMORANDUM

## GOETHALS BRIDGE REPLACEMENT EIS

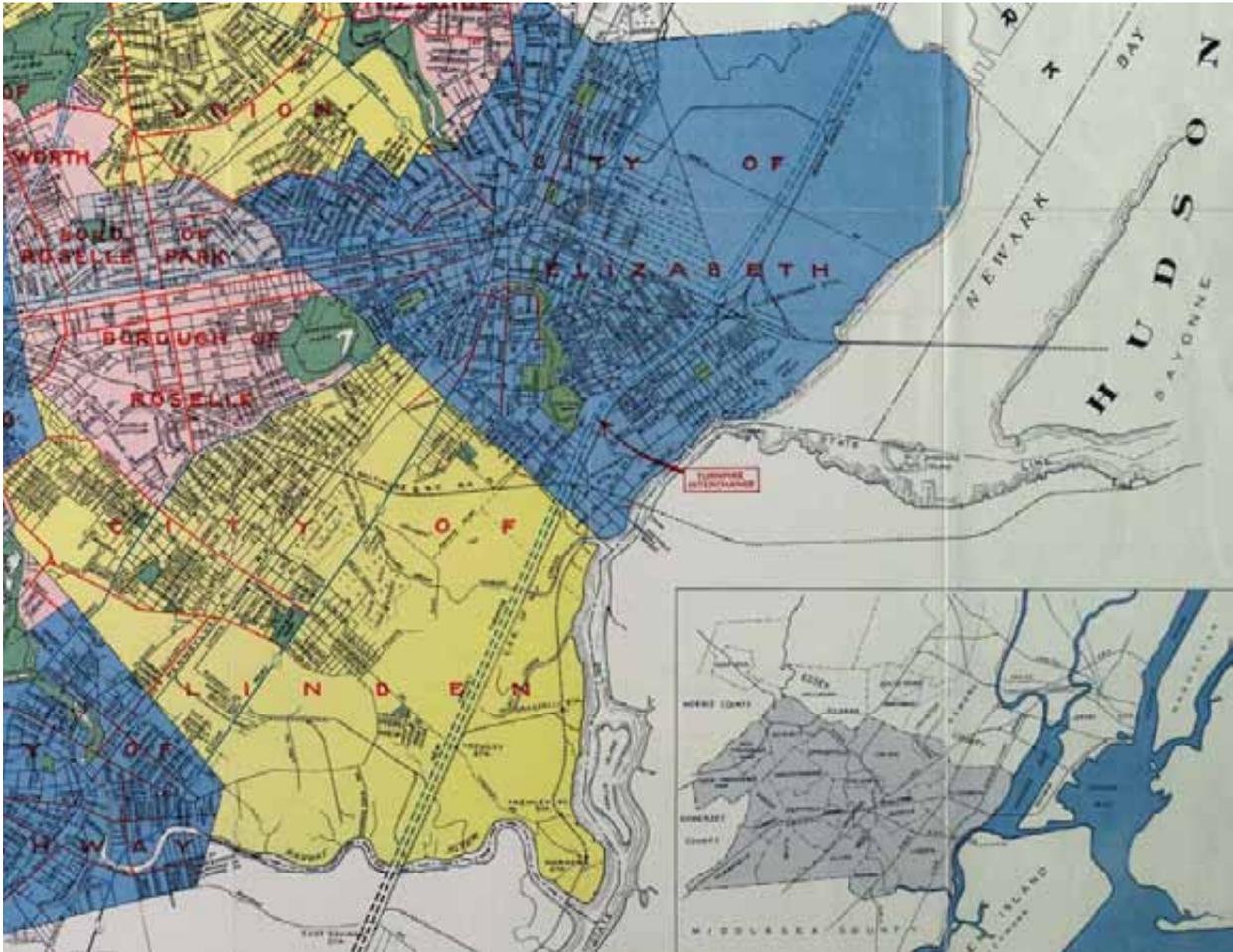


Source: <http://mapmaker.rutgers.edu>

**FIGURE 2: MAP OF UNION COUNTY, NEW JERSEY (1923)**

# TECHNICAL MEMORANDUM

## GOETHALS BRIDGE REPLACEMENT EIS



Source: <http://mapmaker.rutgers.edu>

**FIGURE 3: MAP OF UNION COUNTY, NEW JERSEY (1951)**

# TECHNICAL MEMORANDUM

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## GOETHALS BRIDGE REPLACEMENT EIS



**Photo 8:  
Panoramic View North of the NJ Turnpike and waterfront from  
Tremley Point Bridge**

# TECHNICAL MEMORANDUM

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## GOETHALS BRIDGE REPLACEMENT EIS



**PHOTO 9:  
View North from Grasselli Road toward Linden Generating Station**

# TECHNICAL MEMORANDUM

## GOETHALS BRIDGE REPLACEMENT EIS



Figure 4: Recommended APE Boundary



Number of pages including coversheet: 3

JON S. CORZINE  
Governor

State of New Jersey  
DEPARTMENT OF ENVIRONMENTAL PROTECTION  
Natural and Historic Resources, Historic Preservation Office  
PO Box 404, Trenton, NJ 08625  
TEL: (609) 292-2023 FAX: (609) 984-0578  
www.state.nj.us/dep/hpo

LISA P. JACKSON  
Commissioner

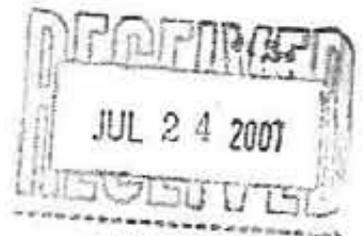
DATE: July 24, 2007

FAX TRANSMITTAL FORM

TO: Gary Kassof  
FAX: 212/668-7967

FROM: Andrea Tingey  
PHONE: 609/984-0539

RE: HPO-G2007-120  
Goethals APE



If you have received this fax in error or if there is a problem with the transmission, please contact Sara Homer at 609/292-0061



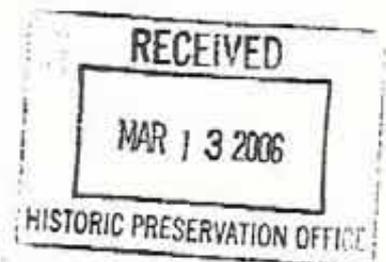
Commander  
First Coast Guard District

Battery Park Bldg.  
One South Street  
New York, NY 10004-5073  
Staff Symbol: (dpg)  
Phone: (212) 668-7195  
Fax: (212) 668-7967  
Email

05-0030-4AT  
HPO-62007-120  
chrano

16591//Goethals Bridge  
March 10, 2006

Ms. Dorothy Guzzo  
Deputy State Historic Preservation Officer  
New Jersey Department of Environmental Protection  
Historic Preservation Office  
P.O. Box 404  
Trenton, NJ 08625-0404



Re: Goethals Bridge EIS/Area of Potential Effect

Dear Ms. Guzzo:

The U.S. Coast Guard thanks you and Andrea Tingey for her recent input via e-mail on December 7, 2005, in which she defined suggested limits of the Area of Potential Effect (APE) for the above-ground cultural (historical/architectural) resources of subject project on the New Jersey side of the project corridor.

Ms. Tingey's transmittal followed our joint field trip to the Goethals Bridge and its environs on October 17, 2005, and my subsequent transmittal to your office of a set of alignment concept drawings for the four build alternatives that are currently being advanced through the alternatives screening process, to identify those that will be studied in greater detail in the Draft Environmental Impact Statement (DEIS). Berger/PB, our environmental consultant team, also forwarded to Ms Tingey, digital photos taken of the study area during our joint field trip, including those locations of historic interest that have views of the bridge.

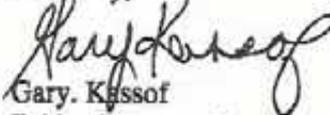
Since the suggested APE defined by Ms. Tingey differed somewhat from our initial submittal to your office, we enlisted the assistance of an architectural historian to assist us in better defining the limits of an appropriate APE. A windshield survey of the project area, including areas within the APE suggested by your office was conducted with the historian and with other Berger/PB cultural resources staff. Based upon the survey and previous documentation, the enclosed *Technical Memorandum on Consideration of the Area of Potential Effect for Historical/Architectural Resources* was developed. The memorandum addresses considerations of the nature and scale of the proposed project, various ways in which the proposed project could reasonably be demonstrated to affect historic properties, and the existing built environment in which the project would be implemented.

On this basis, we feel that the limits of the APE should be defined, as shown on the aerial map in the enclosed memorandum as it represents a reasonable area of potential effect for this undertaking. I have directed Berger/PB to undertake the identification of historic/architectural

properties within this APE and to prepare the inventory documentation for subsequent transmittal to your office for review.

The U.S. Coast Guard looks forward to your continued involvement in this EIS process and associated Section 106 consultation process. Please call me at 212-668-7021 if you have any questions or comments concerning the enclosed information and our determination of the limits of the APE for this project.

Sincerely,



Gary Kasso

Bridge Program Manager

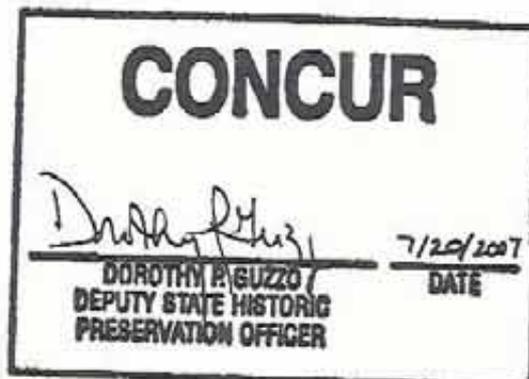
First Coast Guard District

By direction of the District Commander

Enclosures:

*Technical Memorandum, Consideration of the Area of Potential Effect for Historical/Architectural Resources*

Copy: Andrea Tingey (NJHPO)  
Michelle Hughes (NJHPO)  
James Warren (NYSHPO)  
Ken Hess (Berger/PB)  
Judith Versenyi (Berger/PB)  
Esther Schwalb (Berger/PB)  
Marty Bowers (Berger/PB)  
Sara Moss (Berger/PB)



# **APPENDIX BB**

## **NJHPO SURVEY FORMS**

# BASE FORM

Historic Sites #:

**Property Name:** 2710 Allen Street Ext., Building D

**Street Address:** Street #: 2710 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Allen Suffix: EXT Type: ST

**County(s):** Union **Zip Code:** 07036

**Municipality(s):** Linden City **Block(s):** 586

**Local Place Name(s):** Linden **Lot(s):** 4

**Ownership::** Private **USGS Quad(s)** Elizabeth

**Description:** see industry attachment

**Registration and Status Dates:** National Historic Landmark: \_\_\_\_\_ SHPO Opinion: \_\_\_\_\_  
National Register: \_\_\_\_\_ Local Designation: \_\_\_\_\_  
New Jersey Register: \_\_\_\_\_ Other Designation: \_\_\_\_\_  
Determination of Eligibility: \_\_\_\_\_ Other Designation Date: \_\_\_\_\_

**Photograph:**

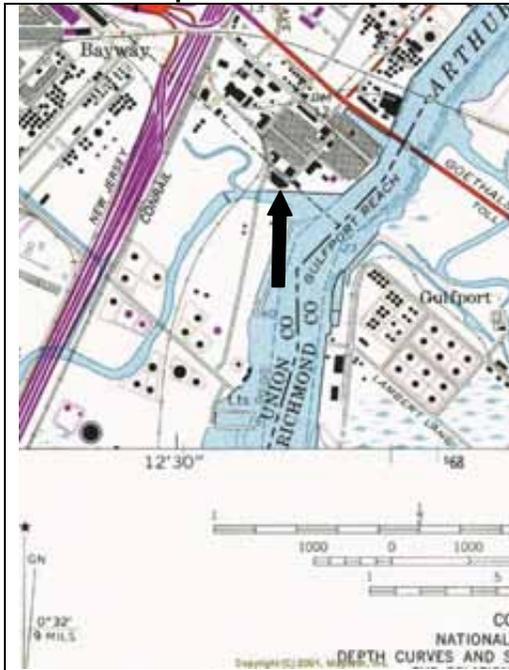


Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

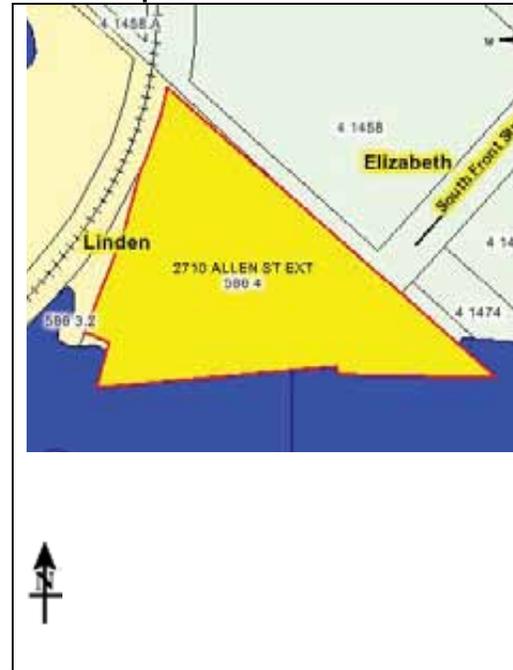
# BASE FORM

Historic Sites #:

Location Map:



Site Map:



### Bibliography/Sources:

Sanborn Fire Insurance Maps. Elizabeth, NJ: 1922/1923 and 1950.  
Maptech. Terrain Navigator, Version 7.03a, 1996-2006.  
Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at  
<http://ucgis.ucnj.org/gismaps/onpoint>.

Additional Information: N/A

More Research Needed?  Yes  No

### INTENSIVE LEVEL USE ONLY

Attachments Included:  Building  Structure  Object  Bridge  
 Landscape  Industry

Within Historic District?  Yes  No

Status:  Key-Contributing  Contributing  Non-Contributing

Associated Archaeological Site/Deposit?  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## INDUSTRIAL BUILDING ATTACHMENT

Historic Sites #:

---

**Common Name:** 2710 Allen Street Ext., Building D

**Historic Name:** Unknown

**Present Use:** Unknown

**Historic Industry:** Unknown **Building ID:** E

**Construction Date:** c. 1920 **Source:** Stylistic and material evidence

**Alteration Date(s):** N/A **Source:** N/A

**Designer:** Unknown **Physical Condition:** Good

**Builder:** Unknown **Remaining Historic Fabric:** Good

**Style:** N/A

**Length:** N/A **Stories:** 3

**Width:** N/A **Bays:** 14

**Exterior Finish Materials:** Other

**Foundation Materials:** Other

**Structural System:** \_\_\_\_\_ **Roof System:** \_\_\_\_\_

**Roof Finish Materials:** Unknown

**Equipment/Machinery:** Unknown

**Transportation Links:** Loading dock, rail siding

**Exterior Description:** This large concrete-block warehouse stands three stories tall with a 14-bay-wide northeast elevation along Allen Street. The northwest elevation is two bays wide. The elevation facing the river was not accessible; however, it does appear to have more than two bays. Each elevation is divided into bays with full-height stucco-clad concrete piers. The first story contains several loading bays with overhead-roll metal doors. Several former loading bays have been in-filled with concrete blocks. The center three loading bays are sheltered by a corrugated-metal pent roof. Various-sized industrial metal windows illuminate the warehouse, including 9, 12, 16, and 20-light windows and 16x12x16 tripartite windows. Several of the window bays that hold the smaller-sized windows appear to have originally held the tripartite windows but have been sized down and in-filled with concrete blocks. In addition, the loading bays on the second and third stories located on the northeast and southwest elevations are in-filled with concrete blocks. Many of the windows feature concrete sills and many of the second and third-story bays are defined with concrete sills or string courses. The roof the building is flat and features a concrete parapet.

**Interior Description:** N/A

**Setting:** This property is located in an area that has been highly industrialized since the 1910s and 1920s. The building on this property is situated with one elevation along the shoreline of Arthur Kill. There is no vegetation on the property and the entire area outside of the building is paved with asphalt.

---

Survey Name: Goethals Bridge Replacement Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** The shoreline between Perth Amboy and Elizabeth in Union County, New Jersey, developed during the last half of the nineteenth century from garden market truck farms into industrial land, becoming home to chemical and fertilizer companies, iron works, and warehouses tied to the shipping industry. Access by several railroads, the shipping ports, and the growth of New York and Newark made the area into a prime outlet for industrial development. The late nineteenth-century industries were replaced during the early twentieth century with oil refineries and as well as plants focused on the production of steel, chrome, and tin. During the mid-twentieth century with the advent of World War II, large tank farms were established along Arthur Kill's shorelines providing storage for gas, oil, and chemicals.

The property at 2710 Allen Street Extension in Linden is associated with the New Jersey "Immigration and Agricultural, Industrial, Commercial, and Urban Expansion" context.

**Significance:** The property at 2710 Allen Street Extension is a typical vernacular industrial/warehouse building dating to circa 1920 in Union County, New Jersey. This modest building lacks any embellishments and is not an uncommon form or building type within the area.

**Eligibility for New Jersey**

**and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:**

The building at 2710 Allen Street Extension does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1920, the vernacular industrial building at 2710 Allen Street Extension does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 2710 Allen Street Extension is unlikely to provide information important to our understanding of history (Criterion D).

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

Industrial Attachment

**Narrative Boundary Description:** This property is located at 2710 Allen Street Extension in Linden, Union County. The triangular-shaped property is associated with block 586 and lot 4. It is bounded to the northeast by Allen Street Extension, to the south by Arthur Kill, and to the northwest by a railroad line.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

2710 Allen Street Ext., Building D, Linden, Union County



North Elevation, Facing Southwest



South Elevation, Facing Northeast

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** Carringer Road Bridge over Morses Creek

**Street Address:** Street #: \_\_\_\_\_ (Low) \_\_\_\_\_ (High) Apartment #: \_\_\_\_\_ (Low) \_\_\_\_\_ (High)

Prefix: \_\_\_\_\_ Street Name: \_\_\_\_\_ Suffix: \_\_\_\_\_ Type: \_\_\_\_\_

**County(s):** Union \_\_\_\_\_ **Zip Code:** 07036 \_\_\_\_\_

**Municipality(s):** Linden City \_\_\_\_\_ **Block(s):** N/A \_\_\_\_\_

**Local Place Name(s):** Linden \_\_\_\_\_ **Lot(s):** N/A \_\_\_\_\_

**Ownership::** Private \_\_\_\_\_ **USGS Quad(s)** Elizabeth \_\_\_\_\_

**Description:** The bridge carrying Carringer Road over Morses Creek consists of a 12-span timber stringer structure. Poured concrete abutments support the bridge's northeast end. Eleven wood bents composed of paired pilings driven into the creek bed and connected together by angled cross-ties support the bridge's deck of railroad tie crossbeams. Close inspection of the ridge was limited by chain link fences.

**Registration and Status Dates:** National Historic Landmark: \_\_\_\_\_ SHPO Opinion: \_\_\_\_\_  
National Register: \_\_\_\_\_ Local Designation: \_\_\_\_\_  
New Jersey Register: \_\_\_\_\_ Other Designation: \_\_\_\_\_  
Determination of Eligibility: \_\_\_\_\_ Other Designation Date: \_\_\_\_\_

**Photograph:**

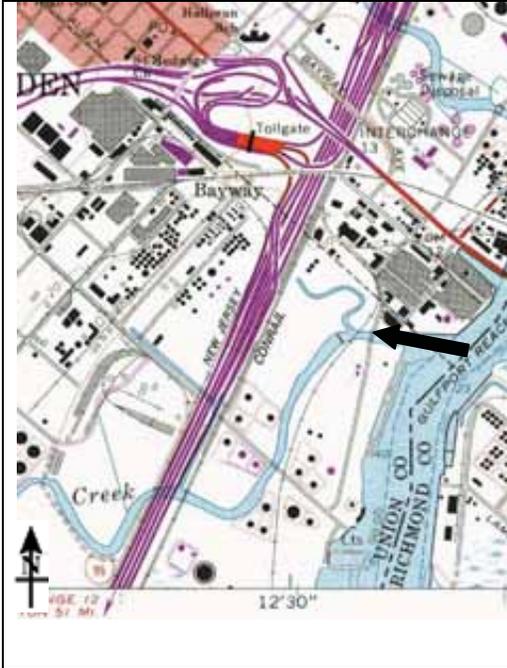


Survey Name: Goethals Bridge Replacement Project Date: May 3, 2007  
Surveyor: Stuart Paul Dixon/Senior Architectural Historian  
Organization: The Louis Berger Group, Inc., East Orange, New Jersey

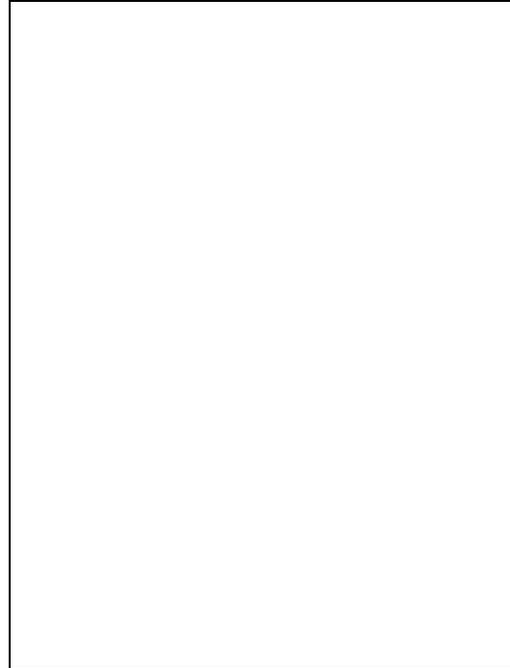
# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:**

**Additional Information:**

More Research Needed?  Yes  No

**INTENSIVE LEVEL USE ONLY**

Attachments Included:  Building  Structure  Object  Bridge  
 Landscape  Industry

Within Historic District?  Yes  No

Status:  Key-Contributing  Contributing  Non-Contributing

Associated Archaeological Site/Deposit?  Yes

---

Survey Name: Goethals Bridge Replacement Project Date: May 3, 2007  
Surveyor: Stuart Paul Dixon/Senior Architectural Historian  
Organization: The Louis Berger Group, Inc., East Orange, New Jersey

## BRIDGE ATTACHMENT

Historic Sites #:

**Common Name:** Carringer Road Bridge over Morses Creek  
**Historic Name:** Unknown  
**Feature Carried:** Carringer Road  
**Feature Crossed:** Morses Creek **Milepost:** Unknown  
**Owner/Operator:** Unknown **SI&A Structure Number**                       
**Construction Date:** ca. 1950 **Source:** Field visit  
**Alteration Date(s):**                      **Source:**                       
**Engineer** Unknown **Physical Condition:** good  
**Builder:** Unknown **Remaining Historic Fabric:** Low  
**Type:** Stringer  
**Design:**                      **Spans:** 12  
**Material:** Wood **Length:** Est. 100 ft.  
**Patent Holder:** Unknown **Width:** Est. 8 ft.  
**Patent Date:** Unknown

**Description:** The bridge carrying Carringer Road over Morses Creek consists of a 12-span timber stringer structure. Poured concrete abutments support the bridge's northeast end. Eleven wood bents composed of paired pilings driven into the creek bed and connected together by angled cross-ties support the bridge's deck of railroad tie crossbeams. Close inspection of the bridge was limited by chain link fences.

**Setting:** The area surrounding Morses Creek consists of tidal wetlands with heavy vegetation intermingled with industrial wastelands. An oil refinery is located south of the bridge. Immediately northeast of the bridge stands a large manufacturing/warehouse complex. The area has been highly industrialized since the early twentieth century. The Arthur Kill is located roughly 100 yards east of the bridge. A second bridge carrying a railroad spur over the creek is located roughly 50 yards west of the bridge.

---

Survey Name: Goethals Bridge Replacement Project Date: May 3, 2007  
Surveyor: Stuart Paul Dixon/Senior Architectural Historian  
Organization: The Louis Berger Group, Inc., East Orange, New Jersey

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** Historic research has not determined a construction date for the bridge. This area of Linden experienced growth as an industrial section during the late nineteenth century. Early twentieth century maps of the area do not depict a bridge carrying Carringer Road over the creek. Based upon its materials, construction of the current bridge structure probably occurred around 1950.

**Significance:** The bridge is an undistinguished example of a common bridge type. Research has not identified any significant historical associations.

**Eligibility for New Jersey**

**and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** Research has not indicated the bridge possesses significant associations with important events, persons, or historic trends (Criteria A and B). The Carringer Road bridge over Moses Creek is not a noteworthy example of a bridge type or the design of a master engineer (Criterion C). The bridge is unlikely to provide information important to our understanding of history (Criterion D).

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

Base, Bridge

**Narrative Boundary Description:**

Survey Name: Goethals Bridge Replacement Project

Date: May 3, 2007

Surveyor: Stuart Paul Dixon/Senior Architectural Historian

Organization: The Louis Berger Group, Inc., East Orange, New Jersey

## CONTINUATION SHEET

Historic Sites #:

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Carringer Road Bridge over Morses Creek, Linden, Union County



Facing Northeast

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Survey Name: Goethals Bridge Replacement Project

Date: May 3, 2007

Surveyor: Stuart Paul Dixon/Senior Architectural Historian

Organization: The Louis Berger Group, Inc., East Orange, New Jersey

# BASE FORM

Historic Sites #:

**Property Name:** Railroad Bridge over Morses Creek

**Street Address:** Street #: \_\_\_\_\_ (Low) \_\_\_\_\_ (High) Apartment #: \_\_\_\_\_ (Low) \_\_\_\_\_ (High)

Prefix: \_\_\_\_\_ Street Name: \_\_\_\_\_ Suffix: \_\_\_\_\_ Type: \_\_\_\_\_

**County(s):** Union \_\_\_\_\_ **Zip Code:** 07036 \_\_\_\_\_

**Municipality(s):** Linden City \_\_\_\_\_ **Block(s):** N/A \_\_\_\_\_

**Local Place Name(s):** Linden \_\_\_\_\_ **Lot(s):** N/A \_\_\_\_\_

**Ownership::** Private \_\_\_\_\_ **USGS Quad(s)** Elizabeth \_\_\_\_\_

**Description:** The railroad bridge over Morses Creek consists of an eight-span timber stringer structure. Steel I-beams span between the wood bents and carry wood railroad ties supporting welded rail spikes to the ties. Currently abandoned, the bridge displays well-rusted rails and a chain link gate blocking its southern end. Access to the bridge's north approach is hindered by overgrown vegetation and piles of construction debris. The bridge also carries a metal and a plastic pipe along its west side.

**Registration and Status Dates:** National Historic Landmark: \_\_\_\_\_ SHPO Opinion: \_\_\_\_\_  
National Register: \_\_\_\_\_ Local Designation: \_\_\_\_\_  
New Jersey Register: \_\_\_\_\_ Other Designation: \_\_\_\_\_  
Determination of Eligibility: \_\_\_\_\_ Other Designation Date: \_\_\_\_\_

**Photograph:**

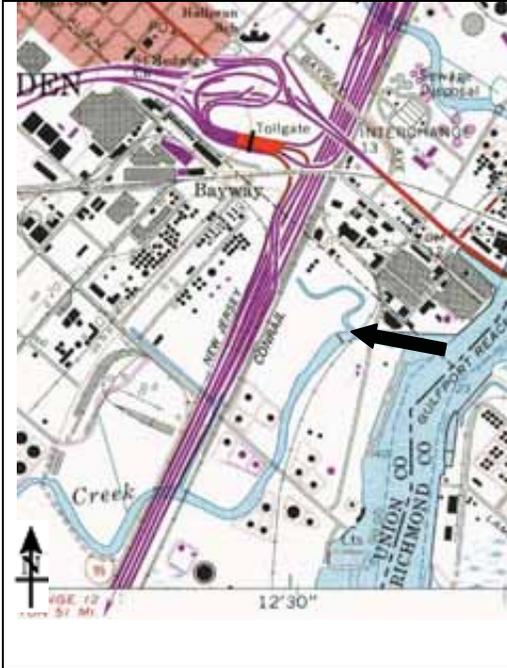


Survey Name: Goethals Bridge Replacement Project Date: May 3, 2007  
Surveyor: Stuart Paul Dixon/Senior Architectural Historian  
Organization: The Louis Berger Group, Inc., East Orange, New Jersey

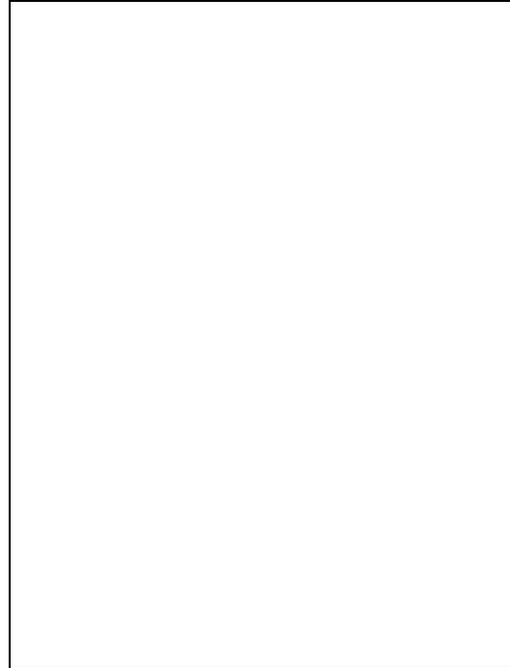
# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:**

**Additional Information:**

More Research Needed?  Yes  No

**INTENSIVE LEVEL USE ONLY**

Attachments Included:  Building  Structure  Object  Bridge  
 Landscape  Industry

Within Historic District?  Yes  No

Status:  Key-Contributing  Contributing  Non-Contributing

Associated Archaeological Site/Deposit?  Yes

---

Survey Name: Goethals Bridge Replacement Project Date: May 3, 2007  
Surveyor: Stuart Paul Dixon/Senior Architectural Historian  
Organization: The Louis Berger Group, Inc., East Orange, New Jersey



## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** The Central Railroad of New Jersey probably erected a bridge over Morses Creek at or near the site of the present structure around 1890. United States Geological Survey maps from 1891 depict a bridge spanning the creek in that year. Sanborn insurance maps from the 1920s label the rail spur as part of the Central Rail Road of New Jersey's Shore Line. The spur, however, ended at the industrial complex located at the south side of Morses Creek. The use of modern timbers and welded rail indicate the bridge was extensively remodeled, probably during the 1990s, a period when welded rail came into common use.

**Significance:** The bridge is an undistinguished and altered example of a common historic bridge type. Research has not identified any significant historical associations.

**Eligibility for New Jersey**

**and National Registers:**

Yes  No

**National**

**Register Criteria:**

A  B  C  D

**Level of Significance**

Local  State  National

**Justification of Eligibility/Ineligibility:** Research has not indicated the bridge possesses significant associations with important events, persons, or historic trends (Criteria A and B). The railroad bridge over Morses Creek is not a noteworthy example of a bridge type or the design of a master engineer (Criterion C). The bridge is unlikely to provide information important to our understanding of history (Criterion D).

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

Base, Bridge

**Narrative Boundary Description:**

---

Survey Name: Goethals Bridge Replacement Project Date: May 3, 2007  
Surveyor: Stuart Paul Dixon/Senior Architectural Historian  
Organization: The Louis Berger Group, Inc., East Orange, New Jersey

## CONTINUATION SHEET

Historic Sites #:

Railroad Bridge over Morses Creek, Linden, Union County



South Approach, Facing Northeast

Survey Name: Goethals Bridge Replacement Project

Date: May 3, 2007

Surveyor: Stuart Paul Dixon/Senior Architectural Historian

Organization: The Louis Berger Group, Inc., East Orange, New Jersey

# BASE FORM

Historic Sites #:

**Property Name:** 735-757 South Front Street

**Street Address:** Street #: 735 757 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: S Street Name: Front Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07207

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 1458

**Ownership::** Private **USGS Quad(s)** Elizabeth

**Description:** The complex of buildings located at 735-757 South Front Street consists of a large three-story warehouse, 23 bays wide on its north elevation and five bays deep, and marked as building "E" by signs applied to its corners, with a one-story, gable-roofed addition on its eastern elevation, and a one-story hyphen appended to the central portion of its southern elevation connecting the warehouse to a two-story monitor-roofed building with a two-story gable-roofed wing extending from its southern elevation.

**Registration and Status Dates:** National Historic Landmark: \_\_\_\_\_ SHPO Opinion: \_\_\_\_\_  
National Register: \_\_\_\_\_ Local Designation: \_\_\_\_\_  
New Jersey Register: \_\_\_\_\_ Other Designation: \_\_\_\_\_  
Determination of Eligibility: \_\_\_\_\_ Other Designation Date: \_\_\_\_\_

## Photograph:

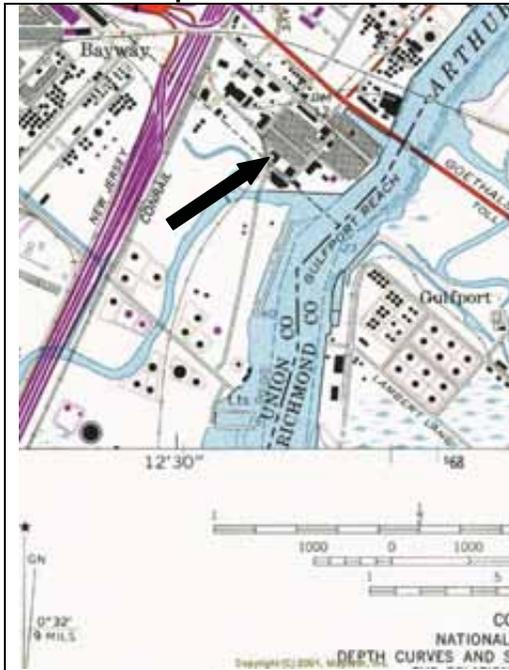


Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

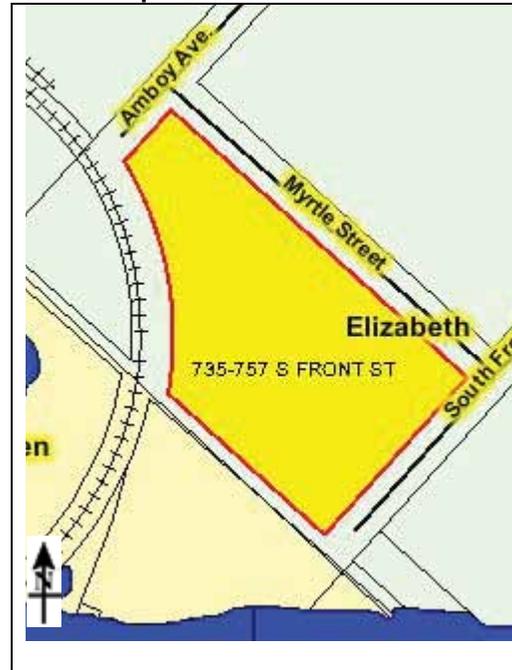
# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Sanborn Fire Insurance Maps. Elizabeth, NJ; 1922/1923 and 1950.  
Maptech. Terrain Navigator, Version 7.03a, 1996-2006.  
Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at  
<http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

## INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# INDUSTRIAL BUILDING ATTACHMENT

Historic Sites #:

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**Common Name:** TGM Myrtle Warehouse

**Historic Name:** Unknown

**Present Use:** Unknown

**Historic Industry:** Unknown **Building ID:** E

**Construction Date:** c. 1919 **Source:** Stylistic Evidence

**Alteration Date(s):** N/A **Source:** N/A

**Designer:** Unknown **Physical Condition:** Fair

**Builder:** Unknown **Remaining Historic Fabric:** High

**Style:** N/A

**Length:** N/A **Stories:** 3

**Width:** N/A **Bays:** 23

**Exterior Finish Materials:** Other

**Foundation Materials:** Other

**Structural System:** Load-bearing walls **Roof System:** Unknown

**Roof Finish Materials:** Unknown

**Equipment/Machinery:** Unknown

**Transportation Links:** Loading dock, rail siding

**Exterior Description:** Built in 1919, Building E features concrete-block walling, a somewhat irregular footprint, a poured concrete foundation, and a flat roof. In 1923 this warehouse stored cotton and then in 1950 was used for oil storage (Sanborn 1923, 1950). The east and west elevations possess five bays and the entire building is symmetrically divided into bays with stucco-clad concrete piers. Large loading bays originally pierced nearly all first-story bays. Most have since been in-filled with concrete blocks and some now feature metal slab pedestrian doors. Loading doors that remain consist of either fiberglass or metal overhead roll doors. Upper story bays primarily contain six-light metal windows with four-light operable awning windows, most with concrete block infill surrounds. Each of the window bays feature concrete sills, most of which span the full width between the bays of the concrete piers. Above the first story is a wide punctuated stringcourse. The center two bays of the north and south elevations feature are crowned by a pedimented parapet flanked by thinner but taller pedimented parapets. *See continuation sheet*

**Interior Description:** N/A

**Setting:** This property is located in an area that has been highly industrialized since the 1910s and 1920s. The buildings on this property are situated adjacent to one another and to a railroad line. There is no vegetation on the property and the entire area outside of the buildings is paved with asphalt.

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** The shoreline between Perth Amboy and Elizabeth in Union County, New Jersey, developed during the last half of the nineteenth century from garden market truck farms into industrial land, becoming home to chemical and fertilizer companies, iron works, and warehouses tied to the shipping industry. Access by several railroads, the shipping ports, and the growth of New York and Newark made the area into a prime outlet for industrial development. The late nineteenth-century industries were replaced during the early twentieth century with oil refineries, and plants focused on the production of steel, chrome, and tin. During the mid-twentieth century with the advent of World War II, large tank farms were established along Arthur Kill's shorelines providing storage for gas, oil, and chemicals. The property at 735-757 South Front Street has a boiler and pump house, which was built circa 1930, and a 1919 warehouse that previously stored cotton (1920s) and oil (1950s). The property at 735-757 South Front Street in Elizabeth is associated with the New Jersey "Immigration and Agricultural, Industrial, Commercial, and Urban Expansion" context.

**Significance:** The property at 735-757 South Front Street in Elizabeth contains a typical vernacular warehouse from 1919 and a typical boiler and pump house (later connected with a hyphen) dating to circa 1930 in Union County, New Jersey. These modest buildings lack any embellishments and are not uncommon forms in the area. Several other buildings in the area are constructed of tile and were constructed during the first half of the twentieth century to be used as warehouses. The boiler room/pump house is the only historic example in the APE, but it is not an unusual form as boiler rooms/pump houses were necessary structures in many industries.

**Eligibility for New Jersey  
and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

### Justification of Eligibility/Ineligibility:

The buildings (currently all one building) at 735-757 South Front Street in Elizabeth do not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1919/1930, the vernacular industrial buildings at 735-757 South Front Street do not represent any distinctive characteristics of the period, nor do they represent the work of a master, or possess high artistic values (Criterion C). The property at 735-757 South Front Street is unlikely to provide information important to our understanding of history (Criterion D).

### For Historic Districts Only:

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

### For Individual Properties Only:

**List the completed attachments related to the property's significance:**

Industrial Attachment

**Narrative Boundary Description:** The property is located at 735-757 South Front Street in Elizabeth, Union County. It is associated with block 4 lot 1458. It is bounded to the northeast by Myrtle Street, to the northwest by Amboy Avenue and a rail line, to the southwest by Allen Street Extension, and to the southeast by South Front Street.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

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### Exterior Description, continued

Three bays on the south elevation are recessed by one bay, and a small shed-roof loading shelter is located in the angle of this recess. A second loading dock on the south elevation is sheltered with a corrugated-metal pent roof, a portion of which has been enclosed. Atop the eastern end of the warehouse is a metal water tank emblazoned with "Bayway Terminal Corp" on its sides.

The one-story addition appended to Building E's east elevation features five bays along its south and north elevations and six bays on its east elevation, all defined by poured concrete piers with five-to-one common bond brick spandrels. The addition features poured concrete sills and a raised foundation of the same material. The east elevation features two loading bays in-filled with concrete blocks and two single-leaf doors. The east and south elevations contain nine-light metal windows in which the bottom row of three lights are hopper windows. The windows are primarily located immediately under the wide eaves and feature poured-concrete sills. This one-story wing has a very shallow-pitched gable roof sheathed with corrugated metal. The north elevation has a two-bay-wide shed-roof porch set in the angle of the ell with the main building. The south elevation possesses a corrugated fiberglass-walled structure appended to its west end.

The one-story hyphen extending southward from the center of the warehouse's south elevation connects the warehouse to a former boiler and pump house built circa 1930. A fenced lot and parked truck trailers blocked views of the hyphen. In 1950 the former boiler and pump house was used by the Alpha Chemical Works and the Arlin Chemical Works. The main section stands two stories tall and three bays wide capped with a monitor roof. It is constructed of stuccoed tile block with stucco-clad concrete piers at the corners. Both the east and west gable-end elevations contain three bays of multi-light windows. Poured concrete piers divide the south elevation into five bays. Both the north and south sides of the monitor roof are entirely glazed with multi-light windows. A yellow brick chimney stack is appended to the boiler house's southwest corner. The south elevation of the main block also features a two-story, five-bay wing. The wing is constructed of stuccoed tile block with a poured concrete foundation capped with a corrugated-metal roof. The bays are divided by stucco-clad concrete piers. The wing's first-story east elevation features three fenestral openings per bay, most containing smaller industrial windows, and the second story contains single large multi-light industrial windows in each bay. The roof of the main structure and the wing both feature wide eaves with exposed purlins and rafters.

Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

735-757 South Front Street, Elizabeth, Union County



Building E, North Elevation, Facing Southeast



Building E, Facing Southwest

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

735-757 South Front Street, Elizabeth, Union County



Building E, South Elevation, Facing Northeast



Building E, Facing Northwest

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# CONTINUATION SHEET

Historic Sites #:

735-757 South Front Street, Elizabeth, Union County



Building E, Facing Southwest



South Elevation, Facing East

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

737-757 South Front Street, Elizabeth, Union County



Southeast Elevation, Facing Northwest

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 760-766 South Front Street

**Street Address:** Street #: 760 766 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: S Street Name: Front Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 1472 and 1474

**Ownership::** Private **USGS Quad(s)** Elizabeth

**Description:** see industry attachment

**Registration and Status Dates:** National Historic Landmark: \_\_\_\_\_ SHPO Opinion: \_\_\_\_\_  
National Register: \_\_\_\_\_ Local Designation: \_\_\_\_\_  
New Jersey Register: \_\_\_\_\_ Other Designation: \_\_\_\_\_  
Determination of Eligibility: \_\_\_\_\_ Other Designation Date: \_\_\_\_\_

**Photograph:**

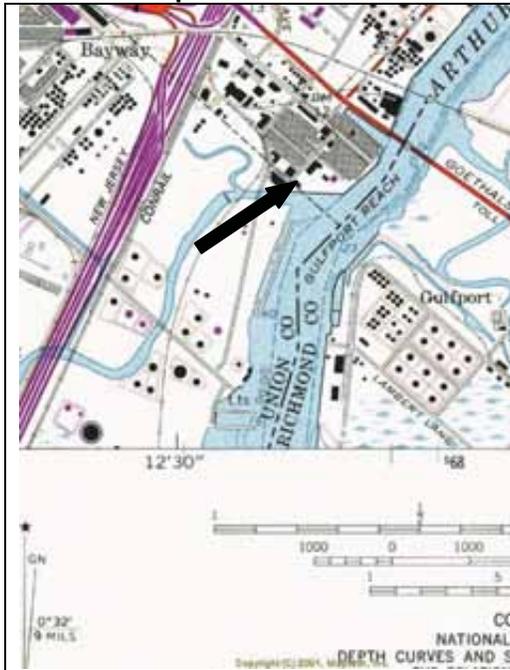


Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

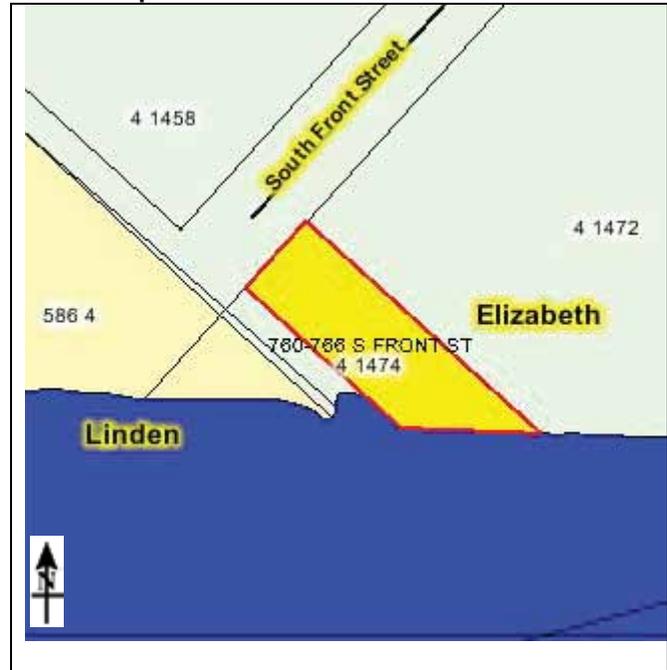
# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Sanborn Fire Insurance Maps. Elizabeth, NJ; 1922/1923 and 1950.  
Maptech. Terrain Navigator, Version 7.03a, 1996-2006.  
Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

## INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# INDUSTRIAL BUILDING ATTACHMENT

Historic Sites #:

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**Common Name:** 760-766 South Front Street

**Historic Name:** Unknown

**Present Use:** Unknown

**Historic Industry:** Unknown      **Building ID:** C

**Construction Date:** c. 1950      **Source:** Stylistic Evidence

**Alteration Date(s):** N/A      **Source:** N/A

**Designer:** Unknown      **Physical Condition:** Good

**Builder:** Unknown      **Remaining Historic Fabric:** High

**Style:** N/A

**Length:** N/A      **Stories:** 1

**Width:** N/A      **Bays:** 8

**Exterior Finish Materials** Concrete block

**Foundation Materials:** Other

**Structural System:** Steel Frame      **Roof System:** Unknown

**Roof Finish Materials:** Unknown

**Equipment/Machinery:** Unknown

**Transportation Links:** Loading dock, rail siding

**Exterior Description:** One story high, this circa 1950 concrete-block, steel-frame building set atop a poured concrete foundation features a rectangular footprint and an aluminum coping on its flat roof. Twenty bays penetrate the building's west elevation; eight bays occupy the south elevation. The building, marked by a metal sign on its northwest corner as a garage and building "C," exhibits 3-foot brick apron walls with a six-course American brick bond (12 courses total) between the foundation and the upper walls composed of concrete block. Windows in the upper section of the wall consist of six-light, metal hopper windows. The south elevation has two metal slab doors, one metal louvered vent, and three bays in-filled with plate metal. The west elevation has three metal vents, 16 six-light industrial windows, and one loading bay, which has been in-filled with concrete blocks.

A smaller, one-story concrete block building stands southeast of circa 1950 structure. Probably erected in the 1960s, the building features a flat roof with an aluminum coping and two bays penetrating both its north and west elevations. A metal overhead roll garage door and a metal slab pedestrian door occupy the north elevation bays. The west elevation displays a metal industrial sash and concrete block infill in its second window opening.

**Interior Description:** N/A

**Setting:** This building is sited on South Front Street in a highly industrialized area. There is no vegetation on the property, and the entire area outside of the building is paved with asphalt. A circa 1960 building stands to the southwest.

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Survey Name: Goethals Bridge Replacement      Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** The shoreline between Perth Amboy and Elizabeth in Union County, New Jersey developed during the last half of the nineteenth century from garden market truck farms into industrial land, becoming home to chemical and fertilizer companies, iron works, and warehouses tied to the shipping industry. Access by several railroads, the shipping ports, and the growth of New York and Newark made the area into a prime outlet for industrial development. The late nineteenth-century industries were replaced during the early twentieth century with oil refineries and as well as plants focused on the production of steel, chrome, and tin. During the mid-twentieth century with the advent of World War II, large tank farms were established along Arthur Kill's shorelines providing storage for gas, oil, and chemicals.

The property at 760-766 South Front Street in Elizabeth was listed as a factory building on the 1950 Sanborn Map of the area.

The property at 760-766 South Front Street in Elizabeth is associated with the New Jersey "Immigration and Agricultural, Industrial, Commercial, and Urban Expansion" context.

**Significance:** The property at 760-766 South Front Street in Elizabeth contains a typical vernacular factory building built circa 1950 in Union County, New Jersey. This modest building lacks any embellishments and is not an uncommon form or building type within the area.

**Eligibility for New Jersey**

**and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:**

The building at 760-766 South Front Street in Elizabeth do not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1950, the vernacular factory building at 760-766 South Front Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 760-766 South Front Street is unlikely to provide information important to our understanding of history (Criterion D).

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

Industrial Attachment

**Narrative Boundary Description:** The property is located at 760-766 South Front Street in Elizabeth, Union County. The building at 760-766 South Front Street is associated with map 4 lot 1472. It is bounded to the northwest by South Front Street and to the west by the Township boundary line and 2710 Allen Street Extension, and to the south and southeast by Arthur Kill.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

760-766 South Front Street, Elizabeth, Union County



Facing Northeast



Northwest Elevation, Facing Southeast

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 349-527 South Front Street

**Street Address:** Street #: 349 527 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: S Street Name: Front Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07206

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 1447

**Ownership::** Private **USGS Quad(s)** Elizabeth

**Description:** see industry attachment

**Registration and Status Dates:**  
National Historic Landmark: \_\_\_\_\_  
National Register: \_\_\_\_\_  
New Jersey Register: \_\_\_\_\_  
Determination of Eligibility: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_  
Local Designation: \_\_\_\_\_  
Other Designation: \_\_\_\_\_  
Other Designation Date: \_\_\_\_\_

**Photograph:**

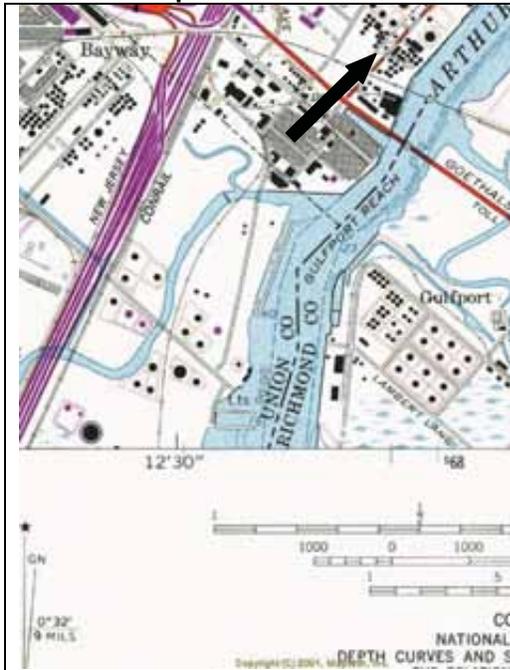


Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

**BASE FORM**

Historic Sites #:

**Location Map:**



**Site Map:**



**Bibliography/Sources:** Sanborn Fire Insurance Maps. Elizabeth, NJ; 1922/1923 and 1950.  
 Maptech. Terrain Navigator, Version 7.03a, 1996-2006.  
 Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at  
<http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

**INTENSIVE LEVEL USE ONLY**

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
 (Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
 Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
 Organization: The Louis Berger Group Inc.

## INDUSTRIAL BUILDING ATTACHMENT

Historic Sites #:

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<b>Common Name:</b>	<u>349-527 South Front Street</u>			
<b>Historic Name:</b>	<u>Unknown</u>			
<b>Present Use:</b>	<u>Unknown</u>			
<b>Historic Industry:</b>	<u>Unknown</u>	<b>Building ID:</b>	<u>N/A</u>	
<b>Construction Date:</b>	<u>c. 1924</u>	<b>Source:</b>	<u>Stylistic Evidence, Sanborn Maps</u>	
<b>Alteration Date(s):</b>	<u>c. 1940</u>	<b>Source:</b>	<u>1950 Sanborn Map</u>	
<b>Designer:</b>	<u>Unknown</u>	<b>Physical Condition:</b>	<u>Good</u>	
<b>Builder:</b>	<u>Unknown</u>	<b>Remaining Historic Fabric:</b>	<u>High</u>	
<b>Style:</b>	<u>N/A</u>			
	<b>Length:</b>	<u>N/A</u>	<b>Stories:</b>	<u>1</u>
	<b>Width:</b>	<u>N/A</u>	<b>Bays:</b>	<u>3</u>
<b>Exterior Finish Materials</b>	<u>Stucco</u>			
<b>Foundation Materials:</b>	<u>Other</u>			
<b>Structural System:</b>	<u>Unknown</u>	<b>Roof System:</b>	<u>Unknown</u>	
<b>Roof Finish Materials:</b>	<u>Unknown</u>			
<b>Equipment/Machinery:</b>	<u>Unknown</u>			
<b>Transportation Links:</b>	<u>Rail siding</u>			

**Exterior Description:** Built circa 1924, this L-shaped building holds a boiler room and is constructed of hollow tile. Covered with stucco, the building terminates in an asphalt-shingled shed roof with a parapet. The northeast elevation has an aluminum roll-up garage door, and the northwest elevation is pierced with a six-light metal window. The southeast elevation contains two two-light metal windows, and a single-leaf pedestrian door, which provides access to a boiler room. This building is depicted on the 1923 Sanborn map; however, the map states it is illustrated according to plans. By 1950 it had undergone an extension and an addition on the northeast elevation.

**Interior Description:** N/A

**Setting:** This building is sited on the north side of South Front Street in a highly industrialized area. There is no vegetation on the property, and it is covered with asphalt paving.

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** The shoreline between Perth Amboy and Elizabeth in Union County, New Jersey developed during the last half of the nineteenth century from garden market truck farms into industrial land, becoming home to chemical and fertilizer companies, iron works, and warehouses tied to the shipping industry. Access by several railroads, the shipping ports, and the growth of New York and Newark made the area into a prime outlet for industrial development. The late nineteenth-century industries were replaced during the early twentieth century with oil refineries and as well as plants focused on the production of steel, chrome, and tin. During the mid-twentieth century with the advent of World War II, large tank farms were established along Arthur Kill's shorelines providing storage for gas, oil, and chemicals.

The property at 534-538 South Front Street in Elizabeth is associated with the New Jersey "Immigration and Agricultural, Industrial, Commercial, and Urban Expansion" context.

**Significance:** The property at 534-538 South Front Street in Elizabeth contains a typical vernacular industrial building dating to circa 1950 in Union County, New Jersey. This modest building lacks any embellishments and is not an uncommon form within the area.

**Eligibility for New Jersey**

**and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:**

The building at 534-538 South Front Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1950, the vernacular industrial building at 534-538 South Front Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 534-538 South Front Street is unlikely to provide information important to our understanding of history (Criterion D).

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

Industrial Attachment

**Narrative Boundary Description:** The property is located 534-538 South Front Street in Elizabeth, Union County. The building at 534-538 South Front Street is associated with block 4 lot 1466. It is bounded to the northwest by South Front Street and to the southeast by Arthur Kill.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

349-527 South Front Street, 534-538 South Front Street, Elizabeth, Union County



349-527 South Front Street, Southeast Corner



534 South Front Street, Southwest Corner

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 346-532 South Front Street

**Street Address:** Street #: 346 532 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: S Street Name: Front Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 1464

**Ownership::** Private **USGS Quad(s)** Elizabeth

**Description:** see industrial attachment

**Registration and Status Dates:** National Historic Landmark: \_\_\_\_\_ SHPO Opinion: \_\_\_\_\_  
National Register: \_\_\_\_\_ Local Designation: \_\_\_\_\_  
New Jersey Register: \_\_\_\_\_ Other Designation: \_\_\_\_\_  
Determination of Eligibility: \_\_\_\_\_ Other Designation Date: \_\_\_\_\_

**Photograph:**

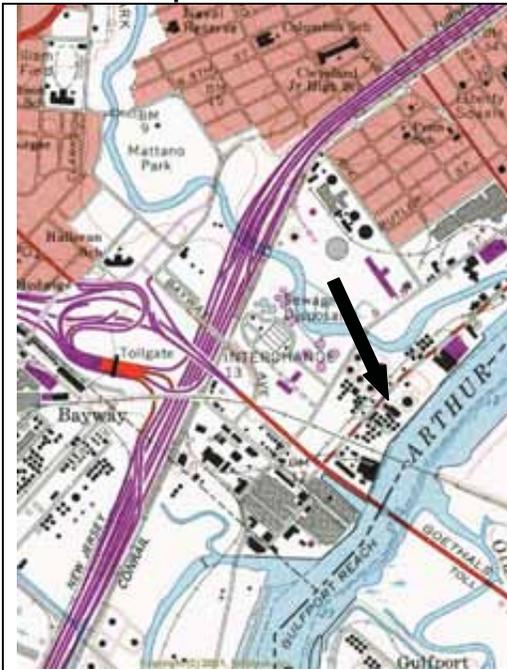


Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Sanborn Fire Insurance Maps. Elizabeth, NJ; 1922/1923 and 1950.  
Maptech. Terrain Navigator, Version 7.03a, 1996-2006.  
Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at  
<http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

## INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## INDUSTRIAL BUILDING ATTACHMENT

Historic Sites #:

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**Common Name:** 346-532 South Front Street

**Historic Name:** Unknown

**Present Use:** Unknown

**Historic Industry:** Unknown      **Building ID:** N/A

**Construction Date:** c. 1940      **Source:** Stylistic and material evidence, 1923/1950 Sanborn map

**Alteration Date(s):** N/A      **Source:** N/A

**Designer:** Unknown      **Physical Condition:** Good

**Builder:** Unknown      **Remaining Historic Fabric:** High

**Style:** N/A

**Length:** N/A      **Stories:** 3

**Width:** N/A      **Bays:** 4

**Exterior Finish Materials:** Other

**Foundation Materials:** Other

**Structural System:** Unknown      **Roof System:** Unknown

**Roof Finish Materials:** Unknown

**Equipment/Machinery:** Unknown

**Transportation Links:** Loading dock, rail siding

**Exterior Description:** Built circa 1940, the large three-story, concrete-block building featuring an L-shaped footprint terminates in a flat, concrete roof. On the rear elevation (southwest side) is a one-story, three-bay-wide by two-bay-deep section. Each elevation is defined by full-height poured concrete piers, which breaks up the wide concrete-block courses in relief at each story, similar to stringcourses. A variety of windows illuminates the building; however, the windows are primarily located on the second story with a few on the first story. Windows include aluminum fixed windows, glass blocks, 12-light units with six-light pivotal, and nine-light units with six-light awnings. The first story also contains a variety of entries, including metal overhead roll-up doors, double-leaf paneled wood doors, and single-leaf flush metal doors.

**Interior Description:** N/A

**Setting:** This building is sited on the south side of South Front Street in a highly industrialized area. There is no vegetation on the property, and it is covered with asphalt paving.

## INDUSTRIAL BUILDING ATTACHMENT

Historic Sites #:

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**Common Name:** 349-527 South Front Street

**Historic Name:** Unknown

**Present Use:** Light industrial

**Historic Industry:** Other **Building ID:** N/A

**Construction Date:** c. 1920 **Source:** Material and stylistic evidence

**Alteration Date(s):** N/A **Source:** N/A

**Designer:** Unknown **Physical Condition:** Good

**Builder:** Unknown **Remaining Historic Fabric:** High

**Style:** None

**Length:** N/A **Stories:** 1

**Width:** N/A **Bays:** 3

**Exterior Finish Materials:** Other

**Foundation Materials:** Other

**Structural System:** Unknown **Roof System:** Unknown

**Roof Finish Materials:** Other

**Equipment/Machinery:** Unknown

**Transportation Links:** Loading dock, rail siding

**Exterior Description:** This building is a one-story, circa 1920 machine shop now used as a garage. Built of concrete blocks, this small three bay-building is capped with a shallow gable roof sheathed with rolled asphalt. The symmetrical southwest facade features one central overhead roll-up replacement garage door flanked with 20-light windows with six-light pivotal units. A smaller one-story shed-roof section is located on the southeast elevation. This section is labeled as a storage area and has one single-leaf flush metal door.

**Interior Description:** N/A

**Setting:** This building is located to the northeast of 346 South Front Street. Both buildings are sited on the south side of South Front Street in a highly industrialized area. There is no vegetation on the property, and it is covered with asphalt paving.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** The shoreline between Perth Amboy and Elizabeth in Union County, New Jersey, developed during the last half of the nineteenth century from garden market truck farms into industrial land, becoming home to chemical and fertilizer companies, iron works, and warehouses tied to the shipping industry. Access by several railroads, the shipping ports, and the growth of New York and Newark made the area into a prime outlet for industrial development. The late nineteenth-century industries were replaced during the early twentieth century with oil refineries and as well as plants focused on the production of steel, chrome, and tin. During the mid-twentieth century with the advent of World War II, large tank farms were established along Arthur Kill's shorelines providing storage for gas, oil, and chemicals.

The main building at 346-532 South Front Street in Elizabeth, Union County, was built circa 1940 as a storage warehouse. This building replaced three structures: a cooper shop, barrel storage, and a wash house. Also associated with this property is a circa 1920 machine shop.

The property at 346-532 South Front Street is associated with the New Jersey "Immigration and Agricultural, Industrial, Commercial, and Urban Expansion" context.

**Significance:** The property at 346-532 South Front Street contains a typical vernacular warehouse and machine shop dating to circa 1940 and 1920 respectively in Union County, New Jersey. These modest buildings lack any embellishments and are not uncommon forms or building types within the area.

**Eligibility for New Jersey**

**and National Registers:**

Yes  No

**National**

**Register Criteria:**

A  B  C  D

**Level of Significance**

Local  State  National

**Justification of Eligibility/Ineligibility:** The buildings at 346-532 South Front Street do not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1940 and 1920, the industrial buildings at 346-532 South Front Street do not represent any distinctive characteristics of the period, nor do they represent the work of a master, or possess high artistic values (Criterion C). The property at 346-532 South Front Street is unlikely to provide information important to our understanding of history (Criterion D).

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

Industrial Attachment

**Narrative Boundary Description:** This property is located at 346-532 South Front Street in Elizabeth, Union County. Associated with block 4 lot 1464, the property is bounded to the northwest by South Front Street and to the south and southeast by Arthur Kill.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

346-532 South Front Street, 349-527 South Front Street, Elizabeth, Union County



346-532 South Front Street, West Corner



346-532 South Front Street, East Corner

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

346-532 South Front Street, 349-527 South Front Street, Elizabeth, Union County



346-532 South Front Street, North Corner



346-532 South Front Street

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

346-532 South Front Street, 349-527 South Front Street, Elizabeth, Union County



346-532 South Front Street

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 15-21, 76-78, 80-312, 314-344 South Front Street

**Street Address:** Street #: 15 344 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: S Street Name: Front Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth City **Block(s):** 4

1438.B, 1461,

**Local Place Name(s):** Elizabeth **Lot(s):** 1462, 1463

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

The original historic building on this lot is a two-story hollow-tile building capped with a flat roof. Built circa 1920, this building was used as a shipping shed in 1923, according to a Sanborn map.

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement Date: May 3, 2007

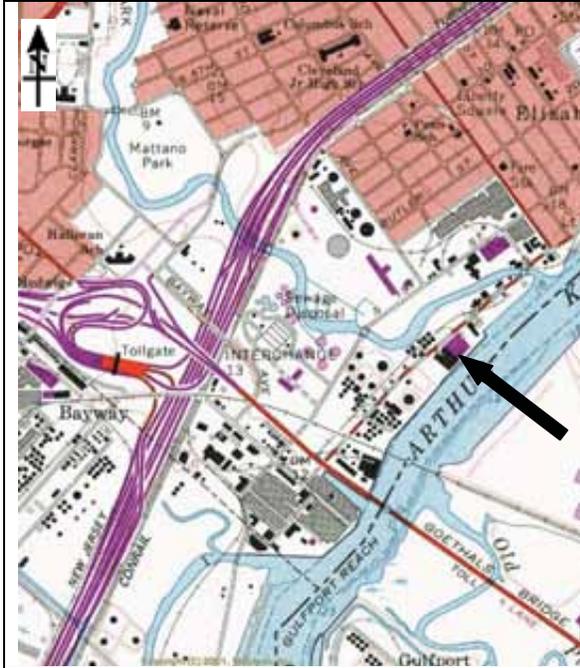
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Sanborn Fire Insurance Maps. Elizabeth, NJ; 1922/1923 and 1950.  
Maptech. Terrain Navigator, Version 7.03a, 1996-2006.  
Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

### INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## INDUSTRIAL BUILDING ATTACHMENT

Historic Sites #:

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<b>Common Name:</b>	<u>15-21, 76-78, 80-312, 314-344 South Front Street</u>			
<b>Historic Name:</b>	<u>Unknown</u>			
<b>Present Use:</b>	<u>Unknown</u>			
<b>Historic Industry:</b>	<u>Unknown</u>	<b>Building ID:</b>	<u>N/A</u>	
<b>Construction Date:</b>	<u>c. 1920</u>	<b>Source:</b>	<u>Stylistic Evidence</u>	
<b>Alteration Date(s):</b>	<u>N/A</u>	<b>Source:</b>	<u>N/A</u>	
<b>Designer:</b>	<u>Unknown</u>	<b>Physical Condition:</b>	<u>Good</u>	
<b>Builder:</b>	<u>Unknown</u>	<b>Remaining Historic Fabric:</b>	<u>High</u>	
<b>Style:</b>	<u>N/A</u>			
	<b>Length:</b>	<u>N/A</u>	<b>Stories:</b>	<u>2</u>
	<b>Width:</b>	<u>N/A</u>	<b>Bays:</b>	<u>N/A</u>
<b>Exterior Finish Materials</b>	<u>Other</u>			
<b>Foundation Materials:</b>	<u>Other</u>			
<b>Structural System:</b>	<u>Unknown</u>	<b>Roof System:</b>	<u>Unknown</u>	
<b>Roof Finish Materials:</b>	<u>Unknown</u>			
<b>Equipment/Machinery:</b>	<u>Unknown</u>			
<b>Transportation Links:</b>	<u>Loading dock, dock, rail siding</u>			

**Exterior Description:** The southeast elevation faces Arthur Kill and is eight bays wide pierced with paired eight-by-eight metal windows, in which the center four lights function as hoppers. It is not entirely clear but at least four periods of construction occurred around this building beginning in the 1960s through the last half of the twentieth century. It now features an irregular footprint, and the non-historic additions sit on a poured-concrete foundation and are capped with a flat roof. The elevation facing South Front Street appears to be the main façade. It is pierced with a number of bays: loading bays, large industrial windows, paired eight-light industrial windows (the center four lights are operable), and single-leaf doors. Many of the loading bays contain metal roll-up doors and are sheltered with a metal pent roof. The elevation facing South Front Street has the label "Eport" painted several times across the top near the roof line. The north elevation of the northern block of the building has modern pre-cast concrete panels and a large loading dock. This block is 18 bays across the west elevation along Front Street. The southern block of the building is 11 bays across and sits on a raised poured-concrete foundation. This section of the roof has an aluminum coping.

**Interior Description:** N/A

**Setting:** This building is sited on the southeast side of South Front Street in a highly industrialized area. The oldest portion of this multi-phase structure is located along the shoreline of Arthur Kill. The area is mostly paved with asphalt with very few small trees and some grass.

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Survey Name:	<u>Goethals Bridge Replacement</u>	Date:	<u>May 3, 2007</u>
Surveyor:	<u>Stuart Dixon, Surveyor; Kristie Baynard, Preparer</u>		
Organization:	<u>The Louis Berger Group Inc.</u>		

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** The shoreline between Perth Amboy and Elizabeth in Union County, New Jersey developed during the last half of the nineteenth century from garden market truck farms into industrial land, becoming home to chemical and fertilizer companies, iron works, and warehouses tied to the shipping industry. Access by several railroads, the shipping ports, and the growth of New York and Newark made the area into a prime outlet for industrial development. The late nineteenth-century industries were replaced during the early twentieth century with oil refineries and as well as plants focused on the production of steel, chrome, and tin. During the mid-twentieth century with the advent of World War II, large tank farms were established along Arthur Kill's shorelines providing storage for gas, oil, and chemicals. In 1923 the property at 15-21, 76-78, 80-312, and 314-344 South Front Street in Elizabeth was owned by the Lubricating Oil Company, which had become the New York Lubricating Oil Company by 1950. In 1923 two additional buildings and several oil tanks were associated with the property. By 1950 a large building had been built immediately northwest of the original shipping shed, and the cooperage shop and barrel shed along the street had been demolished. The property at 15-21, 76-78, 80-312, and 314-344 South Front Street is associated with the New Jersey "Immigration and Agricultural, Industrial, Commercial, and Urban Expansion" context.

**Significance:** The original building associated with the property at 15-21, 76-78, 80-312, and 314-344 South Front Street in Elizabeth is a hollow-tile shipping shed along the shoreline of Arthur Kill. A number of additions have been made to this structure, creating at least four different periods of construction and a very large and irregular-shaped footprint. The original building has is a typical vernacular warehouse dating to circa 1920 in Union County, New Jersey. This modest building lacks any embellishments and is not an uncommon form or building type within the area.

**Eligibility for New Jersey  
and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The buildings at 15-21, 76-78, 80-312, and 314-344 South Front Street in Elizabeth do not appear to meet National Register eligibility criteria owing to a significant loss of integrity. Additions have been made to the original structure in at least four different building phases, all of which are non-historic (1960s and later). The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1920, these vernacular industrial buildings at 15-21, 76-78, 80-312, and 314-344 South Front Street do not represent any distinctive characteristics of the period, nor do they represent the work of a master, or possess high artistic values (Criterion C). The property at 15-21, 76-78, 80-312, and 314-344 South Front Street is unlikely to provide information important to our understanding of history (Criterion D).

### For Historic Districts Only:

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

### For Individual Properties Only:

**List the completed attachments related to the property's significance:**

Industrial Attachment

**Narrative Boundary Description:** These buildings are located at 15-21, 76-78, 80-312, and 314-344 South Front Street in Elizabeth, Union County, and are associated with block 4 lot 1438.B, 1461, 1462, 1463. These lots are bounded to the south and southeast by Arthur Kill and to the north and northwest by South Front Street.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# CONTINUATION SHEET

Historic Sites #:

15-21, 76-78, 80-312, 314-344 South Front Street, Elizabeth, Union County



Eport Building, West Elevation, Facing Southeast



Eport Building, West Elevation, Facing Southeast

Survey Name: Goethals Bridge Replacement  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

Date: May 3, 2007

# CONTINUATION SHEET

Historic Sites #:

15-21, 76-78, 80-312, 314-344 South Front Street, Elizabeth, Union County



Eport Building, Facing Northeast



Eport Building, Southwest Corner, Facing Northeast

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

15-21, 76-78, 80-312, 314-344 South Front Street, Elizabeth, Union County



Eport Building, North Elevation, Facing Southeast



Arthur Kill Building, East Elevation, Looking Southwest

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 2-74 South Front Street

**Street Address:** Street #: 2 74 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: S Street Name: Front Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 1459

**Ownership::** Private **USGS Quad(s)** Elizabeth

**Description:** see industrial attachment

**Registration and Status Dates:** National Historic Landmark: \_\_\_\_\_ SHPO Opinion: \_\_\_\_\_  
National Register: \_\_\_\_\_ Local Designation: \_\_\_\_\_  
New Jersey Register: \_\_\_\_\_ Other Designation: \_\_\_\_\_  
Determination of Eligibility: \_\_\_\_\_ Other Designation Date: \_\_\_\_\_

**Photograph:**

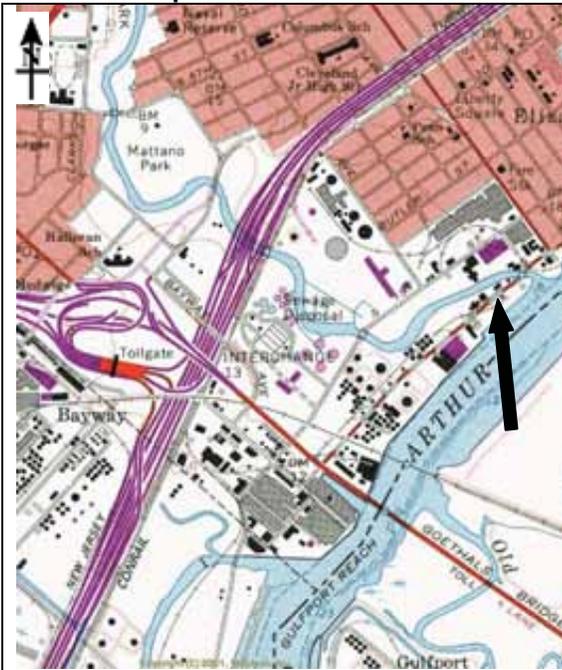


Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Sanborn Fire Insurance Maps. Elizabeth, NJ; 1922/1923 and 1950.  
Maptech. Terrain Navigator, Version 7.03a, 1996-2006.  
Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

### INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## INDUSTRIAL BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** 2-74 South Front Street  
**Historic Name:** Loizeaux Builders Supply Company  
**Present Use:** Unknown  
**Historic Industry:** Unknown **Building ID:** N/A  
**Construction Date:** c. 1920 **Source:** Stylistic Evidence  
**Alteration Date(s):** N/A **Source:** N/A  
**Designer:** Unknown **Physical Condition:** Good  
**Builder:** Unknown **Remaining Historic Fabric:** High  
**Style:** N/A  
**Length:** N/A **Stories:** 2  
**Width:** N/A **Bays:** N/A  
**Exterior Finish Materials:** Other  
**Foundation Materials:** Other  
**Structural System:** Unknown **Roof System:** Unknown  
**Roof Finish Materials:** Unknown  
**Equipment/Machinery:** Unknown  
**Transportation Links:** Loading dock, dock, rail siding

**Exterior Description:** The main building on this large property is a one-story hollow-tile building supply warehouse covered with stucco originally constructed circa 1930. Featuring a rectangular-shaped footprint, this building has a large stepped parapet front wall with a terra cotta coping. The front elevation contains two large roll-up tractor-size doors. The rear elevation has one large oversized doorway with a roll-up door, one single-leaf pedestrian door, one bay with a sliding door, and one bay filled in with concrete blocks. This rear elevation (southeast) originally had two rear sections, which are illustrated on the 1950 Sanborn map. Both of these two rear sections have been demolished, as evident in the unfinished wall of the building, as well as a few protruding ceiling joists. Both the southwest and northeast elevation originally contained four metal windows. Two windows have been in-filled with concrete blocks on the southwest elevation and one bay was in-filled on the northeast.

**Interior Description:** N/A

**Setting:** This building is sited on the southeast side of South Front Street in a highly industrialized area. The oldest portion of this multi-phase structure is located along the shoreline of Arthur Kill. The area is mostly paved with asphalt with very few small trees and some grass.

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** The shoreline between Perth Amboy and Elizabeth in Union County, New Jersey, developed during the last half of the nineteenth century from garden market truck farms into industrial land, becoming home to chemical and fertilizer companies, iron works, and warehouses tied to the shipping industry. Access by several railroads, the shipping ports, and the growth of New York and Newark made the area into a prime outlet for industrial development. The late nineteenth-century industries were replaced during the early twentieth century with oil refineries and as well as plants focused on the production of steel, chrome, and tin. During the mid-twentieth century with the advent of World War II, large tank farms were established along Arthur Kill's shorelines providing storage for gas, oil, and chemicals. In 1923 the property at 15-21, 76-78, 80-312, and 314-344 South Front Street in Elizabeth was owned by the Lubricating Oil Company, which had become the New York Lubricating Oil Company by 1950. In 1923 two additional buildings and several oil tanks were associated with the property. By 1950 a large building had been built immediately northwest of the original shipping shed, and the cooperage shop and barrel shed along the street had been demolished. The property at 15-21, 76-78, 80-312, and 314-344 South Front Street is associated with the New Jersey "Immigration and Agricultural, Industrial, Commercial, and Urban Expansion" context.

**Significance:** The original building associated with the property at 15-21, 76-78, 80-312, and 314-344 South Front Street in Elizabeth is a hollow-tile shipping shed along the shoreline of Arthur Kill. A number of additions have been made to this structure, creating at least four different periods of construction and a very large and irregular-shaped footprint. The original building has is a typical vernacular warehouse dating to circa 1920 in Union County, New Jersey. This modest building lacks any embellishments and is not an uncommon form or building type within the area.

**Eligibility for New Jersey**

**and National Registers:**

Yes  No

**National**

**Register Criteria:**

A  B  C  D

**Level of Significance**

Local  State  National

**Justification of Eligibility/Ineligibility:** The buildings at 15-21, 76-78, 80-312, and 314-344 South Front Street in Elizabeth do not appear to meet National Register eligibility criteria owing to a significant loss of integrity. Additions have been made to the original structure in at least four different building phases, all of which are non-historic (1960s and later). The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1920, these vernacular industrial buildings at 15-21, 76-78, 80-312, and 314-344 South Front Street do not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 15-21, 76-78, 80-312, and 314-344 South Front Street is unlikely to provide information important to our understanding of history (Criterion D).

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

Industrial Attachment

**Narrative Boundary Description:** These buildings are located 15-21, 76-78, 80-312, and 314-344 South Front Street in Elizabeth, Union County, and are associated with block 4 lot 1438.B, 1461, 1462, 1463. These lots are bounded to the south and southeast by Arthur Kill and to the north and northwest by South Front Street.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

### Exterior Description

Several other structures exist on this property, including a mixing plant (1968), a hopper tank/storage tank, and an operations facility. The mixing plant and operations facility are sited adjacent to one another toward the northern end of the property. The hopper tank is located near the circa 1930 warehouse at the southern end of the property. This hopper tank/storage tank consists of one steel frame structure with steel dust collar. The main body of the structure has been clad with plywood and is pierced with one-over-one wood-sash windows. This structure was possibly used as a supervision/operations room. Linked with this building is a storage tank with an elevator that transported the materials into the tank. It is of steel frame construction with a conical roof and one leg feeding the materials from the elevator into the storage bin. Below the storage bin is what appears to be supervision room similar to the first hopper tank described. It is covered with plywood and also contains one-over-one windows. The mixing plant dates to 1968 and is a tall steel structure with an elevator leading to transporting materials. Adjacent to this structure is the operations facility which is two stories, constructed of brick and capped with a flat roof with wide eaves.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# CONTINUATION SHEET

Historic Sites #:

2-74 South Front Street, Elizabeth, Union County



Main Building, Southeast Corner, Facing Northwest



East and North Elevations

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

2-74 South Front Street, Elizabeth, Union County



Mixing Plant, North and West Elevations



Mixing Plant, South and East Elevations

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

2-74 South Front Street, Elizabeth, Union County



Modern Plant, West and South Elevations

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Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 5 South Front Street

**Street Address:** Street #: 5 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: S Street Name: Front Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07206

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 1436

**Ownership::** Private **USGS Quad(s)** Elizabeth

**Description:**

See building attachments

**Registration and Status Dates:**

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

**Photograph:**



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

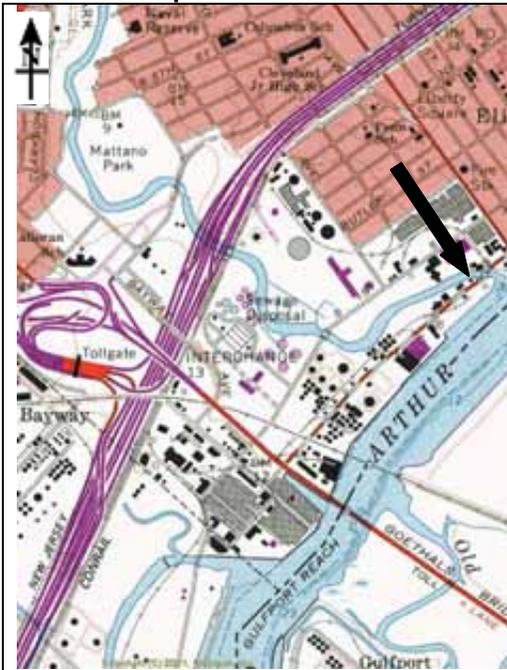
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

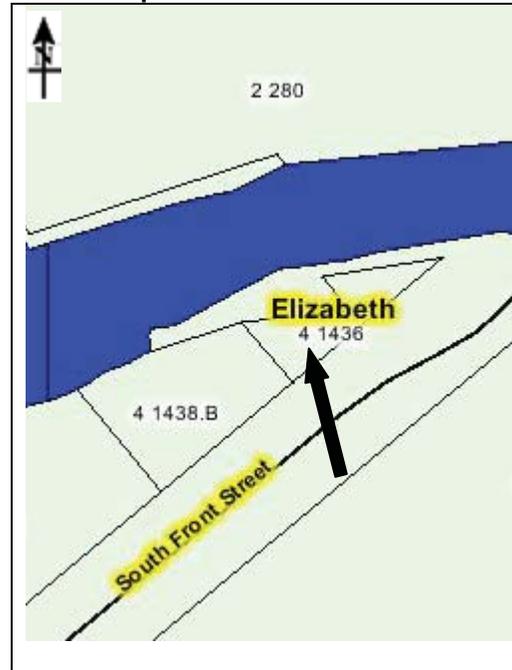
# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Sanborn Fire Insurance Maps. Elizabeth, NJ; 1922/1923 and 1950.  
Maptech. Terrain Navigator, Version 7.03a, 1996-2006.  
Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at  
<http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

## INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** 5 South Front Street

**Historic Name:** Unknown

**Present Use:** Residential, Permanent

**Historic Use:** Residential, Permanent; Unclassifiable activity

**Construction Date:** c. 1875      **Source:** Material evidence and owner

**Alteration Date(s):** c. 2000.      **Source:** Material evidence

**Designer:** Unknown      **Physical Condition:** Excellent

**Builder:** Unknown      **Remaining Historic Fabric:** Medium

**Style:** N/A

**Form:** Other      **Stories:** 2

**Type:** Other      **Bays:** 3

**Roof Finish Materials:** Unknown

**Exterior Finish Materials** Wood, Clapboard

**Exterior Description:** Built circa 1875, this two-story wood-frame building is depicted as a dwelling/warehouse on the 1923 and 1950 Sanborn maps. It is three bays wide on the northeast elevation with the original dwelling featuring a rectangular footprint. Clad with weatherboard siding, this building has a replacement concrete-block foundation, and many of the original windows are missing or covered over with plastic. The shed roof features a bracketed cornice on its northeast elevation. On the southeast elevation is a shed-roof porch on the second story supported by metal posts. On this same elevation is a non-historic shed-roof addition, which has a single-leaf entry providing access to the house. This section is clad with vinyl siding and contains fixed vinyl windows. A hipped-roof, one-story addition stands along the building's northwest elevation. It contains vinyl windows and a three-panel wood door with three upper lights. A modern rectangular building at 11-13 South Front Street is less than 50 years old.

**Interior Description:** N/A

**Setting:** Facing southeast, this dwelling is immediately adjacent to the Elizabeth River and is located on a level lot with one large mature tree to the east.

Survey Name: Sentinel Pipeline Mountain View Loop

Date: May 3, 2007

Surveyor: Kristie Baynard

Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** The shoreline between Perth Amboy and Elizabeth in Union County, New Jersey, developed during the last half of the nineteenth century from garden market truck farms into industrial land, becoming home to chemical and fertilizer companies, iron works, and warehouses tied to the shipping industry. Access by several railroads, the shipping ports, and the growth of New York and Newark made the area into a prime outlet for industrial development. The late nineteenth-century industries were replaced during the early twentieth century with oil refineries and as well as plants focused on the production of steel, chrome, and tin. During the mid-twentieth century with the advent of World War II, large tank farms were established along Arthur Kill's shorelines providing storage for gas, oil, and chemicals.

This building was associated with the Heidritter Lumber Company in 1923 and then the Albemarle Lumber Company in 1950, according to area Sanborn maps. Both 1923 and 1950 Sanborn maps label this building as a dwelling/warehouse. The property at 5 South Front Street in Elizabeth is associated with the New Jersey "Immigration and Agricultural, Industrial, Commercial, and Urban Expansion" context.

**Significance:** The property at 5 South Front Street in Elizabeth contains a typical residential house built for laborers around 1875 in Union County, New Jersey. This modest building lacks any embellishments and is not an entirely uncommon form or building type within the area. Several other modest dwellings built for laborers are located near the APE.

**Eligibility for New Jersey**

**and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The building at 5 South Front Street in Elizabeth does not appear to meet National Register eligibility criteria owing to a lack of integrity. The property is not related to significant historic trends, events, or persons (Criteria A and B). The house built circa 1875, located at 5 South Front Street, does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 5 South Front Street is unlikely to provide information important to our understanding of history (Criterion D).

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

Industrial Attachment

**Narrative Boundary Description:** This building is located at 5 South Front Street in Elizabeth, Union County. It is associated with block 4 lot 1436 and is sited at the northeastern end of a small peninsula. It is bounded to the south and southeast by South Front Street, and to the north by a creek that feeds into Arthur Kill.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

5 South Front Street, Elizabeth, Union County



Southeast Corner



1-13 South Front Street, Facing Southeast

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

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5 South Front Street, Elizabeth, Union County



1-13 South Front Street, Southeast Corner, Facing Northwest

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Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** Russo Brothers Auto Wreckers

**Street Address:** Street #: 65 85 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: S Street Name: Front Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07206

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 1438.A

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

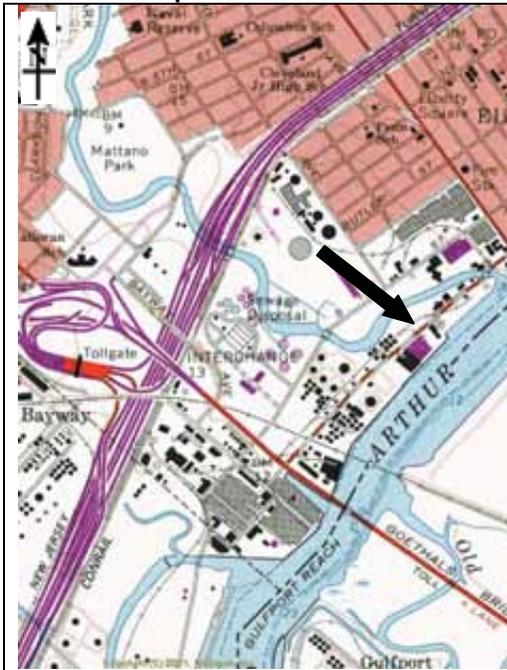
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Sanborn Fire Insurance Maps. Elizabeth, NJ; 1922/1923 and 1950.  
Maptech. Terrain Navigator, Version 7.03a, 1996-2006.  
Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at  
<http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

### INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## INDUSTRIAL BUILDING ATTACHMENT

Historic Sites #:

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<b>Common Name:</b>	<u>Russo Brothers Auto Wreckers</u>			
<b>Historic Name:</b>	<u>Unknown</u>			
<b>Present Use:</b>	<u>Unknown</u>			
<b>Historic Industry:</b>	<u>Unknown</u>	<b>Building ID:</b>	<u>N/A</u>	
<b>Construction Date:</b>	<u>c. 1950</u>	<b>Source:</b>	<u>Stylistic and material evidence</u>	
<b>Alteration Date(s):</b>	<u>N/A</u>	<b>Source:</b>	<u>N/A</u>	
<b>Designer:</b>	<u>Unknown</u>	<b>Physical Condition:</b>	<u>Good</u>	
<b>Builder:</b>	<u>Unknown</u>	<b>Remaining Historic Fabric:</b>	<u>High</u>	
<b>Style:</b>	<u>N/A</u>			
	<b>Length:</b>	<u>N/A</u>	<b>Stories:</b>	<u>1</u>
	<b>Width:</b>	<u>N/A</u>	<b>Bays:</b>	<u>2</u>
<b>Exterior Finish Materials</b>	<u>Concrete block</u>			
<b>Foundation Materials:</b>	<u>Other</u>			
<b>Structural System:</b>	<u>Load-bearing walls</u>	<b>Roof System:</b>	<u>Unknown</u>	
<b>Roof Finish Materials:</b>	<u>Unknown</u>			
<b>Equipment/Machinery:</b>	<u>Unknown</u>			
<b>Transportation Links:</b>	<u>Rail siding</u>			

**Exterior Description:** One story in height and built of concrete blocks, this circa 1950 building features a rectangular footprint with a small wing along its east side facing Front Street. The building contains several metal roll-up doors, and the east elevation contains one multi-light industrial window. A pedestrian door is also covered by an overhead roll security gate. The shed-roof features a wood coping. "Russo Bros Auto Wreckers" is painted on both the east and south elevations. The wing has a sign that reads "J & L Used Auto Parts." A fence obscures views of the building's north elevation.

**Interior Description:** N/A

**Setting:** This property is located in an area that has been highly industrialized since the 1910s and 1920s. The building on this property is situated on a large lot on which there is no vegetation.

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Survey Name:	<u>Goethals Bridge Replacement</u>	Date:	<u>May 3, 2007</u>
Surveyor:	<u>Stuart Dixon, Surveyor; Kristie Baynard, Preparer</u>		
Organization:	<u>The Louis Berger Group Inc.</u>		

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** The shoreline between Perth Amboy and Elizabeth in Union County, New Jersey, developed during the last half of the nineteenth century from garden market truck farms into industrial land, becoming home to chemical and fertilizer companies, iron works, and warehouses tied to the shipping industry. Access by several railroads, the shipping ports, and the growth of New York and Newark made the area into a prime outlet for industrial development. The late nineteenth-century industries were replaced during the early twentieth century with oil refineries and as well as plants focused on the production of steel, chrome, and tin. During the mid-twentieth century with the advent of World War II, large tank farms were established along Arthur Kill's shorelines providing storage for gas, oil, and chemicals.

The properties at 65-85 South Front Street in Elizabeth is associated with the New Jersey "Immigration and Agricultural, Industrial, Commercial, and Urban Expansion" context.

**Significance:** The property at 65-85 South Front Street in Elizabeth, Union County, contains a typical vernacular industrial/garage building dating to circa 1950. This modest building lacks any embellishments and is not an uncommon form within the area.

**Eligibility for New Jersey**

**and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:**

The building at 65-85 South Front Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). The building built circa 1950, located at 65-85 South Front Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 65-85 South Front Street is unlikely to provide information important to our understanding of history (Criterion D).

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

Industrial Attachment

**Narrative Boundary Description:** This building is located at 65-85 South Front Street in Elizabeth, Union County. It is associated with block 4 lot 1438.B, and is bounded to the northwest by a creek that feeds into Arthur Kill, and to the southeast by South Front Street.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** Mattano Park

**Street Address:** Street #: 360 484 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Fifth Suffix: \_\_\_\_\_ Type: AVE

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 453B

**Ownership::** Public **USGS Quad(s)** Elizabeth

**Description:** See attachments.

**Registration and Status Dates:**  
National Historic Landmark: \_\_\_\_\_  
National Register: \_\_\_\_\_  
New Jersey Register: \_\_\_\_\_  
Determination of Eligibility: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_  
Local Designation: \_\_\_\_\_  
Other Designation: \_\_\_\_\_  
Other Designation Date: \_\_\_\_\_

**Photograph:**



Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.



## LANDSCAPE ATTACHMENT

Historic Sites #:

**Common Name:** Mattano Park  
**Historic Name:** Elizabeth River Park, Mattano Park  
**Present Use:** Recreational, active and passive  
**Historic Use:** Recreational, active and passive  
**Construction Date:** 1926      **Source:** \_\_\_\_\_  
**Alteration Date(s):** N/A      **Source:** N/A

### Primary Landscape

**Architect/Designer:** Olmsted Brothers Landscape Architecture  
Grounds designed for outdoor recreation  
**Type:** and/or sports activities      **Physical Condition:** Good  
**Style:** Olmstedian      **Remaining Historic Fabric:** Medium  
**Acreage:** 27  
**Hardscape:** Walkways, asphalt  
**Plantings:** Mature hardwoods  
**Other Features:** Benches

**Description:** Approximately 27 acres in size, Mattano Park is a block-shaped park on the north and east sides of the Elizabeth River northwest of the New Jersey Turnpike. Designed by the Olmsted Brothers Landscape Architecture Firm, it was begun in 1926 with its last design addition in 1958. Large trees landscape the northern and eastern boundaries of the park. A playground is located in the northern section of the park, and four tennis courts, two basketball courts, and an asphalt-paved parking lot are in the southeastern section. The center and southern section of the park consists of open grasslands with numerous fields devoted to softball, baseball, and soccer. A macadamized circular path surrounds the playground area and then creates a curvilinear path extending along the park's eastern boundary down to the parking lot. Concrete and wood benches are placed intermittently along the path, most sited underneath shade trees. A non-historic one-story concrete-block park building stands adjacent to the parking lot. This building is capped with a hipped roof sheathed with rolled asphalt. Rising from the center is a short tower topped with a shallow-pitched hipped roof. The east elevation has one concession window. The walls of the building feature an extensive mural depicting the city landscape with athletes of a variety of sports in the foreground. Earthen embankments have been created as a buffer along the water's edge on the western and southern boundary of the park.

**Setting:** The park is bounded to the west and south by Elizabeth River, to the northeast by Fifth Avenue, to the east by Trenton Avenue, and to the north by South Fifth Street. To the east and northeast of the park is suburban residential development. The area to the south of the park and river is highly industrialized. The New Jersey Turnpike runs northeast-southwest approximately one tenth of a mile from the southeast corner.

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Survey Name: Goethals Bridge Replacement      Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabethport Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. Mattano Park is associated with the New Jersey "Suburban Development" context.

**Significance:** Mattano Park is important as one of the links to Union County's county-wide park system, which was specifically designed to interconnect with the regional park system of the entire New York metropolitan area. This massive greenway system was developed by the Olmsted Brothers to create a system of parkland available to more people than just the surrounding local residents. Mattano Park is also important in more specific terms as containing one of the first designed playgrounds in the nation.

**Eligibility for New Jersey  
and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** Mattano Park in Elizabeth does not appear to meet National Register eligibility criteria. It was designed by the Olmsted Brothers, who designed the Union County park system and nine of the parks within this system, including Mattano Park, originally called Elizabeth River Park. Its integrity has been diminished with the addition of a non-historic park building, as well as the demolition of a historic field house located near the center of the park. The original 1926 plan depicts the park extending to the west side of Elizabeth River to be bounded by Clifton Street. This plan had pathways all throughout the park and included a baseball field, playfield, girl's field, an area for lawn games, a rose garden, pergola, a playground with a wading pool, a picnic grove, and a concert grove. The park has changed extensively with the arrangement of the fields, the addition of field lights, goal posts, tennis courts, and the parking lot.

### For Historic Districts Only:

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

### For Individual Properties Only:

List the completed attachments related to the property's significance:

**Narrative Boundary Description:** Mattano Park is located at 340-484 Fifth Avenue in Elizabeth, Union County, associated with block 4 lot 453B. The park is bounded to the northwest by South Fifth Street, to the northeast by Fifth Avenue, to the south and west by Elizabeth River, and on a small corner to the east by Atlantic Street. The park contains approximately 27 acres.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# CONTINUATION SHEET

Historic Sites #:

Mattano Park, Elizabeth, Union County



Playground, Looking Northwest



Mattano Park, Looking South

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# CONTINUATION SHEET

Historic Sites #:

Mattano Park, Elizabeth, Union County



Mattano Park, Looking North



Park Building, Northwest Corner, Looking Southeast

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

Mattano Park, Elizabeth, Union County



Seating and Game Table, Looking Southwest



Park Building, Southeast Corner, Looking Northwest

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

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Mattano Park, Elizabeth, Union County



Mattano Park, Looking Northeast

Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Property Name: 437 Doyle Street

Street Address: Street #: 437 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Doyle Suffix: \_\_\_\_\_ Type: ST

County(s): Union Zip Code: 07202

Municipality(s): Elizabeth City Block(s): 5

Local Place Name(s): Elizabeth Lot(s): 241.1

Ownership: Private USGS Quad(s) Elizabeth

## Description:

See building attachments

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

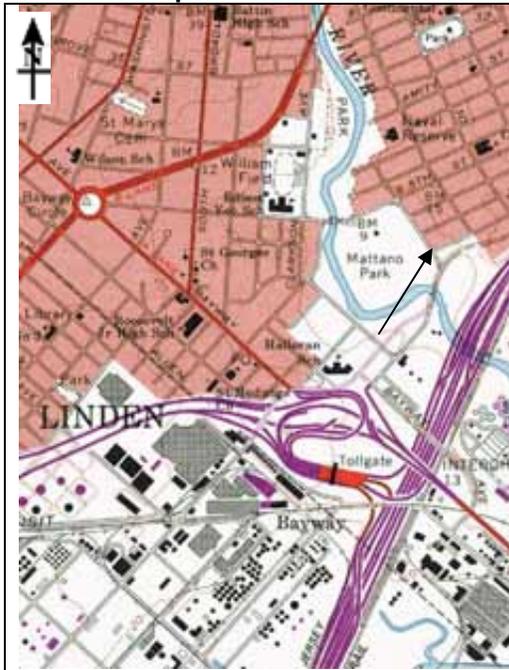
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

**BASE FORM**

Historic Sites #:

**Location Map:**



**Site Map:**



**Bibliography/Sources:**

Maptech. Terrain Navigator, Version 7.03a, 1996-2006.

Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**     Yes     No

**INTENSIVE LEVEL USE ONLY**

**Attachments Included:**     Building     Structure     Object     Bridge  
     Landscape     Industry

**Within Historic District?**     Yes     No

**Status:**     Key-Contributing     Contributing     Non-Contributing

**Associated Archaeological Site/Deposit?**     Yes  
 (Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** House, 437 Doyle Street  
**Historic Name:** N/A  
**Present Use:** Residential, permanent  
**Historic Use:** Residential, permanent  
**Construction Date:** c. 1950 **Source:** Stylistic and material evidence  
**Alteration Date(s):** N/A **Source:** N/A  
**Designer:** Unknown **Physical Condition:** Good  
**Builder:** Unknown **Remaining Historic Fabric:** Medium  
**Style:** None  
**Form:** Gable front **Stories:** 1.5  
**Type:** Other **Bays:** 3  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials** Aluminum Siding

**Exterior Description:** Constructed circa 1950, this one-story, wood-frame dwelling is three bays wide terminating in a front-gable roof. Set on a stretcher bond brick foundation, the modest vernacular house features a rectangular-shaped footprint with a smaller gable wing on the front elevation. The wing has an enclosed porch clad with aluminum siding and pierced with four sets of one-light metal awning windows grouped into three units. The main entry is through this enclosed porch by a single-leaf door. The front elevation features a tripartite window unit with a one-light fixed window flanked by one-over-one wood-sash windows. The house is clad with aluminum siding, and a central-interior brick chimney rises above the roof.

**Interior Description:** N/A

**Setting:** This property is located at the north corner of Doyle Street and Fifth Avenue. It is on a level lot with a chain link fence and a paved driveway, which leads to a garage on the northwest side. A few trees and shrubs dot the property.

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

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**Common Name:** Garage, 437 Doyle Street

**Historic Name:** N/A

**Present Use:** Residential

**Historic Use:** Residential

**Construction Date:** c.1950

**Source:** Stylistic and material evidence

**Alteration Date(s):** N/A

**Source:** N/A

**Designer:** Unknown

**Physical Condition:** Good

**Builder:** Unknown

**Remaining Historic Fabric:** Medium

**Style:** None

**Form:** Gable front

**Stories:** 1

**Type:** Garage

**Bays:** 1

**Roof Finish Materials:** Asphalt shingle

**Exterior Finish Materials** Aluminum siding

**Exterior Description:** The circa 1950 one-story wood-frame garage is set on a concrete foundation, clad with asbestos siding, and capped with a front-gable asphalt-shingle roof. The façade contains an off-center roll-up vinyl garage door.

**Interior Description:** N/A

**Setting:** The garage is on the northwest side of the house.

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Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the River occurred in the last half of the twentieth century, particularly following World War II. The property at 437 Doyle Street in Elizabeth is associated with the "Modern New Jersey" historic context.

**Significance:** The property at 437 Doyle Street in Elizabeth contains a typical vernacular front-gable house and garage dating to circa 1950 in Union County, New Jersey. This modest dwelling lacks any embellishments and is not an uncommon form within the area. Several other houses of similar form and style neighbor this house, suggesting that it was part of speculative construction by one developer. Speculative construction following World War II was not uncommon in Union County.

**Eligibility for New Jersey  
and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The house at 437 Doyle Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1950, the vernacular house at 437 Doyle Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 437 Doyle Street is unlikely to provide information important to our understanding of history (Criterion D).

### For Historic Districts Only:

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

### For Individual Properties Only:

List the completed attachments related to the property's significance:

**Narrative Boundary Description:** The property is located at 437 Doyle Street in Elizabeth, Union County. It is associated with block 5 lot 241.I. It is located at the northeast corner of Fifth Avenue and Doyle Street. Mattano Park is located to the west of the property.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

437 Doyle Street, Elizabeth, Union County



East Corner



Looking Northeast

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 436 Redcliffe Street

**Street Address:** Street #: 436 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Redcliffe Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07206

**Municipality(s):** Elizabeth City **Block(s):** 5

**Local Place Name(s):** Elizabeth **Lot(s):** 1153.i

**Ownership::** Private **USGS Quad(s)** Elizabeth

**Description:** See building attachment

**Registration and Status Dates:** National Historic Landmark: \_\_\_\_\_ SHPO Opinion: \_\_\_\_\_  
National Register: \_\_\_\_\_ Local Designation: \_\_\_\_\_  
New Jersey Register: \_\_\_\_\_ Other Designation: \_\_\_\_\_  
Determination of Eligibility: \_\_\_\_\_ Other Designation Date: \_\_\_\_\_

**Photograph:**

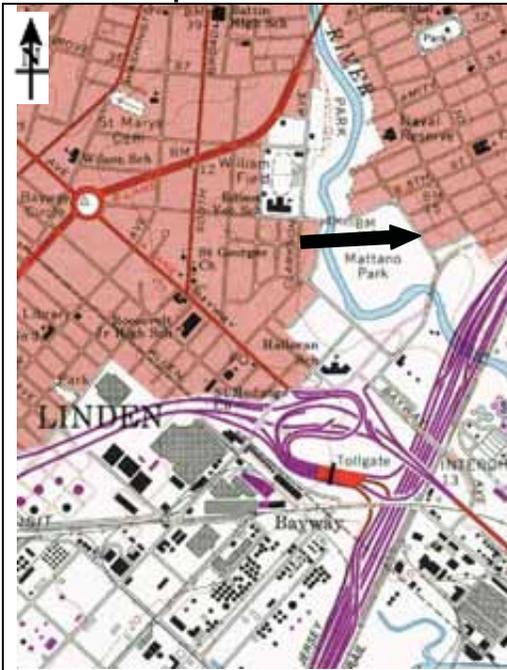


Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Sanborn Fire Insurance Maps. Elizabeth, NJ; 1922/1923 and 1950.  
Maptech. Terrain Navigator, Version 7.03a, 1996-2006.  
Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at  
<http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

### INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** House, 436 Redcliffe Street  
**Historic Name:** Unknown  
**Present Use:** Residential, Permanent  
**Historic Use:** Residential, Permanent  
**Construction Date:** c. 1950      **Source:** Stylistic evidence  
**Alteration Date(s):** N/A      **Source:** N/A  
**Designer:** Unknown      **Physical Condition:** Good  
**Builder:** Unknown      **Remaining Historic Fabric:** Medium  
**Style:** None  
**Form:** Gable front      **Stories:** 1.5  
**Type:** Other      **Bays:** 3  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials** Vinyl Siding

**Exterior Description:** Constructed circa 1950, this one-story wood-frame dwelling is three bays wide terminating in a front-gable roof. Set on a stucco-covered foundation, the modest vernacular house features a rectangular-shaped footprint with a smaller gable wing on the front elevation. The wing has an inset one-bay porch supported with one brick post. The main entry is sheltered by the inset porch and contains a single-leaf nine-panel door. The front elevation features a tripartite window unit with a one-light fixed window flanked by one-over-one wood-sash windows. The front gable, front wing, and the northeast elevation contain one-over-one wood-sash windows. The house is clad with vinyl siding, and a central-interior brick chimney rises above the asphalt-shingle roof.

**Interior Description:** N/A

**Setting:** Facing northwest, this property is located at the south corner of Redcliffe Street and Fifth Avenue. It is on a level grassy lot, and a wood fence encloses the backyard.

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Survey Name: Goethals Bridge Replacement      Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. The property at 436 Redcliffe Street in Elizabeth is associated with the "Modern New Jersey" historic context.

**Significance:** The property at 436 Redcliffe Street in Elizabeth contains a typical vernacular front-gable house dating to circa 1950 in Union County, New Jersey. This modest dwelling lacks any embellishments and is not an uncommon form within the area. Several other houses of similar form and style neighbor this house, suggesting that it was part of speculative construction by one developer. Speculative construction following World War II was not uncommon in Union County.

**Eligibility for New Jersey**

**and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The house at 436 Redcliffe Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1950, the vernacular house at 436 Redcliffe Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 436 Redcliffe Street is unlikely to provide information important to our understanding of history (Criterion D).

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

**Narrative Boundary Description:** The property is located at 436 Redcliffe Street in Elizabeth, Union County. It is associated with block 5 lot 1153I. It is located at the southeast corner of Fifth Avenue and Redcliffe Street. Mattano Park is located to the west of the property.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

436 Redcliffe Street, Elizabeth, Union County



West Corner

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 437 Redcliffe Street

**Street Address:** Street #: 437 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Redcliffe Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07206

**Municipality(s):** Elizabeth City **Block(s):** 5

**Local Place Name(s):** Elizabeth **Lot(s):** 1144D

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

**Registration and Status Dates:** National Historic Landmark: \_\_\_\_\_ SHPO Opinion: \_\_\_\_\_  
National Register: \_\_\_\_\_ Local Designation: \_\_\_\_\_  
New Jersey Register: \_\_\_\_\_ Other Designation: \_\_\_\_\_  
Determination of Eligibility: \_\_\_\_\_ Other Designation Date: \_\_\_\_\_

## Photograph:

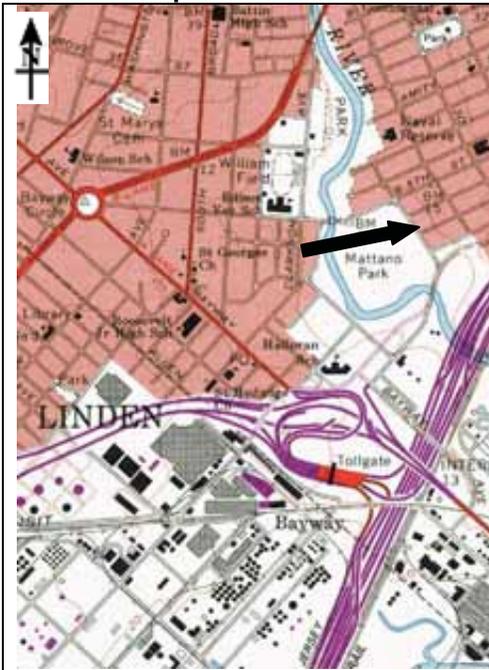


Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

**BASE FORM**

**Historic Sites #:**

**Location Map:**



**Site Map:**



**Bibliography/Sources:** Sanborn Fire Insurance Maps. Elizabeth, NJ; 1922/1923 and 1950.  
 Maptech. Terrain Navigator, Version 7.03a, 1996-2006.  
 Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at  
<http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

**INTENSIVE LEVEL USE ONLY**

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
 (Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
 Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
 Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** House, 437 Redcliffe Street  
**Historic Name:** Unknown  
**Present Use:** Residential, Permanent  
**Historic Use:** Residential, Permanent  
**Construction Date:** c. 1950      **Source:** Stylistic evidence  
**Alteration Date(s):** N/A      **Source:** N/A  
**Designer:** Unknown      **Physical Condition:** Good  
**Builder:** Unknown      **Remaining Historic Fabric:** Medium  
**Style:** None  
**Form:** Gable front      **Stories:** 1.5  
**Type:** Other      **Bays:** 3  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials:** Vinyl siding

**Exterior Description:** Constructed circa 1950, this one-and-a-half story, wood-frame dwelling is three bays wide terminating in a front-gable roof. Set on a stucco-covered foundation, the modest vernacular house features a rectangular-shaped footprint with a smaller gable wing on the front elevation. The wing has an inset one-bay porch supported with one decorative metal post and a metal balustrade. The main entry is sheltered by the inset porch and contains a single-leaf door. The northeast elevation features a centered tripartite window unit with a one-light fixed window flanked by one-over-one vinyl sash windows. The remaining elevations contain one-over-one vinyl sash windows. The house is clad with German vinyl siding, and a central-interior brick chimney rises above the asphalt-shingle roof. Other architectural features include diamond-pane windows in the basement, a brick and poured-concrete porch deck and stoop, and a shed-roof dormer on the southwest elevation.

**Interior Description:** N/A

**Setting:** Facing southwest toward Mattano Park, this dwelling is sited on a level grassy lot set slightly back from Fifth Avenue. It is located at the north corner of Fifth Avenue and Redcliffe Street. A concrete driveway leads to the garage at the northwest side of the house.

Survey Name: Goethals Bridge Replacement      Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** Garage, 437 Redcliffe Street  
**Historic Name:** N/A  
**Present Use:** Residential  
**Historic Use:** Residential  
**Construction Date:** c. 1950 **Source:** Stylistic evidence  
**Alteration Date(s):** N/A **Source:** N/A  
**Designer:** Unknown **Physical Condition:** Good  
**Builder:** Unknown **Remaining Historic Fabric:** Medium  
**Style:** None  
**Form:** Gable front **Stories:** 1  
**Type:** Garage **Bays:** 1  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials:** Vinyl Siding

**Exterior Description:** Sited adjacent to the dwelling to the southwest is a circa 1950 one-story wood-frame garage. Set on a concrete-block foundation, the garage is clad with German vinyl siding and capped with a front-gable asphalt-shingle roof featuring a boxed cornice. The façade contains an off-center roll-up garage door.

**Interior Description:** N/A

**Setting:** The garage is located on the northwest side of the house.

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. The property at 437 Redcliffe Street in Elizabeth is associated with the "Modern New Jersey" historic context.

**Significance:** The property at 437 Redcliffe Street in Elizabeth contains a typical vernacular front-gable house and garage dating to circa 1950 in Union County, New Jersey. This modest dwelling lacks any embellishments and is not an uncommon form within the area. Several other houses of similar form and style neighbor this house, suggesting that it was part of speculative construction by one developer. Speculative construction following World War II was not uncommon in Union County.

**Eligibility for New Jersey**

**and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The house at 437 Redcliffe Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1950, the vernacular house at 437 Redcliffe Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 437 Redcliffe Street is unlikely to provide information important to our understanding of history (Criterion D).

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

**Narrative Boundary Description:** The property is located at 437 Redcliffe Street in Elizabeth, Union County. It is associated with block 5 lot 1144D. It is located at the northeast corner of Fifth Avenue and Redcliffe Street. Mattano Park is located to the west of the property.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# CONTINUATION SHEET

Historic Sites #:

437 Redcliffe Street, Elizabeth, Union County



East Corner



Looking East

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Property Name: 436 Loomis Street

Street Address: Street #: 436 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Loomis Suffix: \_\_\_\_\_ Type: ST

County(s): Union Zip Code: 07206

Municipality(s): Elizabeth City Block(s): 5

Local Place Name(s): Elizabeth Lot(s): 859E

Ownership: Private USGS Quad(s) Elizabeth

## Description:

See building attachment

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

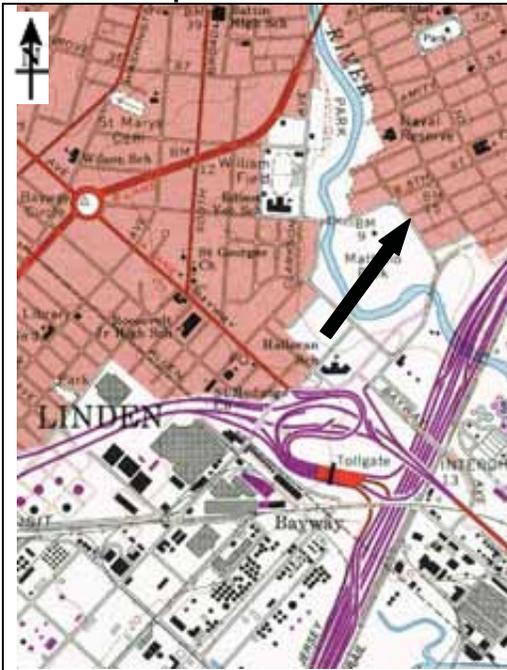
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Sanborn Fire Insurance Maps. Elizabeth, NJ; 1922/1923 and 1950.  
Maptech. Terrain Navigator, Version 7.03a, 1996-2006.  
Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at  
<http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

### INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** House, 436 Loomis Street  
**Historic Name:** Unknown  
**Present Use:** Residential, Permanent  
**Historic Use:** Residential, Permanent  
**Construction Date:** c. 1950 **Source:** Stylistic evidence  
**Alteration Date(s):** N/A **Source:** N/A  
**Designer:** Unknown **Physical Condition:** Good  
**Builder:** Unknown **Remaining Historic Fabric:** Medium  
**Style:** None  
**Form:** Gable front **Stories:** 1.5  
**Type:** Other **Bays:** 3  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials** Vinyl Siding

**Exterior Description:** Constructed circa 1950, this one-and-a-half-story, wood-frame dwelling is three bays wide terminating in a front-gable roof. Set on a stucco-covered foundation, the modest vernacular house features a rectangular-shaped footprint with a smaller gable wing on the front elevation. The wing originally had an inset one-bay porch, which has been enclosed. The main entry is into the enclosed porch section, and a shed-roof porch addition shelters the entry and a tripartite window on the façade of the main block. The entry contains a single-leaf door, and the tripartite window has a one-light fixed window flanked by one-over-one vinyl sash windows. Other windows throughout the house are one-over-one vinyl sash units. The southwest elevation is not pierced with any openings. The house is clad with vinyl siding, and a central-interior brick chimney rises above the asphalt-shingle roof. Other architectural features include a shed-roof dormer on the northeast elevation and a wood deck on the rear (southeast) elevation.

**Interior Description:** N/A

**Setting:** Facing northwest toward Loomis Street, this dwelling is located at the south corner of Loomis Street and Fifth Avenue. It is on a level grassy lot set slightly back from the road. The property is fronted with foundation plantings and a few trees and shrubs.

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. The property at 436 Loomis Street in Elizabeth is associated with the "Modern New Jersey" historic context.

**Significance:** The property at 436 Loomis Street in Elizabeth contains a typical vernacular front-gable house dating to circa 1950 in Union County, New Jersey. This modest dwelling lacks any embellishments and is not an uncommon form within the area. Several other houses of similar form and style neighbor this house suggesting it was part of speculative construction by one developer. Speculative construction following World War II was not uncommon in Union County.

**Eligibility for New Jersey  
and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The house at 436 Loomis Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1950, the vernacular house at 436 Loomis Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 436 Loomis Street is unlikely to provide information important to our understanding of history (Criterion D).

### For Historic Districts Only:

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

### For Individual Properties Only:

List the completed attachments related to the property's significance:

**Narrative Boundary Description:** The property is located at 436 Loomis Street in Elizabeth, Union County. It is associated with block 5 lot 859.E. It is located at the southeast corner of Fifth Avenue and Loomis Street. Mattano Park is located to the west of the property.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

436 Loomis Street, Elizabeth, Union County



Southeast Elevation



Northeast Elevation

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 475 Fifth Avenue

**Street Address:** Street #: 475 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Fifth Suffix: \_\_\_\_\_ Type: AVE

**County(s):** Union **Zip Code:** 07206

**Municipality(s):** Elizabeth City **Block(s):** 5

**Local Place Name(s):** Elizabeth **Lot(s):** 895

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement Date: May 3, 2007

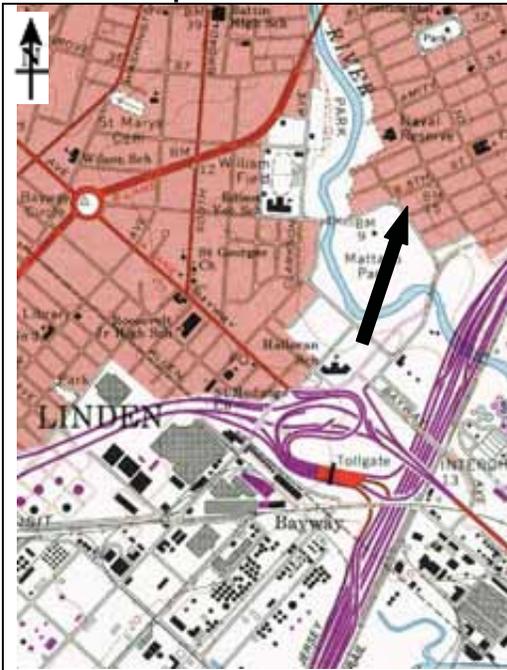
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Sanborn Fire Insurance Maps. Elizabeth, NJ; 1922/1923 and 1950.  
Maptech. Terrain Navigator, Version 7.03a, 1996-2006.  
Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at  
<http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

### INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** House, 475 Fifth Avenue  
**Historic Name:** Unknown  
**Present Use:** Residential, Permanent  
**Historic Use:** Residential, Permanent  
**Construction Date:** c. 1950 **Source:** Stylistic evidence  
**Alteration Date(s):** N/A **Source:** N/A  
**Designer:** Unknown **Physical Condition:** Good  
**Builder:** Unknown **Remaining Historic Fabric:** Medium  
**Style:** None  
**Form:** Other **Stories:** 1.5  
**Type:** Other **Bays:** 3  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials** Brick, Running bond

**Exterior Description:** Built circa 1950, the one-and-a-half-story, brick house is three bays wide and two bays deep. Featuring a rectangular-shaped footprint, this dwelling is covered in stretcher bond and is capped with a side-gable, asphalt-shingle roof. The main façade contains a central, single-leaf entry flanked by one pair of one-over-one vinyl windows and a triple one-over-one vinyl window unit. The façade has an inset porch on the northwestern end supported with a brick post. The porch shelters one single-leaf door on the northwest elevation. The main roof slope is adorned with two vinyl-clad gable dormers, each with one one-over-one vinyl windows, and the rear elevation has a vinyl-clad, four-bay shed dormer. Other architectural features of the house include brick rowlock window sills, overhanging eaves with a boxed cornice, and one exterior-end brick chimney.

**Interior Description:** N/A

**Setting:** Facing northwest toward Mattano Park, this dwelling is located at the north corner of Loomis Street and Fifth Avenue. It is on a level grassy lot set slightly back from the road. The property is fronted with foundation plantings and dotted with a few trees and shrubs.

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. The property at 475 Fifth Avenue in Elizabeth is associated with the "Modern New Jersey" historic context.

**Significance:** The property at 475 Fifth Avenue in Elizabeth contains a typical vernacular front-gable house dating to circa 1950 in Union County, New Jersey. This modest dwelling lacks any embellishments and is not an uncommon form within the area. Several other houses of similar form and style neighbor this house, suggesting that it was part of speculative construction by one developer. Speculative construction following World War II was not uncommon in Union County.

**Eligibility for New Jersey  
and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The house at 475 Fifth Avenue in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1950, the vernacular house at 475 Fifth Avenue does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 475 Fifth Avenue is unlikely to provide information important to our understanding of history (Criterion D).

### For Historic Districts Only:

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

### For Individual Properties Only:

List the completed attachments related to the property's significance:

**Narrative Boundary Description:** The property is located at 475 Fifth Avenue in Elizabeth, Union County. It is associated with block 5 lot 859. It is located at the northeast corner of Fifth Avenue and Loomis Street. Mattano Park is located to the west of the property.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

475 Fifth Avenue, Elizabeth, Union County



Northwest Elevation



Southeast Elevation

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 505 South Fifth Street

**Street Address:** Street #: 505 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: S Street Name: Fifth Street Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07206

**Municipality(s):** Elizabeth City **Block(s):** 5

**Local Place Name(s):** Elizabeth **Lot(s):** 1251

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

**Registration and Status Dates:** National Historic Landmark: \_\_\_\_\_ SHPO Opinion: \_\_\_\_\_  
National Register: \_\_\_\_\_ Local Designation: \_\_\_\_\_  
New Jersey Register: \_\_\_\_\_ Other Designation: \_\_\_\_\_  
Determination of Eligibility: \_\_\_\_\_ Other Designation Date: \_\_\_\_\_

## Photograph:

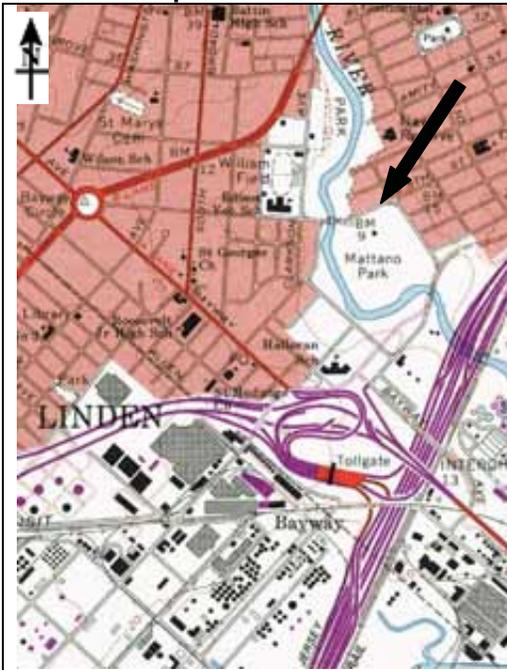


Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Sanborn Fire Insurance Maps. Elizabeth, NJ; 1922/1923 and 1950.  
Maptech. Terrain Navigator, Version 7.03a, 1996-2006.  
Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at  
<http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

### INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** House, 505 South Fifth Street  
**Historic Name:** Unknown  
**Present Use:** Residential, Permanent  
**Historic Use:** Residential, Permanent  
**Construction Date:** c. 1945      **Source:** Stylistic evidence  
**Alteration Date(s):** N/A      **Source:** N/A  
**Designer:** Unknown      **Physical Condition:** Good  
**Builder:** Unknown      **Remaining Historic Fabric:** Medium  
**Style:** Colonial Revival, vernacular  
**Form:** Gable front      **Stories:** 1.5  
**Type:** Other      **Bays:** 3  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials** Brick, Running bond

**Exterior Description:** Built circa 1945, the one-story brick house is three bays wide and three bays deep and set on a poured-concrete foundation. Featuring a rectangular-shaped footprint, this dwelling is covered in a stretcher bond and is capped with a front-gable, asphalt-shingle roof. The front entry is in a centered brick gable vestibule on the façade and contains a paneled wood door with an oval light. The house is pierced with one-over-one vinyl windows, and the front gable features paired one-over-one windows, all with brick rowlock sills and soldier-course lintels. Fiberglass awnings shade the main southeast elevation's windows and door. An interior brick chimney rises above the roof, and the roof features overhanging eaves with a boxed cornice.

**Interior Description:** N/A

**Setting:** Facing southeast toward Mattano Park, this dwelling is on the north side South Fifth Street. A small one-story, pre-fabricated metal shed stands on the rear portion of the lot behind the house. It is on a level lot set just off the road and a concrete driveway is on the southwest side.

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Survey Name: Goethals Bridge Replacement      Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. The property at 505 South Fifth Street in Elizabeth is associated with the "Modern New Jersey" historic context.

**Significance:** The property at 505 South Fifth Street in Elizabeth contains a typical vernacular front-gable house dating to circa 1945 in Union County, New Jersey. This modest dwelling lacks any embellishments and is not an uncommon form within the area. Several other houses of similar form and style are located in this community.

**Eligibility for New Jersey  
and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The house at 505 South Fifth Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1945, the vernacular house at 505 South Fifth Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 505 South Fifth Street is unlikely to provide information important to our understanding of history (Criterion D).

### For Historic Districts Only:

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

### For Individual Properties Only:

**List the completed attachments related to the property's significance:**

**Narrative Boundary Description:** The property is located at 505 South Fifth Street in Elizabeth, Union County. It is associated with block 5 lot 1251. It is located on the north side of South Fifth Street and Mattano Park is to the south of the property.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

505 South Fifth Street, Elizabeth, Union County



South Corner

Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 511-513 South Fifth Street

**Street Address:** Street #: 511 513 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: S Street Name: Fifth Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07206

**Municipality(s):** Elizabeth City **Block(s):** 5

**Local Place Name(s):** Elizabeth **Lot(s):** 1253

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

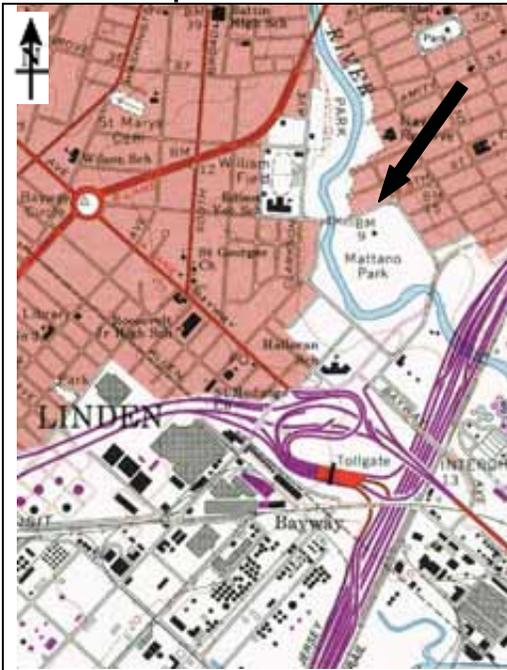
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Sanborn Fire Insurance Maps. Elizabeth, NJ; 1922/1923 and 1950.  
Maptech. Terrain Navigator, Version 7.03a, 1996-2006.  
Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at  
<http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

## INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** 511-513 South Fifth Street  
**Historic Name:** Unknown  
**Present Use:** Residential, Permanent  
**Historic Use:** Residential, Permanent; Commercial  
**Construction Date:** c. 1940      **Source:** Stylistic evidence  
**Alteration Date(s):** N/A      **Source:** N/A  
**Designer:** Unknown      **Physical Condition:** Good  
**Builder:** Unknown      **Remaining Historic Fabric:** Medium  
**Style:** None  
**Form:** Other      **Stories:** 2  
**Type:** Other      **Bays:** 4  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials** Vinyl siding

**Exterior Description:** Two stories in height and four bays wide on the first story of its main (southeast) elevation, this circa 1940 mixed-use, wood-frame building is six bays deep and capped with a flat roof. Two centered single-leaf entries on the main façade are sheltered with a non-historic gable-roof hood supported with knee braces. The entries are illuminated with transoms, and flanking the doors are paired one-over-one aluminum windows with aluminum surrounds. The façade's first story is covered with a stucco veneer and across the full-width is a hipped pent roof, which is mimicked with a larger and broader hipped pent roof above the second story. The building is clad with vinyl siding, and the top pent roof is sheathed with asphalt shingles.

**Interior Description:** N/A

**Setting:** Facing southeast toward Mattano Park, this building is on the north side South Fifth Street. It is on a level lot set adjacent to the road, and a chain link fence stands along the northeast side of the building.

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Survey Name: Goethals Bridge Replacement      Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. The property at 511-513 South Fifth Street in Elizabeth is associated with the "Metropolitan New Jersey" historic context.

**Significance:** The property at 511-513 South Fifth Street in Elizabeth contains a typical vernacular mixed-use building dating to circa 1940 in Union County, New Jersey. This modest building originally functioned as a dwelling and a store. This dwelling lacks any embellishments and is not an uncommon form within the area. Several other mixed-use as well as two-story, shed-roof buildings are located in the immediate area, such as the building at 539 South Fifth Street on the same block.

**Eligibility for New Jersey**

**and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The building at 511-513 South Fifth Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1940, the vernacular building at 511-513 South Fifth Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 511-513 South Fifth Street is unlikely to provide information important to our understanding of history (Criterion D).

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

**Narrative Boundary Description:** The property is located at 511-513 South Fifth Street in Elizabeth, Union County. It is associated with block 5 lot 1253. It is located on the north side of South Fifth Street and Mattano Park is to the south of the property.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

511-513 South Fifth Street, Elizabeth, Union County



South Corner

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 515 South Fifth Street

**Street Address:** Street #: 515 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: S Street Name: Fifth Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07206

**Municipality(s):** Elizabeth City **Block(s):** 5

**Local Place Name(s):** Elizabeth **Lot(s):** 1254

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

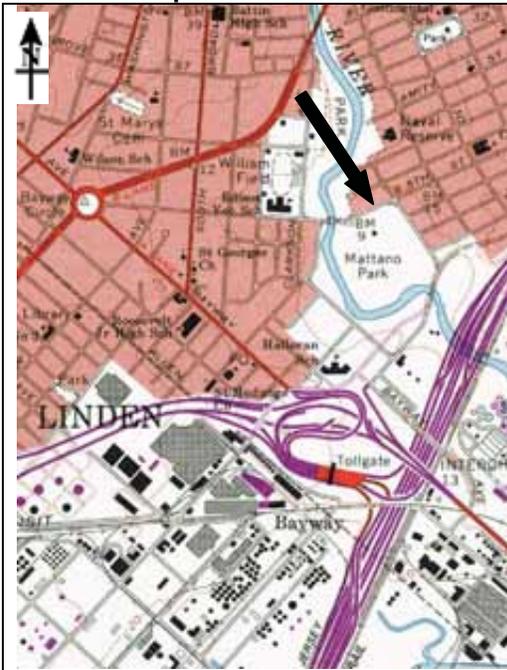
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Sanborn Fire Insurance Maps. Elizabeth, NJ; 1922/1923 and 1950.  
Maptech. Terrain Navigator, Version 7.03a, 1996-2006.  
Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at  
<http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

## INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** House, 515 South Fifth Street  
**Historic Name:** Unknown  
**Present Use:** Residential, Permanent  
**Historic Use:** Residential, Permanent  
**Construction Date:** c. 1930      **Source:** Stylistic evidence  
**Alteration Date(s):** N/A      **Source:** N/A  
**Designer:** Unknown      **Physical Condition:** Good  
**Builder:** Unknown      **Remaining Historic Fabric:** Medium  
**Style:** Other  
**Form:** Other      **Stories:** 2  
**Type:** Other      **Bays:** 3  
**Roof Finish Materials:** Unknown  
**Exterior Finish Materials** Aluminum siding

### Exterior Description:

Built circa 1930, this multiple-family dwelling is two stories and three bays across, clad with aluminum siding and capped with a shed roof. The façade has two off-center single-leaf six-panel vinyl doors sheltered with a metal awning. The house is pierced by one-over-one vinyl windows with aluminum surrounds. Other architectural features include an aluminum cornice, a poured-concrete stoop, and a two-story rear porch with an enclosed second story.

**Interior Description:** N/A

### Setting:

A small one-story, gable-roofed shed, younger than 50 years old, stands on the rear portion of the lot behind the house. The property is on a level lot and the dwelling is set close to the road.

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Survey Name: Goethals Bridge Replacement      Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the River occurred in the last half of the twentieth century, particularly following World War II. The property at 515 South Fifth Street in Elizabeth is associated with the "Metropolitan New Jersey" historic context.

**Significance:** The property at 515 South Fifth Street in Elizabeth contains a typical vernacular dwelling dating to circa 1930 in Union County, New Jersey. This dwelling lacks any embellishments and is not an uncommon form within the area. Several other two-story multiple-family dwellings are located in the immediate area.

**Eligibility for New Jersey**

**and National Registers:**

Yes  No

**National**

**Register Criteria:**

A  B  C  D

**Level of Significance**

Local  State  National

**Justification of Eligibility/Ineligibility:** The building at 515 South Fifth Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1930, the vernacular building at 515 South Fifth Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 515 South Fifth Street is unlikely to provide information important to our understanding of history (Criterion D). Its integrity has diminished with the replacement siding and replacement windows.

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

**Narrative Boundary Description:**

The property is located at 515 South Fifth Street in Elizabeth, Union County. It is associated with block 5 lot 1254. It is located on the north side of South Fifth Street and Mattano Park is to the south of the property.

Survey Name: Goethals Bridge Replacement Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

515 South Fifth Street, Elizabeth, Union County



South Corner

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Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 517 South Fifth Street

**Street Address:** Street #: 517 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: South Street Name: Fifth Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07206

**Municipality(s):** Elizabeth City **Block(s):** 5

**Local Place Name(s):** Elizabeth **Lot(s):** 1256

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

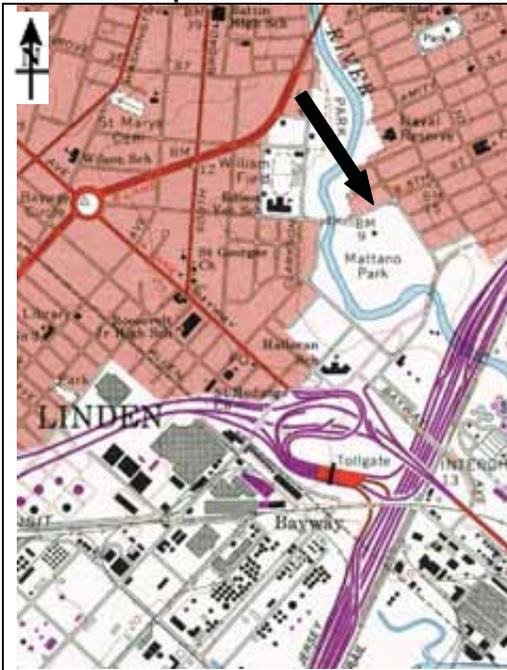
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Sanborn Fire Insurance Maps. Elizabeth, NJ; 1922/1923 and 1950.  
Maptech. Terrain Navigator, Version 7.03a, 1996-2006.  
Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at  
<http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

### INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** House, 517 South Fifth Street  
**Historic Name:** N/A  
**Present Use:** Residential, Permanent  
**Historic Use:** Residential, Permanent  
**Construction Date:** c. 1920 **Source:** Stylistic and material evidence  
**Alteration Date(s):** N/A **Source:** N/A  
**Designer:** Unknown **Physical Condition:** Good  
**Builder:** Unknown **Remaining Historic Fabric:** Medium  
**Style:** Colonial Revival  
**Form:** Other **Stories:** 2.5  
**Type:** Other **Bays:** 3  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials** Asbestos siding

**Exterior Description:** Built circa 1920, the two-and-a-half story wood-frame dwelling features a T-shaped footprint with a projecting front-gable bay on the southwest elevation. Set on a stucco-covered foundation and clad with asbestos siding, the house is three bays wide on the main façade. This section has a hipped roof enclosed porch across the full width of the façade with an off-center aluminum single-leaf door. The pedimented gable is illuminated by one-over-one wood-sash windows with square-edged aluminum surrounds. Two stuccoed interior chimneys rise above the asphalt-shingle cross-gable roof area. Other architectural features of the house include overhanging eaves, a boxed wood cornice, and partial returns on the projecting gable section.

**Interior Description:** N/A

### Setting:

Facing southeast toward Mattano Park, this building is on the north side South Fifth Street. It is on a level lot set adjacent to the road, and an asphalt-paved driveway leads a garage at the northwest side of the house.

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

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**Common Name:** Garage, 517 South Fifth Street

**Historic Name:** N/A

**Present Use:** Residential

**Historic Use:** Residential

**Construction Date:** c. 1940      **Source:** Stylistic and material evidence

**Alteration Date(s):** N/A      **Source:** N/A

**Designer:** Unknown

**Physical Condition:** Good

**Builder:** Unknown

**Remaining Historic Fabric:** Medium

**Style:** Other

**Form:** Other

**Stories:** 1

**Type:** Garage

**Bays:** 2

**Roof Finish Materials:** Asphalt shingle

**Exterior Finish Materials** Asbestos siding

**Exterior Description:** Built circa 1940, the one-story wood-frame garage is two bays wide with two roll-up garage doors. It is clad with asbestos siding and has a front-gable asphalt-shingle roof with a full pediment on the facade similar to the dwelling.

**Interior Description:** N/A

**Setting:** This garage is located on the northwest side of the house.

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Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. The property at 517 South Fifth Street is associated with the "Metropolitan New Jersey" historic context.

**Significance:** The property at 517 South Fifth Street in Elizabeth is a typical two-story, gable-front, Colonial Revival-style dwelling dating to circa 1920. During the first half of the twentieth century, Colonial Revival became the most popular style choice across the United States among middle- and upper-class Americans for their houses, as well as other types such as ecclesiastical and institutional. Features like gable roofs, molded cornices, full pediments, and two stories are typical characteristics of Colonial Revival houses of this period, as seen on this house. Many of the houses in this style are modest and lack any great amount of detail. The style's zenith occurred between the 1920s and 1950s and continues to be prevalent at the turn of the twenty-first century. This dwelling lacks many embellishments and is not an uncommon form within the area. Several other similar dwellings are located in the immediate area, e.g., neighboring houses at 521 and 525 South Fifth Street.

**Eligibility for New Jersey  
and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The building at 517 South Fifth Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1920, the vernacular building at 517 South Fifth Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 517 South Fifth Street is unlikely to provide information important to our understanding of history (Criterion D).

### For Historic Districts Only:

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

### For Individual Properties Only:

List the completed attachments related to the property's significance:

**Narrative Boundary Description:** The property is located at 517 South Fifth Street in Elizabeth, Union County, and associated with block 5 lot 1256. It is on the north side of South Fifth Street; Mattano Park is to the south.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

517 South Fifth Street, Elizabeth, Union County



South Corner

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 521 South Fifth Street

**Street Address:** Street #: 521 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: S Street Name: Fifth Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07206

**Municipality(s):** Elizabeth City **Block(s):** 5

**Local Place Name(s):** Elizabeth **Lot(s):** 1257

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement Date: May 3, 2007

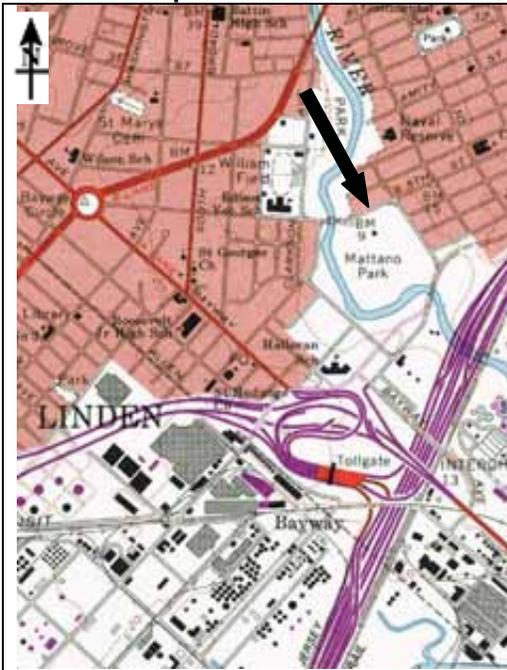
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Sanborn Fire Insurance Maps. Elizabeth, NJ; 1922/1923 and 1950.  
Maptech. Terrain Navigator, Version 7.03a, 1996-2006.  
Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at  
<http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

### INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** House, 521 South Fifth Street  
**Historic Name:** Unknown  
**Present Use:** Residential, Permanent  
**Historic Use:** Residential, Permanent  
**Construction Date:** c. 1920 **Source:** Stylistic evidence  
**Alteration Date(s):** N/A **Source:** N/A  
**Designer:** Unknown **Physical Condition:** Good  
**Builder:** Unknown **Remaining Historic Fabric:** Medium  
**Style:** Colonial Revival  
**Form:** Gable front **Stories:** 2  
**Type:** Other **Bays:** 3  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials:** Vinyl siding

**Exterior Description:** Two and a half stories in height, this wood-frame dwelling set on a stucco-covered foundation terminates in a front-gable asphalt-shingle roof. Featuring a rectangular-shaped footprint, the house has a two-story gable addition across the full width of the rear elevation. Across the main façade is a shed-roof enclosed porch three bays wide with an off-center single-leaf entry. The house is clad with vinyl siding and contains one-over-one replacement vinyl windows with aluminum surrounds. Above the roof are two stuccoed chimneys, one of which is corbeled. Other architectural features of the house include a full pediment on the facade and a gable dormer on the southwest elevation.

**Interior Description:** N/A

### Setting:

Facing southeast toward Mattano Park, this dwelling is on the north side of South Fifth Street. It is on a level grassy lot set just off the road and is fronted with several foundation plantings.

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. The property at 521 South Fifth Street in Elizabeth is associated with the "Metropolitan New Jersey" historic context.

**Significance:** The property at 521 South Fifth Street in Elizabeth contains a typical two-story, gable-front, Colonial Revival-style dwelling dating to circa 1920. During the first half of the twentieth century, the Colonial Revival style, as well as other types such as ecclesiastical and institutional, became the most popular style choice across the United States among middle- and upper-class Americans for their houses. Features such as a gable roof, molded cornice, full pediment, two stories are typical characteristics of Colonial Revival-style houses of this period as seen with this house. Many of the houses in this style are modest and lack any great amount of detail. The style's zenith occurred between the 1920s and 1950s and continues to be prevalent at the turn of the twenty-first century. This dwelling lacks many embellishments and is not an uncommon form in the area. Several other similar dwellings are located in the immediate area, e.g., neighboring houses at 517 and 525 South Fifth Street.

**Eligibility for New Jersey**

**and National Registers:**

Yes  No

**National**

**Register Criteria:**

A  B  C  D

**Level of Significance**

Local  State  National

**Justification of Eligibility/Ineligibility:** The building at 521 South Fifth Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1920, the vernacular building at 521 South Fifth Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 521 South Fifth Street is unlikely to provide information important to our understanding of history (Criterion D).

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

**Narrative Boundary Description:** The property is located at 521 South Fifth Street in Elizabeth, Union County. It is associated with block 5 lot 1257. It is located on the north side of South Fifth Street and Mattano Park is to the south of the property.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

521 South Fifth Street, Elizabeth, Union County



East Corner

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Property Name: 525 South Fifth Street

Street Address: Street #: 525 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: S Street Name: Fifth Suffix: \_\_\_\_\_ Type: ST

County(s): Union Zip Code: 07206

Municipality(s): Elizabeth City Block(s): 5

Local Place Name(s): Elizabeth Lot(s): 1258

Ownership: Private USGS Quad(s) Elizabeth

## Description:

See attachments

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

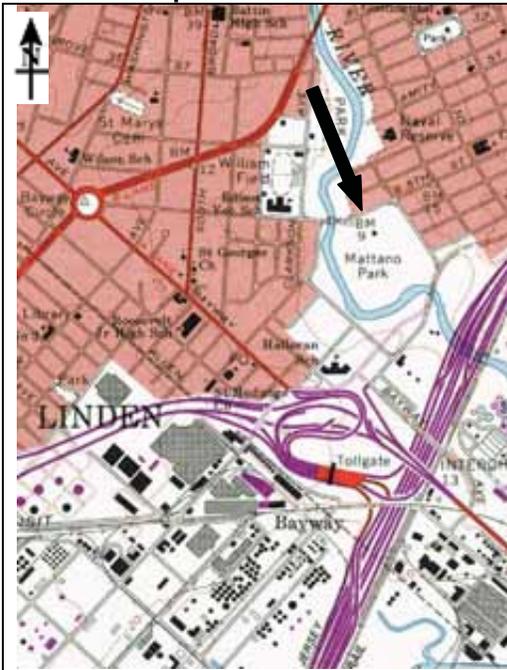
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Sanborn Fire Insurance Maps. Elizabeth, NJ; 1922/1923 and 1950.  
Maptech. Terrain Navigator, Version 7.03a, 1996-2006.  
Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at  
<http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

### INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** House, 525 South Fifth Street  
**Historic Name:** Unknown  
**Present Use:** Residential, Permanent  
**Historic Use:** Residential, Permanent  
**Construction Date:** c. 1920 **Source:** Stylistic evidence  
**Alteration Date(s):** N/A **Source:** N/A  
**Designer:** Unknown **Physical Condition:** Good  
**Builder:** Unknown **Remaining Historic Fabric:** Medium  
**Style:** Colonial Revival  
**Form:** Gable front **Stories:** 2.5  
**Type:** Other **Bays:** 2  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials** Wood, shingles

**Exterior Description:** Two and a half stories and two bays wide, this wood-frame dwelling features a large rectangular-shaped footprint and is capped with a front-gable roof. Clad with wood shingles, the house is five bays deep and is adorned with a hipped-roof aluminum-clad dormer atop its northeast elevation. The dormer contains a pair of one-over-one wood-sash windows. The façade has an off-center single-leaf entry and the first and second story contain banks of one-light casement windows. The side elevations are pierced with paired and single one-over-one wood sash as well as casement windows. The front gable is covered with vertical-board siding and the wide eaves feature a boxed cornice with partial returns. Above the roof is one interior stuccoed chimney.

**Interior Description:** N/A

### Setting:

Facing southeast toward Mattano Park, this dwelling is on the north side of South Fifth Street. It is on a level lot set just off the road and is fronted with several foundation plantings. A wood fence lines the perimeter of the property. A small one-story gable-roofed non-historic shed stands on the rear portion of the lot behind the house.

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. This multiple-family dwelling is associated with the "Metropolitan New Jersey" historic context.

**Significance:** The property at 525 South Fifth Street in Elizabeth contains a typical two-story gable-front, Colonial Revival-style dwelling dating to circa 1920. During the first half of the twentieth century, the Colonial Revival style became the most popular style choice across the United States among middle- and upper-class Americans for their houses, as well as other types such as ecclesiastical and institutional. Features such as a gable roof, molded cornice with returns, and two stories are typical characteristics of Colonial Revival-style houses of this period, as seen with this house. Many of the houses in this style are modest and lack any great amount of detail. The Colonial Revival style became a popular choice during the first half of the twentieth century. The style's zenith occurred between the 1920s and 1950s and continues to be prevalent at the turn of the twenty-first century. This dwelling lacks many embellishments and is not an uncommon form in the area. Several other similar dwellings are located in the immediate area, e.g., neighboring houses at 517 and 521 South Fifth Street.

**Eligibility for New Jersey**

**and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The building at 525 South Fifth Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1920, the vernacular building at 525 South Fifth Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 525 South Fifth Street is unlikely to provide information important to our understanding of history (Criterion D).

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

**Narrative Boundary Description:** The property is located at 525 South Fifth Street in Elizabeth, Union County. It is associated with block 5 lot 1258. It is on the north side of South Fifth Street; Mattano Park is to the south.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

525 South Fifth Street, Elizabeth, Union County



South Corner

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 539 South Fifth Street  
**Street Address:** Street #: 539 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)  
Prefix: S Street Name: Fifth Suffix: \_\_\_\_\_ Type: ST  
**County(s):** Union **Zip Code:** 07206  
**Municipality(s):** Elizabeth City **Block(s):** 5  
**Local Place Name(s):** Elizabeth **Lot(s):** 1261  
**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

**Registration and Status Dates:** National Historic Landmark: \_\_\_\_\_ SHPO Opinion: \_\_\_\_\_  
National Register: \_\_\_\_\_ Local Designation: \_\_\_\_\_  
New Jersey Register: \_\_\_\_\_ Other Designation: \_\_\_\_\_  
Determination of Eligibility: \_\_\_\_\_ Other Designation Date: \_\_\_\_\_

## Photograph:

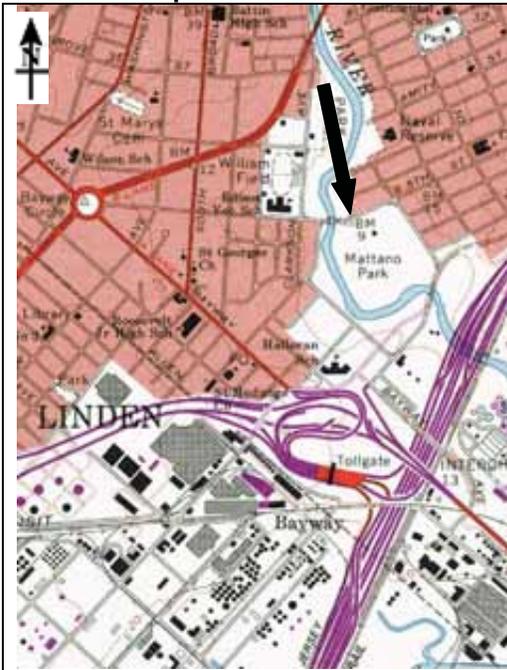


Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

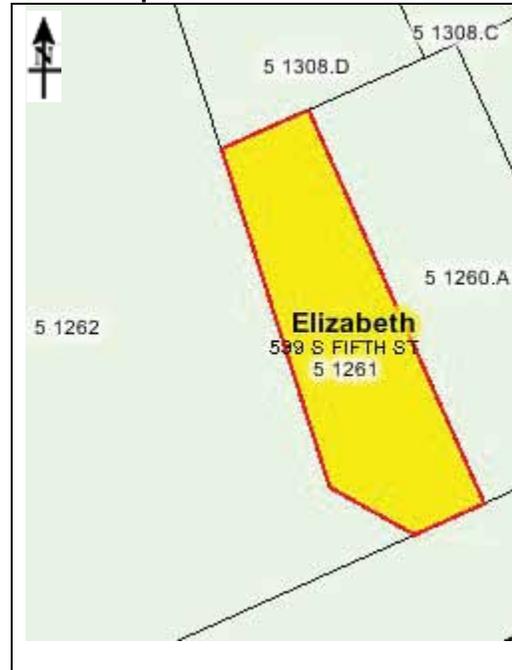
# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:**

Maptech. Terrain Navigator, Version 7.03a, 1996-2006.  
Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

Additional Information: N/A

More Research Needed?  Yes  No

**INTENSIVE LEVEL USE ONLY**

Attachments Included:  Building  Structure  Object  Bridge  
 Landscape  Industry

Within Historic District?  Yes  No

Status:  Key-Contributing  Contributing  Non-Contributing

Associated Archaeological Site/Deposit?  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** 539 South Fifth Street  
**Historic Name:** Unknown  
**Present Use:** Residential, Permanent; Commercial  
**Historic Use:** Residential, Permanent; Commercial  
**Construction Date:** c. 1940 **Source:** Stylistic evidence  
**Alteration Date(s):** N/A **Source:** N/A  
**Designer:** Unknown **Physical Condition:** Good  
**Builder:** Unknown **Remaining Historic Fabric:** Medium  
**Style:** None  
**Form:** Other **Stories:** 2  
**Type:** Other **Bays:** 3  
**Roof Finish Materials:** Unknown  
**Exterior Finish Materials** Brick, Running bond

**Exterior Description:** Built circa 1940, the mixed-use building at 539 South Fifth Street is two stories, three bays wide, and capped with a shed roof. The first story is used as an auto shop and the second as living quarters. Featuring a rectangular-shaped footprint, this building is stuccoed concrete-block construction on the first story and aluminum-clad wood-frame construction on the second story. The main elevation facing South Fifth Street is covered with a brick veneer. The façade's first story contains a roll-up garage door, a fixed one-light store window, and an off-center single-leaf door with a rectangular transom. The second story has a one one-over-one window and a tripartite window of one-over-one windows flanking a one-light fixed picture window. Other windows through the building are two-over-two horizontal wood-sash units. The southwest elevation has a two-story shed-roof addition enclosing what was originally an exterior stair to the second-floor apartment. This addition is covered with aluminum siding and the entry into the stairway is sheltered with a flat-roof portico supported with a metal post and balustrade.

**Interior Description:** N/A

**Setting:** Facing southeast toward Mattano Park, this building is on the north side South Fifth Street. It is on a level grassy lot adjacent to the road and to the west is the Elizabeth River.

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. This house is associated with the "Metropolitan New Jersey" historic context.

**Significance:** The property at 539 South Fifth Street in Elizabeth contains a typical vernacular mixed-use building dating to circa 1940 in Union County, New Jersey. This modest building functions as a dwelling and a store/auto repair shop. This dwelling lacks any embellishments and is not an uncommon form within the area. Several other mixed-use as well as two-story shed-roof buildings are located in the immediate area, such as the building at 511-513 South Fifth Street on the same block.

**Eligibility for New Jersey  
and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The building at 539 South Fifth Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1940, the vernacular building at 539 South Fifth Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 539 South Fifth Street is unlikely to provide information important to our understanding of history (Criterion D).

### For Historic Districts Only:

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

### For Individual Properties Only:

**List the completed attachments related to the property's significance:**

**Narrative Boundary Description:** The property is located at 539 South Fifth Street in Elizabeth, Union County. It is associated with block 5 lot 1261. It is located on the north side of South Fifth Street and Mattano Park is to the south of the property.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

539 South Fifth Street, Elizabeth, Union County



South Corner

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** Bayway Switching Station

**Street Address:** Street #: 530 614 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Trenton Suffix: \_\_\_\_\_ Type: AVE

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 1582

**Ownership::** Private **USGS Quad(s)** Elizabeth

**Description:**

See industrial attachment.

**Registration and Status Dates:**

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

**Photograph:**



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

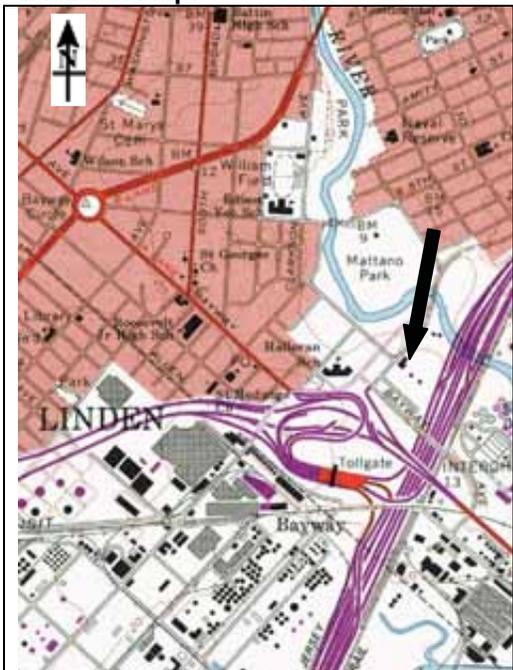
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Location Map:



Site Map:



### Bibliography/Sources:

Maptech. Terrain Navigator, Version 7.03a, 1996-2006.  
Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

Additional Information: N/A

More Research Needed?  Yes  No

### INTENSIVE LEVEL USE ONLY

Attachments Included:  Building  Structure  Object  Bridge  
 Landscape  Industry

Within Historic District?  Yes  No

Status:  Key-Contributing  Contributing  Non-Contributing

Associated Archaeological Site/Deposit?  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## INDUSTRIAL BUILDING ATTACHMENT

Historic Sites #:

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**Common Name:** Bayway Switching Station

**Historic Name:** Public Service Electric and Gas Company

**Present Use:** Industrial

**Historic Industry:** Electrical and Gas Utilities      **Building ID:** \_\_\_\_\_

**Construction Date:** c. 1955      **Source:** Stylistic and material evidence

**Alteration Date(s):** N/A      **Source:** N/A

**Designer:** Unknown      **Physical Condition:** Good

**Builder:** Unknown      **Remaining Historic Fabric:** Good

**Style:** N/A

**Length:** N/A      **Stories:** 4

**Width:** N/A      **Bays:** 5

**Exterior Finish Materials** Brick, American bond

**Foundation Materials:** Unknown

**Structural System:** Unknown      **Roof System:** Unknown

**Roof Finish Materials:** Unknown

**Equipment/Machinery:** Unknown

**Transportation Links:** N/A

**Exterior Description:** The Bayway Switching Station at 530-614 Trenton Avenue is an L-shaped brick building comprising four sections terminating in a flat roof. A four-story square block comprises the central portion of the building. A two-story rectangular wing is appended to main block's southern side. A smaller one-story wing ell adjoins the southeast elevation of the two-story wing. The northern side of the main block features a smaller one-story rectangular wing. The northern side of the main block features a smaller one-story rectangular wing. The building features light brown brick walls laid in five-course American bond with darker brown brick corbelled quoins on the four- and two-story sections of the building. A red brick water table crowned by a cast stone belt course encircles the building. The four-story block has slightly off-center double roll-up metal doors that span both the first and second stories. This large bay is detailed with a corbeled surround and a cast stone lintel with circular corner blocks. Above the door at the third story is a circular cast stone cartouche with "Public Service" in relief. This section is also detailed with wide corner window openings with cast stone sills on the third and fourth stories. *See Continuation Sheet.*

**Interior Description:** N/A

**Setting:** The Bayway Switching Station is on a large level lot to the west of I-95 and the Elizabeth River. The property is a large power station and is paved with asphalt.

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Survey Name: Goethals Bridge Replacement      Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. The Bayway Switching Station in Elizabeth is associated with the New Jersey "Modern New Jersey" historic context.

**Significance:** The Bayway Switching Station in Elizabeth contains a typical mid-twentieth-century switching station in New Jersey. Reflecting a modern, simplistic style, this building lacks many embellishments and is not an uncommon form in New Jersey.

**Eligibility for New Jersey  
and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The Bayway Switching Station at 530 Trenton Avenue in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1955, the vernacular building at 530 Trenton Avenue does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The Bayway Switching Station is unlikely to provide information important to our understanding of history (Criterion D).

### For Historic Districts Only:

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

### For Individual Properties Only:

**List the completed attachments related to the property's significance:**

Industrial Attachment

**Narrative Boundary Description:** The Bayway Switching Station is located at 530 Trenton Avenue in Elizabeth, Union County. It is associated with block 4 lot 1582. It is located on the south side of the Elizabeth River. It is bounded to the east and southeast by I-95, to the northwest and southwest by Trenton Avenue.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

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### Exterior Description

The openings are now covered with corrugated fiberglass panels. The two-story block features a centered single-leaf door illuminated with sidelights sheltered with a small metal hood. The doorway is flanked with one-over-one aluminum windows. The second story is detailed with paired one-over-one aluminum windows with a brick corbeled spandrel, a corbeled surround, and a cast stone lintel with round corner blocks. At the roof line is a sign that reads "P.S.E.G.," which stands for Public Service Electric and Gas Company. The southwest elevation is pierced with six bays with both individual one-over-one window units and one-over-one units grouped into four. A corbelled pier divides the elevation into bays on the west and two bays on its east side. Each of these bays is supported with cast stone sills. The one-story southeast wing displays paired metal slab pedestrian doors on its western elevation. The one-story northern block features four bays on its main facade. Each bay holds triple one-over-one aluminum windows. Between each bay near the roof line are inset cast stone panels. Each block features a cast stone cornice and a cast stone belt course.

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Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# CONTINUATION SHEET

Historic Sites #:

Bayway Switching Station, Elizabeth, Union County



North Corner



Looking Southeast

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** City of Elizabeth Pumping Station

**Street Address:** Street #: 500 526 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Trenton Suffix: \_\_\_\_\_ Type: AVE

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 1582.A

**Ownership::** Public **USGS Quad(s)** Elizabeth

## Description:

See attachments

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement Date: May 3, 2007

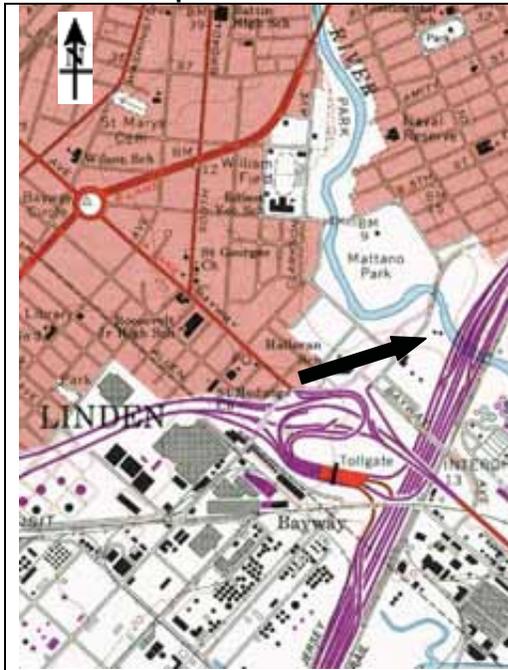
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

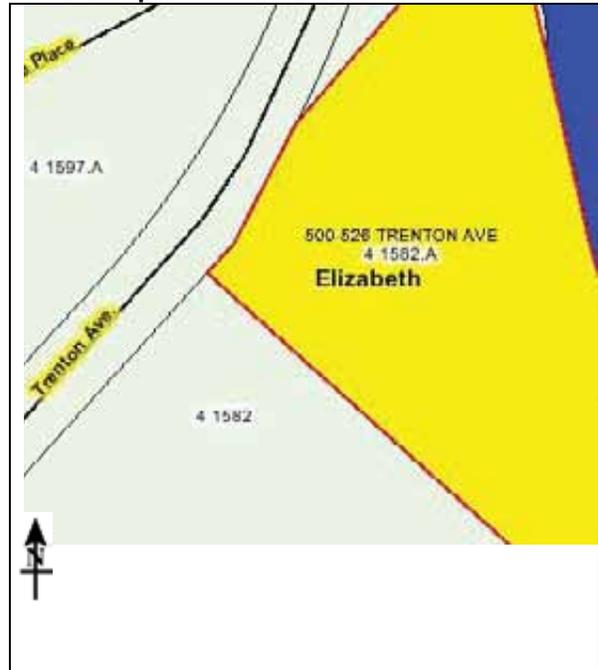
# BASE FORM

Historic Sites #:

Location Map:



Site Map:



### Bibliography/Sources:

Maptech. Terrain Navigator, Version 7.03a, 1996-2006.  
Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

Additional Information: N/A

More Research Needed?  Yes  No

### INTENSIVE LEVEL USE ONLY

Attachments Included:  Building  Structure  Object  Bridge  
 Landscape  Industry

Within Historic District?  Yes  No

Status:  Key-Contributing  Contributing  Non-Contributing

Associated Archaeological Site/Deposit?  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.



## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. The City of Elizabeth Pumping Station in Elizabeth is associated with the New Jersey "Metropolitan New Jersey" context.

**Significance:** The Elizabeth Pumping Station is a typical circa 1940 government building in Union County, New Jersey. Reflecting a simplistic style and form, this building lacks many embellishments and is not an uncommon form in New Jersey. Its reflects a modest vernacular Art Deco style with its defined entryway; however, it is not an excellent representation of the style.

**Eligibility for New Jersey**

**and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The City of Elizabeth Pumping Station at 500 Trenton Avenue in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1940, the vernacular building at 500 Trenton Avenue does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The Pumping Station is unlikely to provide information important to our understanding of history (Criterion D).

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

Industrial Attachment

**Narrative Boundary Description:** The City of Elizabeth Pumping Station is located at 500 Trenton Avenue in Elizabeth, Union County. It is associated with block 4 lot 1582A. It is located on the south and southwest side of the Elizabeth River. It is bounded to the east and southeast by I-95, to the northwest and southwest by Trenton Avenue.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# CONTINUATION SHEET

Historic Sites #:

City of Elizabeth Pumping Station, Elizabeth, Union County



Southwest Elevation



Looking North

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

City of Elizabeth Pumping Station, Elizabeth, Union County



West Elevation, Looking East

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 630 Clarkson Avenue

**Street Address:** Street #: 630 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Clarkson Suffix: \_\_\_\_\_ Type: AVE

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 378.A

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

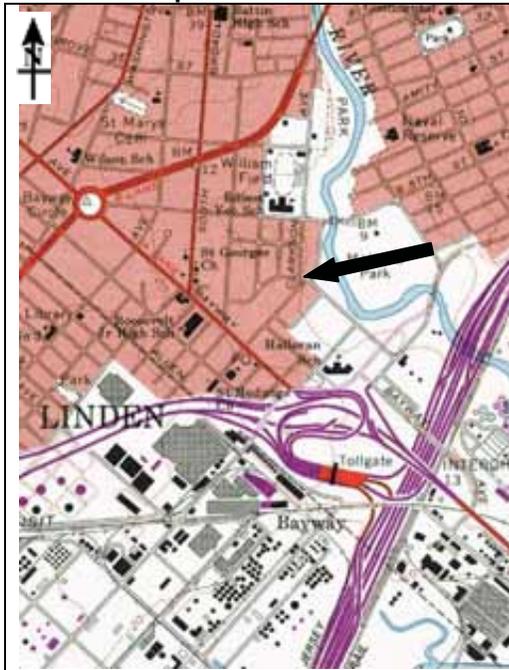
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Maptech. Terrain Navigator, Version 7.03a, 1996-2006.

Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

## INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** Apartment Building, 630 Clarkson Avenue  
**Historic Name:** Unknown  
**Present Use:** Residential, Permanent; Commercial  
**Historic Use:** Residential, Permanent; Commercial  
**Construction Date:** c. 1940 **Source:** Stylistic evidence  
**Alteration Date(s):** N/A **Source:** N/A  
**Designer:** Unknown **Physical Condition:** Good  
**Builder:** Unknown **Remaining Historic Fabric:** Medium  
**Style:** None  
**Form:** Other **Stories:** 2  
**Type:** Other **Bays:** 10  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials** Brick, stretcher bond

**Exterior Description:** Two stories in height, the building at 630 Clarkson Avenue is covered in stretcher-bond brick and features a rectangular-shaped footprint with a corner entry. Set on a stuccoed foundation, this mixed-use building has a restaurant on the first floor and apartments on the second floor. The corner entry accesses the restaurant and contains a metal and glass door flanked with glass store windows. The original façade, which faces south, originally had a one-story brick wing, which now has a second-story wood-frame addition. The addition has one-over-one vinyl windows on the south elevation and tripartite windows of one-over-one units flanking a one-light fixed window on the east elevation. The façade is 10 bays across and contains paired metal and glass doors sheltered by a vinyl awning and illuminated with a large fixed transom. The building contains one-over-one replacement vinyl-sash windows with brick soldier lintels and rowlock sills. The east elevation shows the ghost of a small one-bay-wide gable wing. Rising above the hipped asphalt-shingle roof is one exterior brick chimney located on the rear elevation.

**Interior Description:** N/A

**Setting:** The mixed-use building at 630 Clarkson Avenue is located at the north corner of Arnett Street, Clifton Street, and Clarkson Avenue. It is on a level lot with a partial grassy yard, mature trees, foundation plantings, and an asphalt-paved parking lot at the east and north sides.

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. This multiple-family dwelling is associated with the "Modern New Jersey" historic context.

**Significance:** The property at 630 Clarkson Avenue is a typical vernacular apartment building dating to circa 1940 in Union County, New Jersey. This modest building lacks any embellishment and is not an uncommon form within the area.

**Eligibility for New Jersey**

**and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The apartment building at 630 Clarkson Avenue in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1940, this Colonial Revival-style multiple dwelling at 630 Clarkson Avenue does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The multiple dwelling at 630 Clarkson Avenue is unlikely to provide information important to our understanding of history (Criterion D). Its integrity has been diminished with the second-story addition on the wing and replacement vinyl windows.

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

**Narrative Boundary Description:** The multiple-family dwelling at 630 Clarkson Avenue in Elizabeth, Union County. It is associated with block 4 lot 378A. It is located at the north corner of Arnett Street, Clarkson Avenue, and Clifton Street. It is bounded to the east by Arnett Street and to the west by Clarkson Avenue, and to the southeast is Clifton Street.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

630 Clarkson Avenue, Elizabeth, Union County



East Elevation

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 627 Arnett Street

**Street Address:** Street #: 627 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Arnett Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 58

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

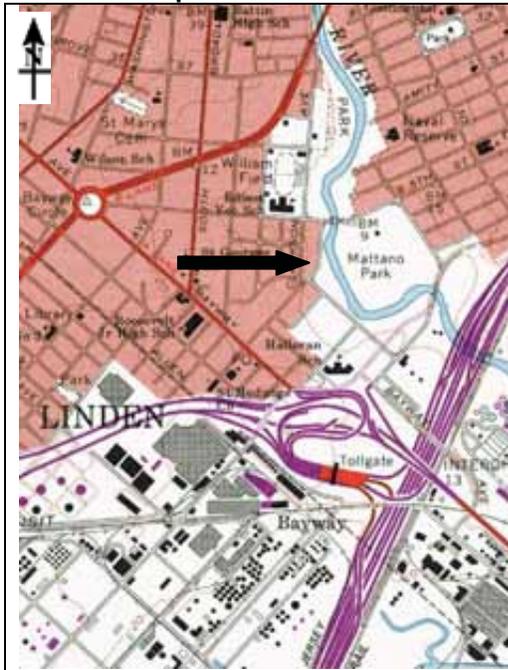
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Maptech. Terrain Navigator, Version 7.03a, 1996-2006.

Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

### INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** Apartment Building, 627 Arnett Street  
**Historic Name:** Unknown  
**Present Use:** Residential, Permanent  
**Historic Use:** Residential, Permanent  
**Construction Date:** c. 1950      **Source:** Stylistic evidence  
**Alteration Date(s):** N/A      **Source:** N/A  
**Designer:** Unknown      **Physical Condition:** Good  
**Builder:** Unknown      **Remaining Historic Fabric:** Medium  
**Style:** None  
**Form:** Other      **Stories:** 2  
**Type:** Other      **Bays:** 3  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials** Brick, stretcher bond

**Exterior Description:** Built circa 1955, this multiple-family brick dwelling has a rectangular-shaped footprint and is covered with a stretcher-bond brick. Three stories and three bays wide, the symmetrical façade has a central single-leaf door illuminated with three-light and one-panel sidelights. Sheltering the entrance is a hipped-roof portico supported with metal posts. Each of the three stories on the façade contains tripartite windows of one-over-one vinyl windows flanking one-light fixed picture window units. Above the door is a one-over-one vinyl-sash window, most likely illuminating an interior stairway. The side elevations, which are four bays deep, have both paired one-over-one and individual one-over-one vinyl windows. All windows are visually supported with brick rowlock sills. The six-apartment building terminates in a hipped asphalt-shingle roof with wide eaves. There is one brick interior chimney.

**Interior Description:** N/A

### Setting:

This dwelling is sited on a level grassy lot with a driveway to the north side of the house. It is set just back from the road and is fronted with foundation plantings.

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Survey Name: Goethals Bridge Replacement      Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. This multiple-family dwelling is associated with the "Modern New Jersey" historic context.

**Significance:** The property at 627 Arnett Street is a typical vernacular apartment building dating to circa 1950 in Union County, New Jersey. This modest building lacks any embellishment and is not an uncommon form within the area.

**Eligibility for New Jersey  
and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The multiple-family dwelling at 627 Arnett Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1950, this multiple-family dwelling at 627 Arnett Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The multiple dwelling at 627 Arnett Street is unlikely to provide information important to our understanding of history (Criterion D).

### For Historic Districts Only:

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

### For Individual Properties Only:

List the completed attachments related to the property's significance:

**Narrative Boundary Description:** The multiple-family dwelling at 627 Arnett Street in Elizabeth, Union County. It is associated with block 4 lot 58. It is located on the west side of Arnett Street.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

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627 Arnett Street, Elizabeth, Union County



Southwest Corner

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 625 Arnett Street

**Street Address:** Street #: 625 Apartment #: A B  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Arnett Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth **Block(s):** 4

**Local Place Name(s):** N/A **Lot(s):** 57.H and 57.G

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

**Registration and Status Dates:** National Historic Landmark: \_\_\_\_\_ SHPO Opinion: \_\_\_\_\_  
National Register: \_\_\_\_\_ Local Designation: \_\_\_\_\_  
New Jersey Register: \_\_\_\_\_ Other Designation: \_\_\_\_\_  
Determination of Eligibility: \_\_\_\_\_ Other Designation Date: \_\_\_\_\_

## Photograph:

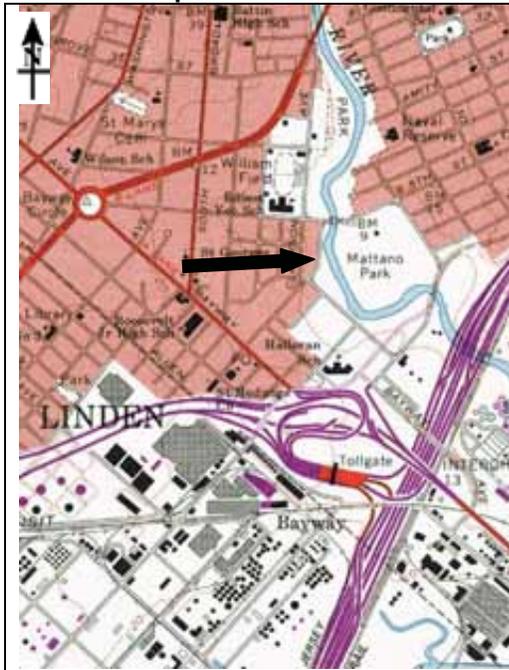


Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

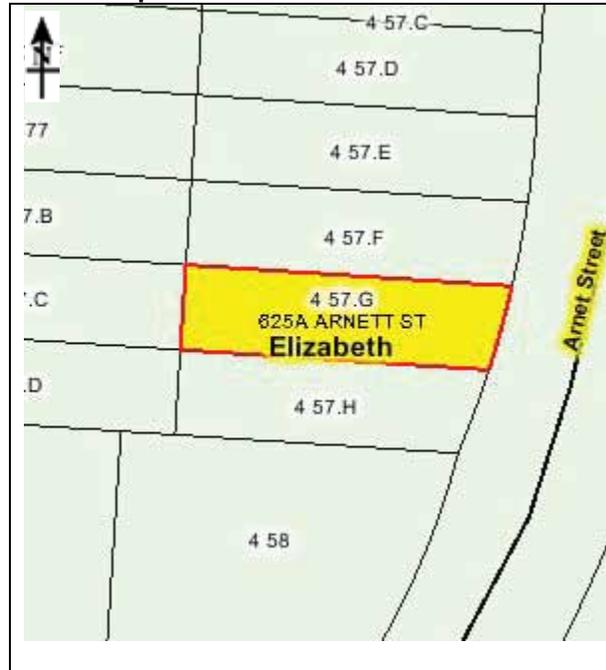
# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Maptech. Terrain Navigator, Version 7.03a, 1996-2006.

Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

### INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** 625 Arnett Street  
**Historic Name:** Unknown  
**Present Use:** Residential, Permanent  
**Historic Use:** Residential, Permanent  
**Construction Date:** c. 1950      **Source:** Stylistic evidence  
**Alteration Date(s):** N/A      **Source:** N/A  
**Designer:** Unknown      **Physical Condition:** Good  
**Builder:** Unknown      **Remaining Historic Fabric:** Medium  
**Style:** Colonial Revival  
**Form:** Duplex      **Stories:** 2  
**Type:** Other      **Bays:** 4  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials** Brick, American bond

**Exterior Description:** Two stories and four bays wide, the twin dwelling at 625 Arnett Street is constructed of wood-frame and features a rectangular-shaped footprint. The foundation was not visible at the time of the survey. This modest Colonial Revival-style house built circa 1950 is two bays deep and terminates in a side-gable asphalt-shingle roof. The brick-veneer facade has a six-course American bond and features a soldier stringcourse immediately below the second-story window brick rowlock sills. Two centered single-leaf six-panel wood doors are sheltered with an asphalt-shingle quarter-hipped roof portico supported with decorative metal posts. Windows throughout the house are one-over-one replacement vinyl-sash windows. The side and rear elevations are clad with vinyl siding. Vinyl also encloses the exterior chimney piles rising along each gable end.

**Interior Description:** N/A

### Setting:

Facing east, this dwelling is sited on a level grassy lot, and driveways are on both sides of the dwelling. A few mature trees are located in the backyard of the property.

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Survey Name: Goethals Bridge Replacement      Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. This dwelling is associated with the "Modern New Jersey" historic context.

**Significance:** The property at 625 Arnett Street in Elizabeth contains a typical two-story side-gable Colonial Revival-style dwelling dating to circa 1950 in Union County, New Jersey. During the first half of the twentieth century, the Colonial Revival style became the most popular stylistic choice across the United States. Features such as a gable roof, brick construction, and symmetrical fenestration with a central entry are typical characteristics of Colonial Revival-style houses of this period. Many of the houses in this style are modest and lack any great amount of detail. The Colonial Revival style became a popular choice among middle- and upper-class Americans for their houses, as well as other types such as ecclesiastical and institutional, during the first half of the twentieth century. The style's zenith occurred between the 1920s and 1950s and continues to be prevalent at the turn of the twenty-first century. The dwelling lacks embellishments and is not an uncommon form within the area. This dwelling is similar to each of the duplex houses on this block, suggesting that it was part of speculative construction by one developer. Speculative construction following World War II was not uncommon in Union County.

**Eligibility for New Jersey**

**and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The duplex at 625 Arnett Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1950, this multiple-family dwelling at 625 Arnett Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The multiple dwelling at 625 Arnett Street is unlikely to provide information important to our understanding of history (Criterion D).

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

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**Narrative Boundary Description:** The duplex at 625 Arnett Street in Elizabeth, Union County. It is associated with block 4 lot 57H and 57G. It is located on the west side of Arnett Street.

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

625 Arnett Street, Elizabeth, Union County



Southwest Corner

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 623 Arnett Street

**Street Address:** Street #: 625 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Arnett Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 57.F

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

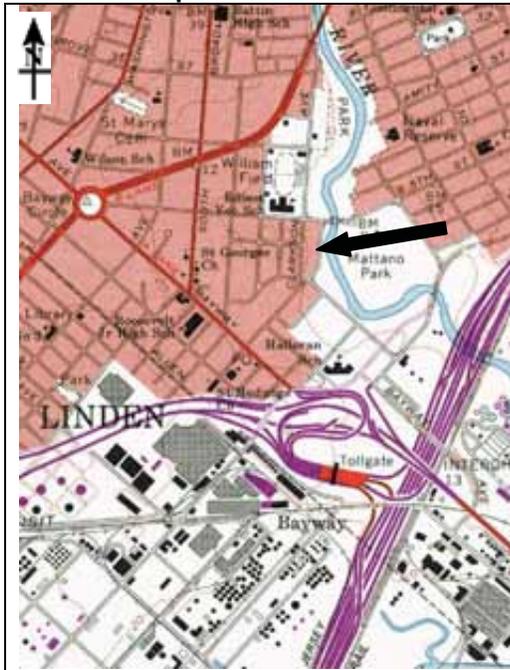
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

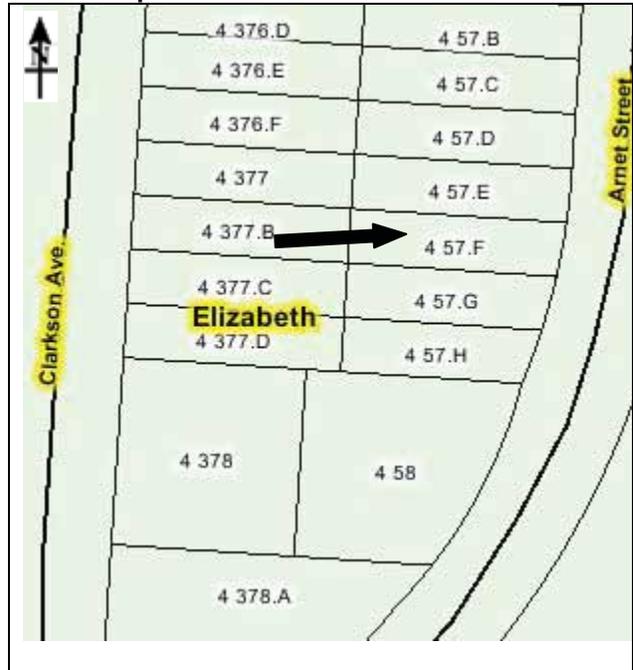
# BASE FORM

Historic Sites #:

**Location Map:**



**Site Map:**



**Bibliography/Sources:** Maptech. Terrain Navigator, Version 7.03a, 1996-2006.

Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

**INTENSIVE LEVEL USE ONLY**

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
 Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
 Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** 623 Arnett Street

**Historic Name:** Unknown

**Present Use:** Residential, Permanent

**Historic Use:** Residential, Permanent

**Construction Date:** c. 1950      **Source:** Stylistic evidence

**Alteration Date(s):** N/A      **Source:** N/A

**Designer:** Unknown      **Physical Condition:** Good

**Builder:** Unknown      **Remaining Historic Fabric:** Medium

**Style:** Colonial Revival

**Form:** Duplex      **Stories:** 2

**Type:** Other      **Bays:** 4

**Roof Finish Materials:** Asphalt shingle

**Exterior Finish Materials** Brick, American bond

**Exterior Description:** Two stories and four bays wide, the twin dwelling at 623 Arnett Street is constructed of wood-frame and features a rectangular-shaped footprint. The foundation was not visible at the time of the survey. This modest Colonial Revival-style house built circa 1950 is two bays deep and terminates in a side-gable asphalt-shingle roof. The brick-veneer facade has a six-course American bond and features a soldier stringcourse immediately below the first story window brick rowlock sills. Two centered single-leaf modern doors are sheltered by an asphalt-shingled, shed-roof portico supported with decorative metal posts. Windows throughout the house are one-over-one aluminum sash. The side elevations are covered with stretcher-bond brick and both have an exterior-end brick flue. The south end of the house has a one-story shed-roof porch addition on the rear and the north end has a two-story gable-roof addition clad with vinyl siding on the rear elevation.

**Interior Description:** N/A

### Setting:

Facing east, this dwelling is sited on a level grassy lot and is fronted with foundation plantings. An asphalt-paved driveway is on the south side of the house and a concrete driveway is on the north side.

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Survey Name: Goethals Bridge Replacement      Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. This dwelling is associated with the "Modern New Jersey" historic context.

**Significance:** The property at 623 Arnett Street in Elizabeth contains a typical two-story side-gable Colonial Revival-style dwelling dating to circa 1950 in Union County, New Jersey. During the first half of the twentieth century, the Colonial Revival style became the most popular style choice across the United States among middle- and upper-class Americans for their houses, as well as other types such as ecclesiastical and institutional. Features such as a gable roof, brick construction, and symmetrical fenestration with a central entry are typical characteristics of Colonial Revival-style houses of this period. Many of the houses in this style are modest and lack any great amount of detail. The style's zenith occurred between the 1920s and 1950s and continues to be prevalent at the turn of the twenty-first century. The dwelling lacks any significant embellishments and is not an uncommon form within the area. The duplex at 623 Arnett Street is similar to each of the duplex houses on this block, suggesting that it was part of speculative construction by a developer or contractor. Speculative construction following World War II was not uncommon in Union County.

**Eligibility for New Jersey**

**and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The duplex at 623 Arnett Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1950, this multiple-family dwelling at 623 Arnett Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The multiple dwelling at 623 Arnett Street is unlikely to provide information important to our understanding of history (Criterion D).

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

**Narrative Boundary Description:** The duplex at 623 Arnett Street in Elizabeth, Union County. It is associated with block 4 lot 57F. It is located on the west side of Arnett Street.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

623 Arnett Street, Elizabeth, Union County



Northeast Corner

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 619 Arnett Street

**Street Address:** Street #: 619 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Arnett Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 57.C and 57.D

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

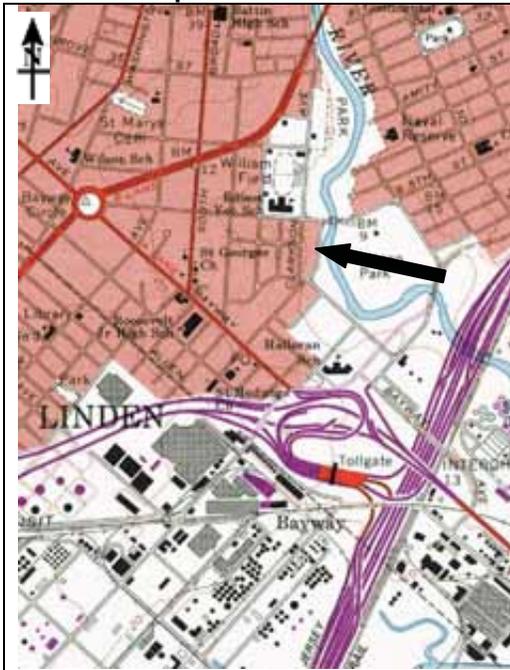
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

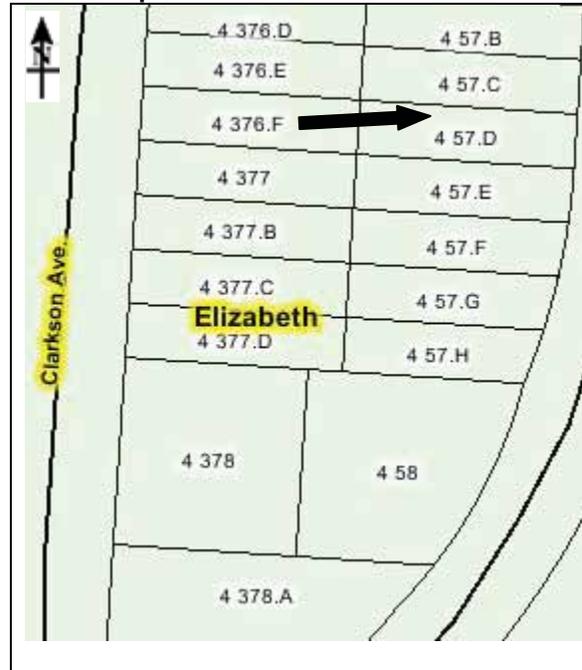
**BASE FORM**

**Historic Sites #:**

**Location Map:**



**Site Map:**



**Bibliography/Sources:** Maptech. Terrain Navigator, Version 7.03a, 1996-2006.

Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

**INTENSIVE LEVEL USE ONLY**

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
 (Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
 Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
 Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** 619 Arnett Street  
**Historic Name:** Unknown  
**Present Use:** Residential, Permanent  
**Historic Use:** Residential, Permanent  
**Construction Date:** c. 1950 **Source:** Stylistic evidence  
**Alteration Date(s):** N/A **Source:** N/A  
**Designer:** Unknown **Physical Condition:** Good  
**Builder:** Unknown **Remaining Historic Fabric:** Medium  
**Style:** Colonial Revival  
**Form:** Duplex **Stories:** 2  
**Type:** Other **Bays:** 4  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials** Brick, American bond

**Exterior Description:** Two stories and four bays wide, the twin dwelling at 619 Arnett Street is constructed of wood-frame and features a rectangular-shaped footprint. The foundation was not visible at the time of the survey. This modest Colonial Revival-style house built circa 1950 is two bays deep and terminates in a side-gable asphalt-shingle roof. The brick-veneer façade has a six-course American bond and features a soldier stringcourse immediately below the second-story window brick rowlock sills. Two centered single-leaf paneled doors are sheltered with an asphalt-shingle gable-roof portico supported with decorative metal posts. Windows throughout the house are one-over-one vinyl sash. The south elevation is covered with stretcher-bond brick and possesses an exterior-end brick flue. Vinyl siding covers the north elevation and its chimney pile.

**Interior Description:** N/A

### Setting:

Facing east, this dwelling is sited on a level grassy lot and is fronted with foundation plantings. An asphalt-paved driveway is on the north side of the house and a concrete driveway is on the south side.

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. This dwelling is associated with the "Modern New Jersey" historic context.

**Significance:** The property at 619 Arnett Street in Elizabeth contains a typical two-story side-gable Colonial Revival-style dwelling dating to circa 1950. During the first half of the twentieth century, the Colonial Revival style became the most popular stylistic choice across the United States among middle- and upper-class Americans for their houses, as well as other types such as ecclesiastical and institutional. Features such as a gable roof, brick construction, and symmetrical fenestration with a central entry are typical characteristics of Colonial Revival-style houses of this period. Many of the houses in this style are modest and lack any great amount of detail. The style's zenith occurred between the 1920s and 1950s and continues to be prevalent at the turn of the twenty-first century. The dwelling lacks any significant embellishments and is not an uncommon form within the area. The duplex at 619 Arnett Street is similar to each of the duplex houses on this block, suggesting that it was part of speculative construction by a developer or contractor. Speculative construction following World War II was not uncommon in Union County.

**Eligibility for New Jersey  
and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The duplex at 619 Arnett Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1950, this multiple-family dwelling at 619 Arnett Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The multiple dwelling at 619 Arnett Street is unlikely to provide information important to our understanding of history (Criterion D).

### For Historic Districts Only:

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

### For Individual Properties Only:

List the completed attachments related to the property's significance:

**Narrative Boundary Description:** The duplex at 619 Arnett Street in Elizabeth, Union County. It is associated with block 4 lot 57C and 57D. It is located on the west side of Arnett Street.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

619 Arnett Street, Elizabeth, Union County



Northeast Corner

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 617 Arnett Street

**Street Address:** Street #: 619 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Arnett Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 57 and 58.B

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

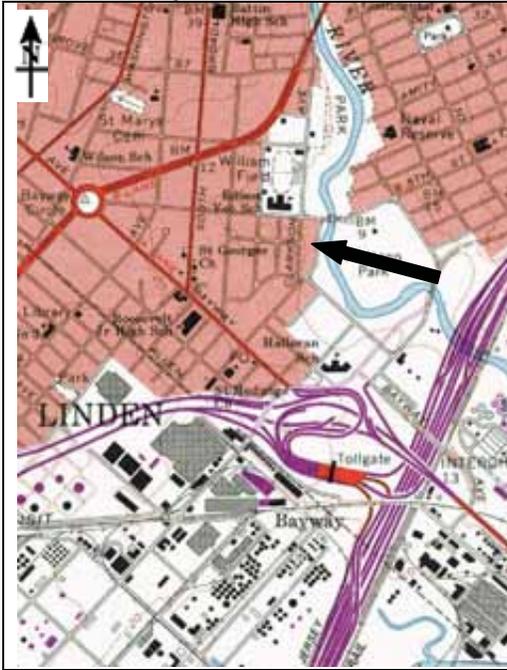
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

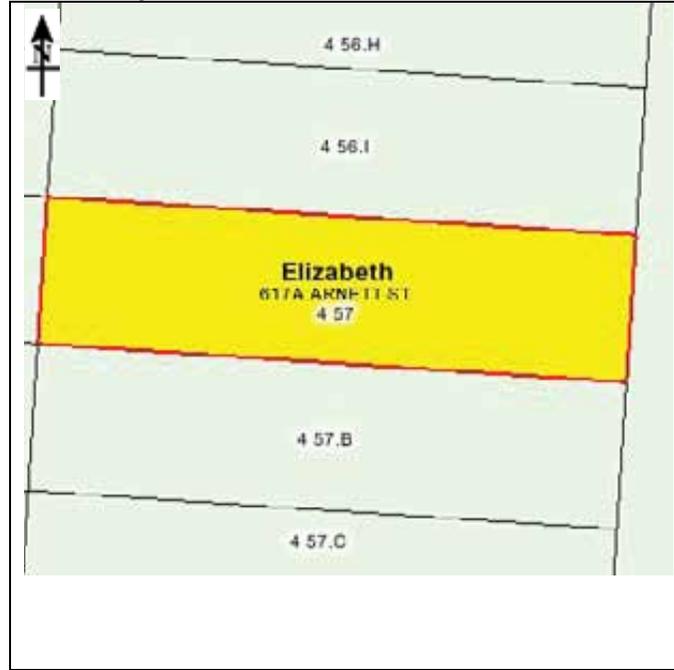
# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Maptech. Terrain Navigator, Version 7.03a, 1996-2006.

Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

### INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** 617 Arnett Street  
**Historic Name:** Unknown  
**Present Use:** Residential, Permanent  
**Historic Use:** Residential, Permanent  
**Construction Date:** c. 1950      **Source:** Stylistic evidence  
**Alteration Date(s):** N/A      **Source:** N/A  
**Designer:** Unknown      **Physical Condition:** Good  
**Builder:** Unknown      **Remaining Historic Fabric:** Medium  
**Style:** Colonial Revival  
**Form:** Duplex      **Stories:** 2  
**Type:** Other      **Bays:** 4  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials** Brick, American bond

**Exterior Description:** Two stories and four bays wide, the twin dwelling at 617 Arnett Street is constructed of wood frame and features a rectangular-shaped footprint. The foundation was not visible at the time of the survey. This modest Colonial Revival-style house built circa 1950 is two bays deep and terminates in a side-gable asphalt-shingle roof. The brick-veneer facade has six-course American bond and features a soldier stringcourse immediately below the second-story window brick rowlock sills. Two centered single-leaf paneled doors are sheltered with an asphalt-shingle shed-roof portico supported with decorative metal posts. Windows throughout the house are one-over-one vinyl sash. The side elevations are covered with vinyl siding and have an exterior-end chimney enclosed in vinyl siding. The north end of the dwelling has a shed-roof porch addition on the rear. The south end has a shed portico sheltering a side entry.

**Interior Description:** N/A

### Setting:

Facing east, this dwelling is sited on a level grassy lot and has asphalt-paved driveways on both the north and south sides of the house.

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Survey Name: Goethals Bridge Replacement      Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. This dwelling is associated with the "Modern New Jersey" historic context.

**Significance:** The property at 617 Arnett Street in Elizabeth contains a typical two-story side-gable Colonial Revival-style dwelling dating to circa 1950 in Union County, New Jersey. During the first half of the twentieth century, the Colonial Revival style became the most popular style choice across the United States among middle- and upper-class Americans for their houses, as well as other types such as ecclesiastical and institutional. Features such as a gable roof, brick construction, and symmetrical fenestration with a central entry are typical characteristics of Colonial Revival-style houses of this period. Many of the houses in this style are modest and lack any great amount of detail. The style's zenith occurred between the 1920s and 1950s and continues to be prevalent at the turn of the twenty-first century. The duplex at 617 Arnett Street lacks any significant embellishments and is not an uncommon form within the area. It is similar to each of the duplex houses on this block, suggesting that it was part of speculative construction by a developer or contractor. Speculative construction following World War II was not uncommon in Union County.

**Eligibility for New Jersey**

**and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The duplex at 617 Arnett Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1950, this multiple-family dwelling at 617 Arnett Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The multiple dwelling at 617 Arnett Street is unlikely to provide information important to our understanding of history (Criterion D).

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

**Narrative Boundary Description:** The duplex at 617 Arnett Street in Elizabeth, Union County. It is associated with block 4 lot 57 and 58B. It is located on the west side of Arnett Street.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

617 Arnett Street, Elizabeth, Union County



Northeast Corner

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 615 Arnett Street

**Street Address:** Street #: 615 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Arnett Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 56.H and 56.I

**Ownership:** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

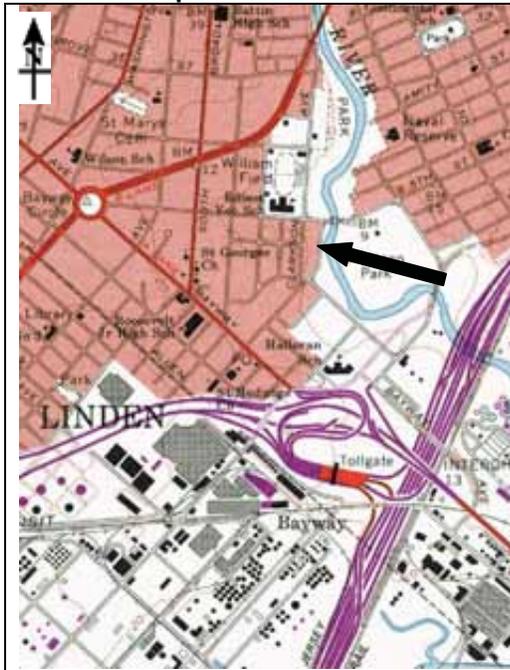
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

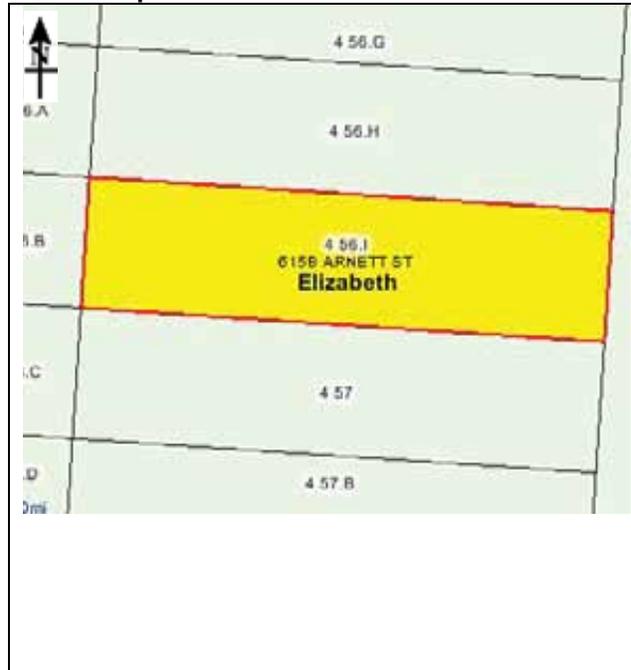
# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Maptech. Terrain Navigator, Version 7.03a, 1996-2006.

Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

## INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** 615 Arnett Street  
**Historic Name:** Unknown  
**Present Use:** Residential, Permanent  
**Historic Use:** Residential, Permanent  
**Construction Date:** c. 1950 **Source:** Stylistic evidence  
**Alteration Date(s):** N/A **Source:** N/A  
**Designer:** Unknown **Physical Condition:** Good  
**Builder:** Unknown **Remaining Historic Fabric:** Medium  
**Style:** Colonial Revival  
**Form:** Duplex **Stories:** 2  
**Type:** Other **Bays:** 4  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials** Brick, American bond

**Exterior Description:** Two stories and four bays wide, the twin dwelling at 615 Arnett Street is constructed of wood frame and features a rectangular-shaped footprint. The foundation was not visible at the time of the survey. This modest Colonial Revival-style house built circa 1950 is two bays deep and terminates in a side-gable, asphalt-shingle roof. The brick-veneer façade has a six-course American bond and features a soldier stringcourse immediately below the first-story window brick rowlock sills. Two centered single-leaf paneled doors are sheltered by an asphalt-shingled gable-roof portico supported with square-wood posts. Windows on the south end are six-over-one vinyl-sash units and one-over-one vinyl-sash on the north end. The south elevation is stucco-covered and has an exterior-end stuccoed flue. The north side is clad with stretcher-bond brick and features an exterior-end brick flue. The north end of the dwelling has a shed-roof porch addition on the rear.

**Interior Description:** N/A

### Setting:

Facing east, this dwelling is sited on a level grassy lot and has asphalt-paved driveways on both the north and south sides of the house. A chain link fence lines the south side of the property.

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. South of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. This dwelling is associated with the "Modern New Jersey" historic context.

**Significance:** The property at 615 Arnett Street in Elizabeth contains a typical two-story side-gable Colonial Revival-style dwelling dating to circa 1950 in Union County, New Jersey. During the first half of the twentieth century, the Colonial Revival style became the most popular stylistic choice across the United States among middle- and upper-class Americans for their houses, as well as other types such as ecclesiastical and institutional. Features such as a gable roof, brick construction, and symmetrical fenestration with a central entry are typical characteristics of Colonial Revival-style houses of this period. Many of the houses in this style are modest and lack any great amount of detail. The style's zenith occurred between the 1920s and 1950s and continues to be prevalent at the turn of the twenty-first century. The duplex at 615 Arnett Street lacks any significant embellishments and is not an uncommon form within the area. It is similar to each of the duplex houses on this block, suggesting that it was part of speculative construction by a developer or contractor. Speculative construction following World War II was not uncommon in Union County.

**Eligibility for New Jersey**

**and National Registers:**

Yes  No

**National**

**Register Criteria:**

A  B  C  D

**Level of Significance**

Local  State  National

**Justification of Eligibility/Ineligibility:** The duplex at 615 Arnett Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1950, this multiple-family dwelling at 615 Arnett Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The multiple dwelling at 615 Arnett Street is unlikely to provide information important to our understanding of history (Criterion D).

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

**Narrative Boundary Description:** The duplex at 615 Arnett Street in Elizabeth, Union County. It is associated with block 4 lot 56H and 56I. It is located on the west side of Arnett Street.

Survey Name: Goethals Bridge Replacement Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

615 Arnett Street, Elizabeth, Union County



Northeast Corner

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 613 Arnett Street

**Street Address:** Street #: 613 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Arnett Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 56.F and 56.G

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

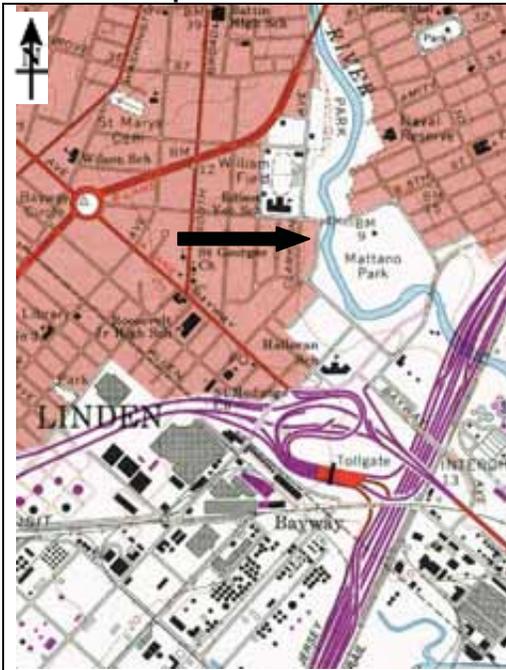
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

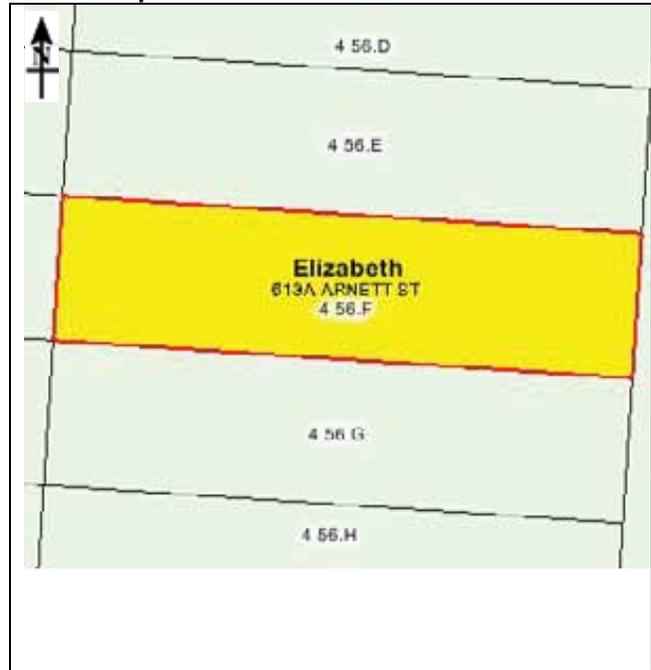
# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Maptech. Terrain Navigator, Version 7.03a, 1996-2006.

Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

### INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** 613 Arnett Street  
**Historic Name:** Unknown  
**Present Use:** Residential, Permanent  
**Historic Use:** Residential, Permanent  
**Construction Date:** c. 1950      **Source:** Stylistic evidence  
**Alteration Date(s):** N/A      **Source:** N/A  
**Designer:** Unknown      **Physical Condition:** Good  
**Builder:** Unknown      **Remaining Historic Fabric:** Medium  
**Style:** Colonial Revival  
**Form:** Duplex      **Stories:** 2  
**Type:** Other      **Bays:** 4  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials** Brick, American bond

**Exterior Description:** Two and one half stories and four bays wide, the twin dwelling at 613 Arnett Street is constructed of wood frame and features a rectangular-shaped footprint. The foundation was not visible at the time of the survey. This modest Colonial Revival-style house built circa 1950 is two bays deep and terminates in a side-gable asphalt-shingle roof. The brick-veneer façade has a six-course American bond and features a soldier stringcourse immediately below the second-story window brick rowlock sills. Two centered single-leaf paneled doors are sheltered with an asphalt-shingle quarter-hipped roof portico supported with decorative metal posts. Windows throughout the house are one-over-one sash units. The side elevations are stucco-covered with exterior-end stuccoed flues. The north end of the dwelling has a shed-roof dormer clad with vinyl siding on its rear slope.

**Interior Description:** N/A

### Setting:

Facing east, this dwelling is sited on a level grassy lot and has asphalt-paved driveways on both the north and south sides of the house.

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Survey Name: Goethals Bridge Replacement      Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. South of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. This dwelling is associated with the "Modern New Jersey" historic context.

**Significance:** The property at 613 Arnett Street in Elizabeth contains a typical two-story side-gable Colonial Revival-style dwelling dating to circa 1950. During the first half of the twentieth century, the Colonial Revival style became the most popular style choice across the United States among middle- and upper-class Americans for their houses, as well as other types such as ecclesiastical and institutional. Features such as a gable roof, brick construction, and symmetrical fenestration with a central entry are typical characteristics of Colonial Revival-style houses of this period. Many of the houses in this style are modest and lack any great amount of detail. The style's zenith occurred between the 1920s and 1950s and continues to be prevalent at the turn of the twenty-first century. The duplex at 613 Arnett Street lacks any significant embellishments and is not an uncommon form in the area. It is similar to each of the duplex houses on this block, suggesting that it was part of speculative construction by a developer or contractor. Speculative construction following World War II was not uncommon in Union County.

**Eligibility for New Jersey  
and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The duplex at 613 Arnett Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1950, this multiple-family dwelling at 613 Arnett Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The multiple dwelling at 613 Arnett Street is unlikely to provide information important to our understanding of history (Criterion D).

### For Historic Districts Only:

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

### For Individual Properties Only:

List the completed attachments related to the property's significance:

**Narrative Boundary Description:** The duplex at 613 Arnett Street in Elizabeth, Union County. It is associated with block 4 lot 56F and 56G. It is located on the west side of Arnett Street.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

613 Arnett Street, Elizabeth, Union County



Northeast Corner

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 609 Arnett Street

**Street Address:** Street #: 609 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Arnett Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 56.D and 56.E

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

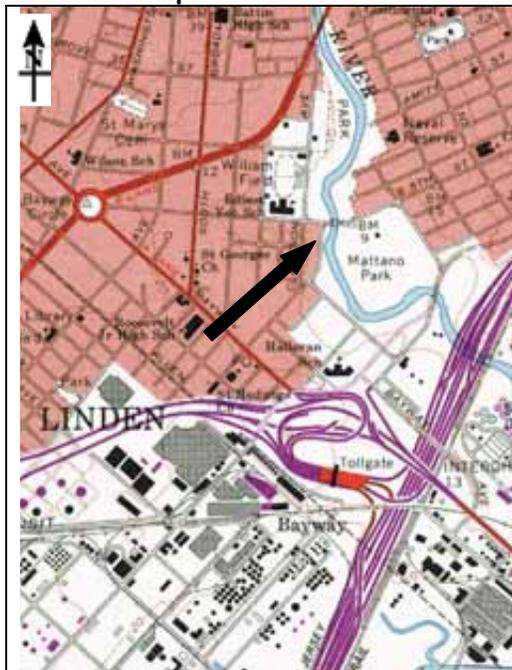
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Maptech. Terrain Navigator, Version 7.03a, 1996-2006.

Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

### INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** 609 Arnett Street  
**Historic Name:** Unknown  
**Present Use:** Residential, Permanent  
**Historic Use:** Residential, Permanent  
**Construction Date:** c. 1950 **Source:** Stylistic evidence  
**Alteration Date(s):** N/A **Source:** N/A  
**Designer:** Unknown **Physical Condition:** Good  
**Builder:** Unknown **Remaining Historic Fabric:** Medium  
**Style:** Colonial Revival  
**Form:** Duplex **Stories:** 2  
**Type:** Other **Bays:** 4  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials** Brick, American bond

**Exterior Description:** Two and one half stories and four bays wide, the twin dwelling at 609 Arnett Street is constructed of wood frame and features a rectangular-shaped footprint. The foundation was not visible at the time of the survey. This modest Colonial Revival-style house built circa 1950 is two bays deep and terminates in a side-gable asphalt-shingle roof. The brick-veneer façade has a six-course American bond and features a soldier stringcourse immediately below the first-story window brick rowlock sills. Two centered single-leaf paneled doors are sheltered with an asphalt-shingle shed-roof portico under temporary supports. The north end windows are one-over-one units with fake six-light dividers in the upper sash, and the south end windows are one-over-one units. The side elevations are stucco-covered and feature stuccoed exterior-end flues. The south end of the dwelling has a shed-roof dormer clad with vinyl siding on its rear slope.

**Interior Description:** N/A

### Setting:

Facing east, this dwelling is sited on a level grassy lot and has asphalt-paved driveways on both the north and south sides of the house. The house is fronted with several foundation plantings.

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. This dwelling is associated with the "Modern New Jersey" historic context.

**Significance:** The property at 609 Arnett Street in Elizabeth contains a typical two-story side-gable Colonial Revival-style dwelling dating to circa 1950. During the first half of the twentieth century, the Colonial Revival style became the most popular style choice across the United States among middle- and upper-class Americans for their houses, as well as other types such as ecclesiastical and institutional. Features such as a gable roof, brick construction, and symmetrical fenestration with a central entry are typical characteristics of Colonial Revival-style houses of this period. Many of the houses in this style are modest and lack any great amount of detail. The style's zenith occurred between the 1920s and 1950s and continues to be prevalent at the turn of the twenty-first century. The duplex at 609 Arnett Street lacks any significant embellishments and is not an uncommon form in the area. It is similar to each of the duplex houses on this block, suggesting that it was part of speculative construction by a developer or contractor. Speculative construction following World War II was not uncommon in Union County.

**Eligibility for New Jersey**

**and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The duplex at 609 Arnett Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1950, this multiple-family dwelling at 609 Arnett Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The multiple dwelling at 609 Arnett Street is unlikely to provide information important to our understanding of history (Criterion D).

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

List the completed attachments related to the property's significance:

**Narrative Boundary Description:** The duplex at 609 Arnett Street in Elizabeth, Union County. It is associated with block 4 lot 56D and 56E. It is located on the west side of Arnett Street.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

609 Arnett Street, Elizabeth, Union County



Northeast Corner

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Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Property Name: 607 Arnett Street

Street Address: Street #: 607 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Arnett Suffix: \_\_\_\_\_ Type: ST

County(s): Union Zip Code: 07202

Municipality(s): Elizabeth City Block(s): 4

Local Place Name(s): Elizabeth Lot(s): 56.B and 56.C

Ownership: Private USGS Quad(s) Elizabeth

## Description:

See building attachment

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

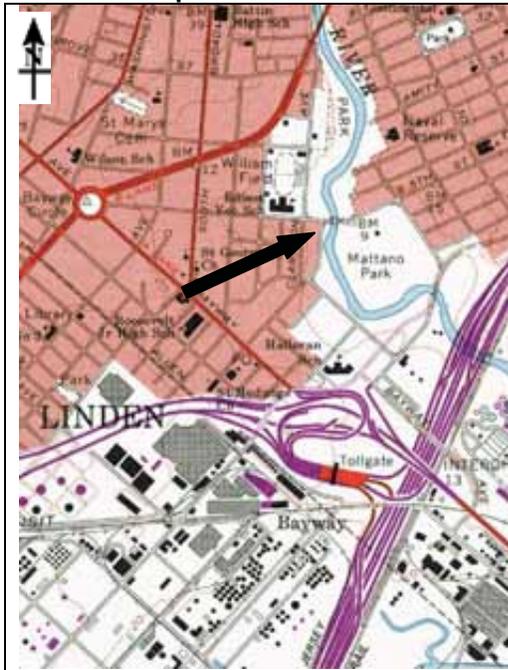
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

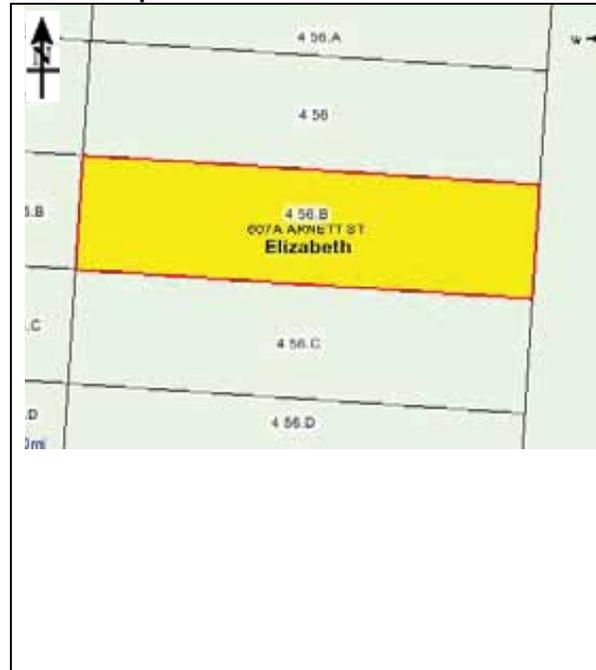
# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Maptech. Terrain Navigator, Version 7.03a, 1996-2006.

Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

**INTENSIVE LEVEL USE ONLY**

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** 607 Arnett Street

**Historic Name:** Unknown

**Present Use:** Residential, Permanent

**Historic Use:** Residential, Permanent

**Construction Date:** c. 1950      **Source:** Stylistic evidence

**Alteration Date(s):** N/A      **Source:** N/A

**Designer:** Unknown      **Physical Condition:** Good

**Builder:** Unknown      **Remaining Historic Fabric:** Medium

**Style:** Colonial Revival

**Form:** Duplex      **Stories:** 2

**Type:** Other      **Bays:** 4

**Roof Finish Materials:** Asphalt shingle

**Exterior Finish Materials** Brick, American bond

**Exterior Description:** Two stories and four bays wide, the twin dwelling at 607 Arnett Street is constructed of wood frame and features a rectangular-shaped footprint. The foundation was not visible at the time of the survey. This modest Colonial Revival-style house built circa 1950 is two bays deep and terminates in a side-gable asphalt-shingle roof. The brick-veneer façade has a six-course American bond. Two centered single-leaf paneled doors are sheltered with an asphalt-shingle gable-roof portico supported with decorative metal posts. The south end windows openings contain six-over-one wood sash on the second story and one-over-one vinyl sash on the first story. North end windows are one-over-one units. The south gable end features stucco veneer and an exterior-end stuccoed chimney. The northern gable end is sheathed with stretcher-bond brick. Each of the two dwellings has a shed-roof porch addition on the rear elevation.

**Interior Description:** N/A

**Setting:** Facing east, this dwelling is sited on a level grassy lot and has a concrete driveway on the north side of the house and an asphalt-paved driveway on the south side.

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Survey Name: Goethals Bridge Replacement      Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. This dwelling is associated with the "Modern New Jersey" historic context.

**Significance:** The property at 607 Arnett Street in Elizabeth contains a typical two-story side-gable Colonial Revival-style dwelling dating to circa 1950. During the first half of the twentieth century, the Colonial Revival style became the most popular style choice across the United States among middle- and upper-class Americans for their houses, as well as other types such as ecclesiastical and institutional. Features such as a gable roof, brick construction, and symmetrical fenestration with a central entry are typical characteristics of Colonial Revival-style houses of this period. Many of the houses in this style are modest and lack any great amount of detail. The style's zenith occurred between the 1920s and 1950s and continues to be prevalent at the turn of the twenty-first century. The duplex at 607 Arnett Street lacks any significant embellishments and is not an uncommon form in the area. It is similar to each of the duplex houses on this block, suggesting that it was part of speculative construction by a developer or contractor. Speculative construction following World War II was not uncommon in Union County.

**Eligibility for New Jersey**

**and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The duplex at 607 Arnett Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1950, this multiple-family dwelling at 607 Arnett Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The multiple dwelling at 607 Arnett Street is unlikely to provide information important to our understanding of history (Criterion D).

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

**Narrative Boundary Description:** The duplex at 607 Arnett Street in Elizabeth, Union County. It is associated with block 4 lot 56B and 56C. It is located on the west side of Arnett Street.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

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607 Arnett Street, Elizabeth, Union County



Northeast Corner

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 605 Arnett Street

**Street Address:** Street #: 605 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Arnett Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 56 and 56.A

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

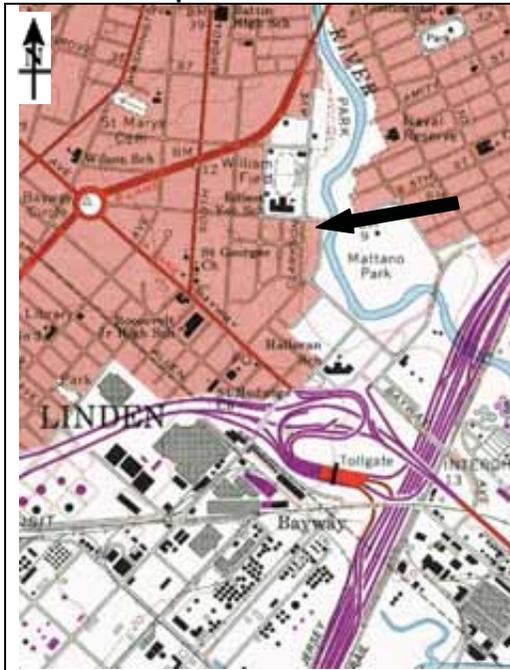
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

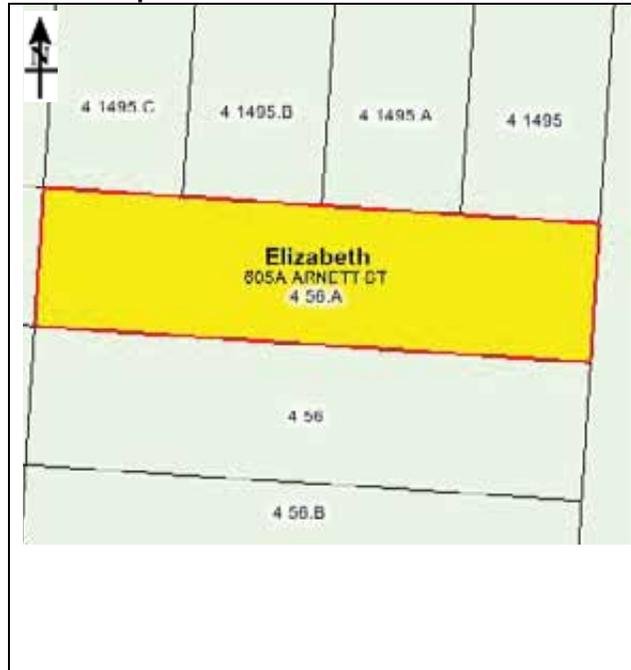
# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Maptech. Terrain Navigator, Version 7.03a, 1996-2006.

Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

### INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** 605 Arnett Street  
**Historic Name:** Unknown  
**Present Use:** Residential, Permanent  
**Historic Use:** Residential, Permanent  
**Construction Date:** c. 1950      **Source:** Stylistic evidence  
**Alteration Date(s):** N/A      **Source:** N/A  
**Designer:** Unknown      **Physical Condition:** Good  
**Builder:** Unknown      **Remaining Historic Fabric:** Medium  
**Style:** Colonial Revival  
**Form:** Duplex      **Stories:** 2  
**Type:** Other      **Bays:** 4  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials** Brick, American bond

**Exterior Description:** Two stories and four bays wide, the twin dwelling at 605 Arnett Street is constructed of wood frame and features a rectangular-shaped footprint. The foundation was not visible at the time of the survey. This modest Colonial Revival-style house built circa 1950 is two bays deep and terminates in a side-gable asphalt-shingle roof. The brick-veneer façade has a six-course American bond and features a soldier stringcourse immediately below the second-story window brick rowlock sills. Two centered single-leaf doors are sheltered with an asphalt-shingle shed-roof portico supported with decorative metal posts. Windows throughout the house are one-over-one wood units. Metal awnings shade all windows on the southern portion of the twin dwelling. The side elevations are covered with stretcher-bond brick and display exterior-end brick flues. Each dwelling has a shed-roof porch addition on the rear elevation.

**Interior Description:** N/A

**Setting:** Facing east, this dwelling is sited on a level grassy lot and has a concrete driveway to the south side of the house. The house is fronted with foundation plantings and the property is dotted with several shrubs and trees.

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Survey Name: Goethals Bridge Replacement      Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. This dwelling is associated with the "Modern New Jersey" historic context.

**Significance:** The property at 605 Arnett Street in Elizabeth contains a typical two-story side-gable Colonial Revival-style dwelling dating to circa 1950. During the first half of the twentieth century, the Colonial Revival style became the most popular style choice across the United States among middle- and upper-class Americans for their houses, as well as other types such as ecclesiastical and institutional. Features such as a gable roof, brick construction, and symmetrical fenestration with a central entry are typical characteristics of Colonial Revival-style houses of this period. Many of the houses in this style are modest and lack any great amount of detail. The style's zenith occurred between the 1920s and 1950s and continues to be prevalent at the turn of the twenty-first century. This duplex at 605 Arnett Street lacks any significant embellishments and is not an uncommon form in the area. It is similar to each of the duplex houses on this block, suggesting that it was part of speculative construction by a developer or contractor. Speculative construction following World War II was not uncommon in Union County.

**Eligibility for New Jersey  
and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The duplex at 605 Arnett Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1950, this multiple-family dwelling at 605 Arnett Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The multiple dwelling at 605 Arnett Street is unlikely to provide information important to our understanding of history (Criterion D).

### For Historic Districts Only:

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

### For Individual Properties Only:

List the completed attachments related to the property's significance:

**Narrative Boundary Description:** The duplex at 605 Arnett Street in Elizabeth, Union County. It is associated with block 4 lot 56 and 56A. It is located on the west side of Arnett Street.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

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605 Arnett Street, Elizabeth, Union County



Northeast Corner

---

Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 584-586 Summer Street

**Street Address:** Street #: 584 586 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Summer Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 1495 and 1495.A

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

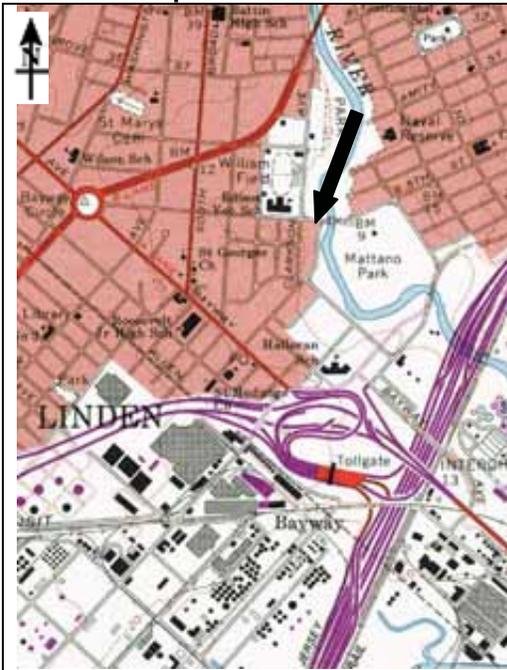
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

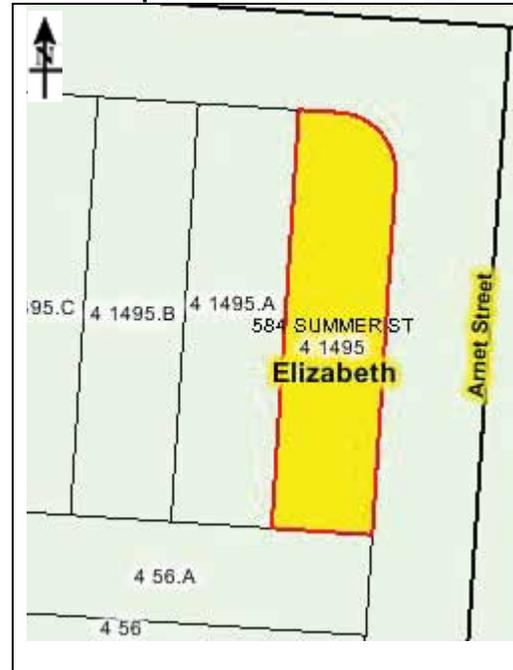
# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Maptech. Terrain Navigator, Version 7.03a, 1996-2006.

Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

## INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** 584-586 Summer Street

**Historic Name:** Unknown

**Present Use:** Residential, Permanent

**Historic Use:** Residential, Permanent

**Construction Date:** c. 1950      **Source:** Stylistic evidence

**Alteration Date(s):** N/A      **Source:** N/A

**Designer:** Unknown      **Physical Condition:** Good

**Builder:** Unknown      **Remaining Historic Fabric:** Medium

**Style:** Colonial Revival

**Form:** Duplex      **Stories:** 2

**Type:** Other      **Bays:** 4

**Roof Finish Materials:** Asphalt shingle

**Exterior Finish Materials** Brick, American bond

**Exterior Description:** Two stories and four bays wide, the twin dwelling at 584-586 Summer Street is constructed of wood frame and features a rectangular-shaped footprint. The foundation was not visible at the time of the survey. This modest Colonial Revival-style house built circa 1950 is two bays deep and terminates in a side-gable asphalt-shingle roof. The brick-veneer façade has a six-course American bond and features a soldier stringcourse immediately below the second-story window brick rowlock sills. Two centered single-leaf wood paneled doors are sheltered with an asphalt-shingle front-gable roof portico supported with square wood posts. Windows throughout the house are a mix of one-over-one vinyl units and six-over-one wood sash. The side and rear elevations are covered with stucco. Both sides have one exterior-end stuccoed flue.

**Interior Description:** N/A

**Setting:** Facing north, this dwelling is sited on a level grassy lot and has a paved driveway to the west side of the house. It is fronted with foundation plantings and there are a few mature trees on the property.

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Survey Name: Goethals Bridge Replacement      Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. South of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. This dwelling is associated with the "Modern New Jersey" historic context.

**Significance:** The property at 584 Summer Street in Elizabeth contains a typical two-story side-gable Colonial Revival-style dwelling dating to circa 1950. During the first half of the twentieth century, the Colonial Revival style became the most popular style choice across the United States among middle- and upper-class Americans for their houses, as well as other types such as ecclesiastical and institutional. Features such as a gable roof, brick construction, and symmetrical fenestration with a central entry are typical characteristics of Colonial Revival-style houses of this period. Many of the houses in this style are modest and lack any great amount of detail. The style's zenith occurred between the 1920s and 1950s and continues to be prevalent at the turn of the twenty-first century. This duplex at 584 Summer Street lacks any significant embellishments and is not an uncommon form in the area. It is similar to each of the duplex houses on this block, suggesting that it was part of speculative construction by a developer or contractor. Speculative construction following World War II was not uncommon in Union County.

**Eligibility for New Jersey  
and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The duplex at 584 Summer Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1950, this multiple-family dwelling at 584 Summer Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The multiple dwelling at 584 Summer Street is unlikely to provide information important to our understanding of history (Criterion D).

### For Historic Districts Only:

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

### For Individual Properties Only:

List the completed attachments related to the property's significance:

**Narrative Boundary Description:** The duplex at 584 Summer Street in Elizabeth, Union County. It is associated with block 4 lot 1495 and 1495A. It is located on the south side of Summer Street.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

584-586 Summer Street, Elizabeth, Union County



Southeast Corner

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 442 Richmond Street

**Street Address:** Street #: 442 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Richmond Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 455

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

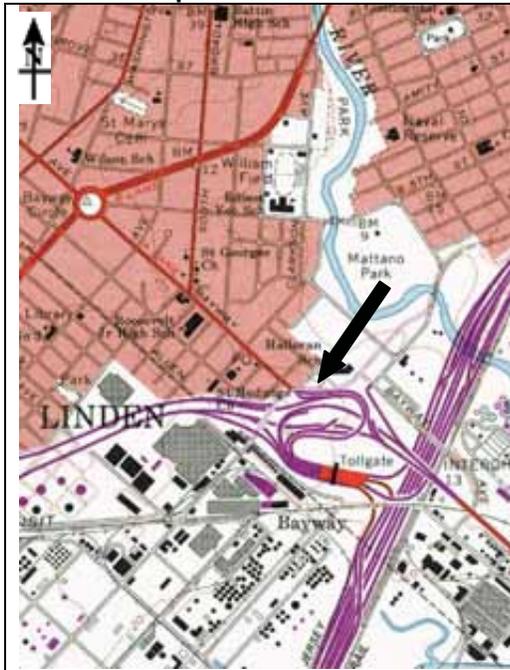
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

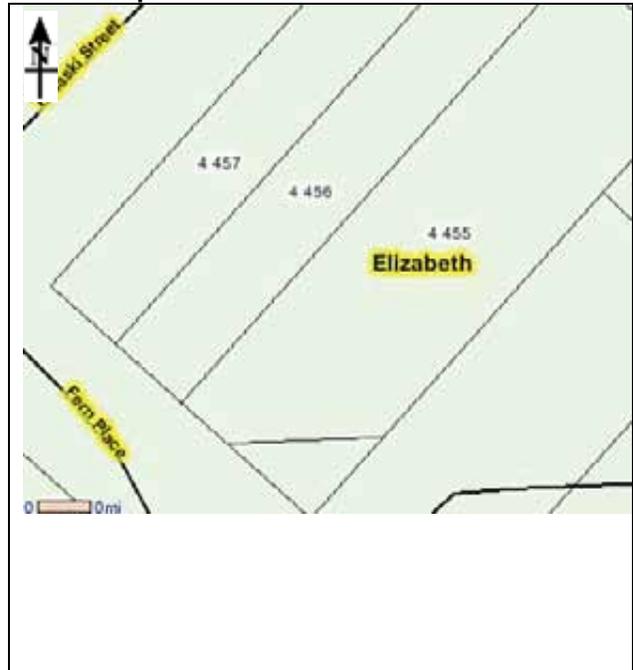
# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Maptech. Terrain Navigator, Version 7.03a, 1996-2006.

Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

### INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** 442 Richmond Street  
**Historic Name:** Unknown  
**Present Use:** Residential, Permanent  
**Historic Use:** Residential, Permanent  
**Construction Date:** c. 1940 **Source:** Stylistic evidence  
**Alteration Date(s):** N/A **Source:** N/A  
**Designer:** Unknown **Physical Condition:** Good  
**Builder:** Unknown **Remaining Historic Fabric:** Medium  
**Style:** Other  
**Form:** Gable Front **Stories:** 1.5  
**Type:** Other **Bays:** 2  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials:** Vinyl siding

**Exterior Description:** Constructed of wood frame, the one-and-a-half-story two-bay house at 442 Richmond Street is clad with vinyl siding and terminates in a front-gable roof. The foundation is covered with stucco, and across the full-width of the façade is a hipped-roof enclosed porch. The porch is pierced with an off-center single-leaf entry and has a three-sided oriel with one-over-one vinyl-sash windows flanking a fixed wood window. Other windows throughout the house are one-over-one vinyl sash except for one hopper window on the northwest elevation. Both side elevations are adorned with shed-roof vinyl-clad dormers nearly the full depth of the house. The dormers contain three window bays. An interior brick flue punctuates the west slope of the asphalt-shingle roof.

**Interior Description:** N/A

### Setting:

Facing northeast, this dwelling is located on Richmond Street on a level lot surrounded by several mature trees and shrubs. A chain link fence surrounds the front yard.

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. This property is associated with the New Jersey "Suburban Development" historic context.

**Significance:** The property at 442 Richmond Street in Elizabeth contains a typical vernacular front-gable house dating to circa 1940 in Union County, New Jersey. This modest dwelling lack embellishment and is not an uncommon form within the area.

**Eligibility for New Jersey**

**and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The house at 442 Richmond Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1940, the vernacular house at 442 Richmond Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 442 Richmond Street is unlikely to provide information important to our understanding of history (Criterion D). Its integrity has been diminished with replacement synthetic siding and replacement windows.

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

**Narrative Boundary Description:** The property is located at 442 Richmond Street in Elizabeth, Union County. It is associated with block 4 lot 455. It is bounded to the northeast by Richmond Street, and to the southwest by Fern Place.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

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442 Richmond Street, Elizabeth, Union County



Southwest Elevation

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** William F. Hollaran School, #22

**Street Address:** Street #: 421 447 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Richmond Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 1278

**Ownership::** Public **USGS Quad(s)** Elizabeth

## Description:

See building attachment and continuation sheet

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

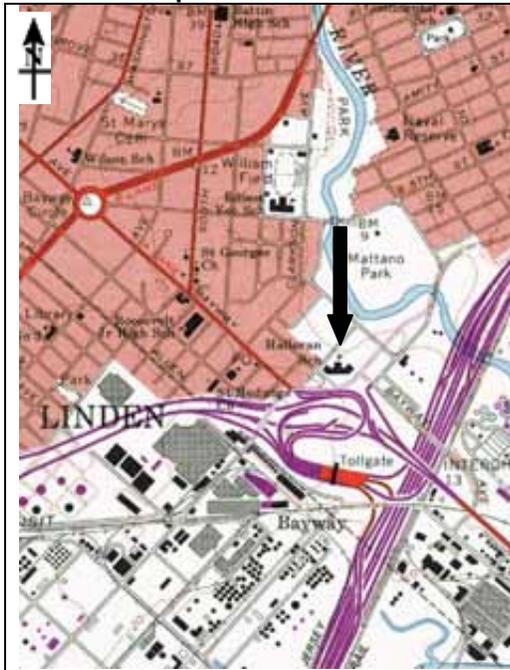
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

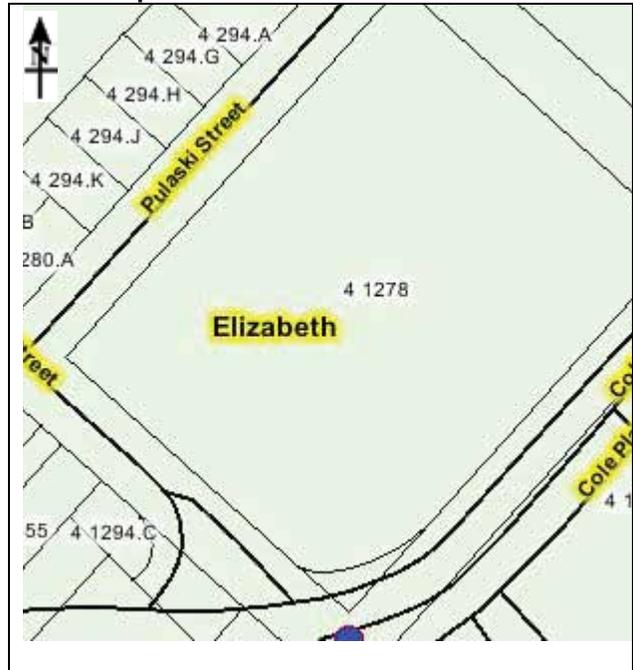
# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Maptech. Terrain Navigator, Version 7.03a, 1996-2006.

Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

### INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** William F. Halloran School, #22  
**Historic Name:** William F. Halloran School, #22  
**Present Use:** Institutional, Educational  
**Historic Use:** Institutional, Educational  
**Construction Date:** 1950      **Source:** Stylistic and material evidence  
**Alteration Date(s):** N/A      **Source:** N/A  
**Designer:** Unknown      **Physical Condition:** Good  
**Builder:** Unknown      **Remaining Historic Fabric:** High  
**Style:** Colonial Revival  
**Form:** Other      **Stories:** 2  
**Type:** Other      **Bays:** 11  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials** Brick, Common bond

**Exterior Description:** Built in 1950, the William F. Halloran School #22 has a three-part plan with a two-story central block and one-story wings. Constructed with a five-course Flemish variation of Common bond, this Colonial Revival-style school is set on a stuccoed foundation and has a side-gable asphalt-shingle roof. The central block is 11 bays across with a central double-leaf entry and double-leaf entries on the end bays. Each window opening bay holds triple six-over-six aluminum sash topped by a fixed metal panel. Each of the windows has cast stone sills and soldier lintels. The center entry is set within a gable roofed pavilion with partial returns and corbelled quoins. The center door surround is cast stone and is embellished with a heavy cast stone entablature. The architrave features large block modillions and above the architrave is a shield motif. See *Continuation Sheet*.

**Interior Description:** N/A

### Setting:

Halloran School is sited on the east corner of Richmond Street, Pulaski Street, and Atlantic Street. It is on a large level grassy lot with a baseball field at the northern end of the lot. A parking lot is located immediately north of the building, and a flagpole and concrete sidewalks are to the south.

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Survey Name: Goethals Bridge Replacement      Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. South of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. The Halloran School is associated with the "Modern New Jersey" historic context.

**Significance:** Dating to circa 1955, the Halloran School at 421 Richmond Street is a common example of the Colonial Revival style associated with an institutional building. During the first half of the twentieth century, the Colonial Revival style became the most popular stylistic choice across the United States. Features such as a gable or hipped roofs, multiple stories, brick construction, and embellished door surrounds are typical characteristics of Colonial Revival-style institutional buildings of this period as seen with Halloran School. Many of the institutional buildings in this style are modest and lack any great amount of detail. The Colonial Revival style became a popular choice among middle- and upper-class Americans for their houses, as well as other types such as ecclesiastical and institutional, during the first half of the twentieth century. The style's zenith occurred between the 1920s and 1950s and continues to be prevalent at the turn of the twenty-first century.

**Eligibility for New Jersey**

**and National Registers:**

Yes  No

**National**

**Register Criteria:**

A  B  C  D

**Level of Significance**

Local  State  National

**Justification of Eligibility/Ineligibility:** The Halloran School at 421 Richmond Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1955, the school at 421 Richmond Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 421 Richmond Street is unlikely to provide information important to our understanding of history (Criterion D).

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

List the completed attachments related to the property's significance:

**Narrative Boundary Description:** The property is located at 421 Richmond Street in Elizabeth, Union County. It is associated with block 4 lot 1278. It is bounded to the southeast by Cole Place, to the northwest by Pulaski Street, and to the southwest by Richmond Street.

Survey Name: Goethals Bridge Replacement Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

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### Exterior Description

The end bays of this center section feature cast stone surrounds with a cast stone architrave. The second story of the end bays are illuminated with multiple glass blocks set in a molded wood surround with a paneled spandrel. A one-story, five-bay-wide, gable roofed wing extending west from the main block possesses a one-story, three-bay-wide, hipped roof wing stretches northwest from its western end. A similar configuration of wings occurs east of the main block; a three-bay-wide gable section terminates with a five-bay-wide wing extending to the northeast. Nearly all window openings feature paired or triple six-over-six windows topped by metal panels. A small one-story wing extends southward from the west end of the west wing. The small wing features banks of six-over-six windows topped by metal panels and a metal slab door with slender vertical light in its western elevation. The northwest elevation of the west wing possesses a central entrance. Another entrance penetrates the northeastern wing's eastern elevation. Other architectural details of the Halloran School include corbelled brick quoins, a heavy molded wood cornice with returns, a date stone at the entry, and several interior brick chimney flues.

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

William F. Hollaran School, #22, Elizabeth, Union County



South Elevation, Looking Northeast



South Elevation, East Wing

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

William F. Hollaran School, #22, Elizabeth, Union County



South Elevation of West Wing

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 445 Fern Place

**Street Address:** Street #: 445 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Fern Suffix: \_\_\_\_\_ Type: PL

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 456

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

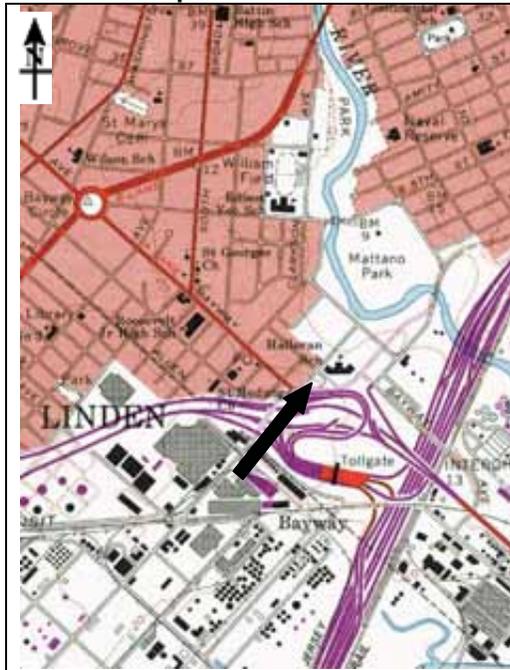
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Maptech. Terrain Navigator, Version 7.03a, 1996-2006.

Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

### INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** 445 Fern Place

**Historic Name:** Unknown

**Present Use:** Residential, Permanent

**Historic Use:** Residential, Permanent

**Construction Date:** c. 1930      **Source:** Stylistic evidence

**Alteration Date(s):** N/A      **Source:** N/A

**Designer:** Unknown      **Physical Condition:** Good

**Builder:** Unknown      **Remaining Historic Fabric:** Medium

**Style:** Other

**Form:** Gable front      **Stories:** 2

**Type:** Other      **Bays:** 3

**Roof Finish Materials:** Asphalt shingle

**Exterior Finish Materials:** Aluminum siding

**Exterior Description:** Surrounded by mature trees and thick bushes that obscure photographic documentation, the circa 1930 house at 445 Fern Place stands two stories tall with a pedimented front-gable roof sheathed with asphalt shingles. The three-bay-wide building has a rectangular-shaped footprint and a full-width hipped-roof porch across its main façade. The porch is supported by decorative metal posts set atop a flagstone deck and brick foundation. The porch shades a metal panel door symmetrically set on west side of the elevation. The building features aluminum siding, a rock-faced concrete-block foundation, and one-over-one aluminum sash with aluminum surrounds.

**Interior Description:** N/A

### Setting:

Facing southwest, this dwelling is located on a level grassy lot surrounded by several mature trees. A modern one-story gable-roofed garage stands on the rear portion of the lot, which is shielded by thick vegetation.

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Survey Name: Goethals Bridge Replacement      Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. This dwelling is associated with the "Suburban Development" historic context.

**Significance:** The property at 445 Fern Place is a typical vernacular dwelling dating to circa 1930 in Union County, New Jersey. This modest house lacks embellishment and is not an uncommon form within the area.

**Eligibility for New Jersey**

**and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The dwelling at 445 Fern Place in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1930, the house at 445 Fern Place does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 445 Fern Place is unlikely to provide information important to our understanding of history (Criterion D). This dwelling's integrity has diminished with replacement aluminum siding.

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

**Narrative Boundary Description:** The property is located at 445 Fern Place in Elizabeth, Union County. It is associated with block 4 lot 456. It is bounded to the northeast by Richmond Street, and to the southwest by Fern Place.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

445 Fern Place, Elizabeth, Union County



Southwest Elevation



Southeast Elevation

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 447 Fern Place

**Street Address:** Street #: 447 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Fern Suffix: \_\_\_\_\_ Type: PL

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 457

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

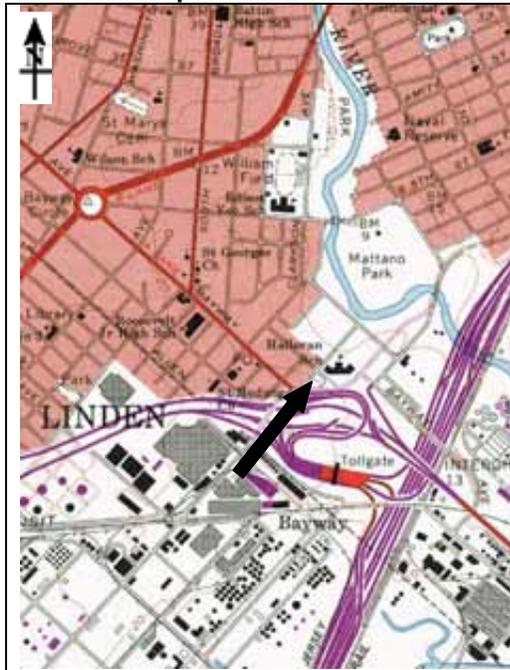
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

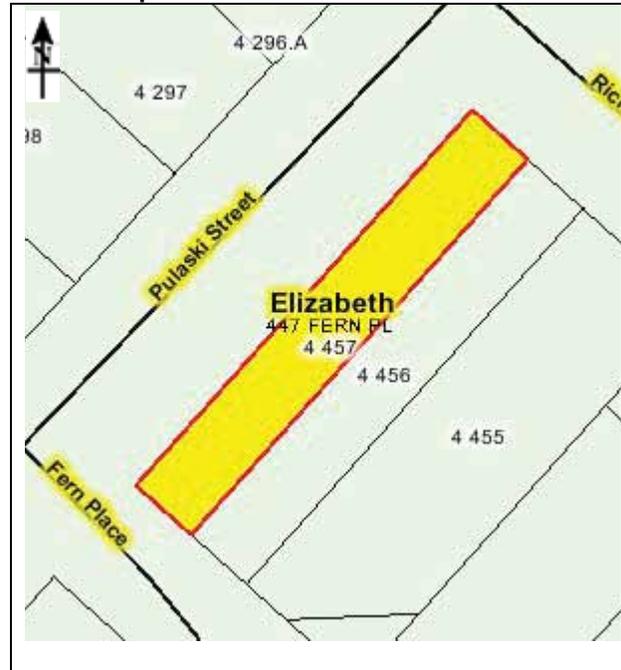
# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Maptech. Terrain Navigator, Version 7.03a, 1996-2006.

Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

### INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** 447 Fern Place

**Historic Name:** Unknown

**Present Use:** Residential, Permanent

**Historic Use:** Residential, Permanent

**Construction Date:** c. 1910      **Source:** Stylistic evidence

**Alteration Date(s):** N/A      **Source:** N/A

**Designer:** Unknown      **Physical Condition:** Good

**Builder:** Unknown      **Remaining Historic Fabric:** Medium

**Style:** Colonial Revival

**Form:** Gable front      **Stories:** 2.5

**Type:** Other      **Bays:** 3

**Roof Finish Materials:** Asphalt shingle

**Exterior Finish Materials:** Vinyl siding

**Exterior Description:** Rectangular in shape, this two-and-a-half-story wood-frame dwelling built circa 1910 is set on a rock-faced concrete-block foundation. The three-bay-wide house features a front-gable roof with asphalt shingles, a box cornice, and a gable-roofed dormer on its northwestern slope set above a three-sided two-story bay. Vinyl siding covers the building's exterior walls. One-over-one vinyl windows occupy most fenestral openings. A shed-roof porch supported with square wood posts and balusters covers the full width of the main façade. The porch protects a vinyl panel door with upper fan light. One corbelled brick chimney penetrates the center of the ridge.

**Interior Description:** N/A

**Setting:** Facing southwest, this dwelling is located at the east corner of Fern Place and Pulaski Street. It is on a level grassy lot surrounded by several mature trees. A wood fence lines the perimeter of the rear yard.

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Survey Name: Goethals Bridge Replacement      Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. This dwelling is associated with the "Suburban Development" historic context.

**Significance:** Dating to circa 1910, the house at 447 Fern Place is a common suburban example of the Colonial Revival style. During the first half of the twentieth century, the Colonial Revival style became the most popular stylistic choice across the United States. Features such as a double-pile plan, gable roof, two to two and a half stories, dormers, and full-width front porches are typical characteristics of Colonial Revival-style houses of this period as seen with 447 Fern Place. Many of the houses in this style are modest and lack any great amount of detail. The Colonial Revival style became a popular choice among middle- and upper-class Americans for their houses, as well as other types such as ecclesiastical and institutional, during the first half of the twentieth century. The style's zenith occurred between the 1920s and 1950s and continues to be prevalent at the turn of the twenty-first century.

**Eligibility for New Jersey  
and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The dwelling at 447 Fern Place in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1910, the house at 447 Fern Place does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 447 Fern Place is unlikely to provide information important to our understanding of history (Criterion D). The dwelling's integrity has diminished with replacement siding and replacement windows.

### For Historic Districts Only:

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

### For Individual Properties Only:

List the completed attachments related to the property's significance:

**Narrative Boundary Description:** The property is located at 447 Fern Place in Elizabeth, Union County. It is associated with block 4 lot 457. It is bounded to the northeast by Richmond Street, to the northwest by Pulaski Street, and to the southwest by Fern Place.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

447 Fern Place, Elizabeth, Union County



North Corner

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Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 661-663 Brunswick Avenue; 663 Pulaski Street

**Street Address:** Street #: 661 663 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Brunswick Suffix: \_\_\_\_\_ Type: AVE

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 300

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

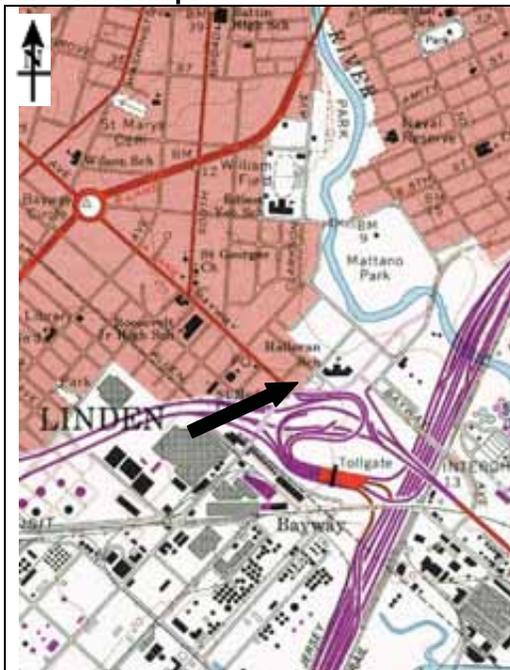
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Maptech. Terrain Navigator, Version 7.03a, 1996-2006.

Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

### INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** 661-663 Brunswick Avenue (663 Pulaski Street)  
**Historic Name:** Unknown  
**Present Use:** Residential, Permanent  
**Historic Use:** Residential, Permanent  
**Construction Date:** c. 1910      **Source:** Stylistic and material evidence  
**Alteration Date(s):** N/A      **Source:** N/A  
**Designer:** Unknown      **Physical Condition:** Good  
**Builder:** Unknown      **Remaining Historic Fabric:** Medium  
**Style:** Colonial Revival  
**Form:** Gable front      **Stories:** 2.5  
**Type:** Other      **Bays:** 3  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials:** Aluminum siding

**Exterior Description:** Built in circa 1910, this two-story dwelling sits on a stuccoed foundation and features a rectangular-shaped footprint. The house is clad with aluminum siding, and across the full width of the façade is a hipped-roof porch supported by wood piers and a solid aluminum-clad balustrade. The first story is three bays wide with two bays across its second story. The porch shades a wood slab door with two upper lights on its south end and paired one-over-one aluminum windows on its second story. The pedimented gable peak is pierced with a six-over-one aluminum window. Other architectural features of the house include overhanging eaves and a boxed cornice.

**Interior Description:** N/A

### Setting:

Located on the northwest side of Pulaski Street, this dwelling is sited on a level lot. A concrete driveway stands to the southwest side of the house and a chain link fence lines the front of the property. Several foundation plantings front the house, which is set close to the street.

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Survey Name: Goethals Bridge Replacement      Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. This dwelling is associated with the "Suburban Development" historic context.

**Significance:** Dating to circa 1910, the house at 661 Brunswick Avenue is a common suburban example of the Colonial Revival style. During the first half of the twentieth century, the Colonial Revival style became the most popular stylistic choice across the United States. Features such as a double-pile plan, gable roof, two to two and a half stories, dormers, and full-width front porches are typical characteristics of Colonial Revival-style houses of this period as seen with 661 Brunswick Avenue. Many of the houses in this style are modest and lack any great amount of detail. The Colonial Revival style became a popular choice among middle- and upper-class Americans for their houses, as well as other types such as ecclesiastical and institutional, during the first half of the twentieth century. The style's zenith occurred between the 1920s and 1950s and continues to be prevalent at the turn of the twenty-first century.

**Eligibility for New Jersey  
and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The dwelling at 661 Brunswick Avenue in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1910, the house at 661 Brunswick Avenue does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 661 Brunswick Avenue is unlikely to provide information important to our understanding of history (Criterion D).

### For Historic Districts Only:

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

### For Individual Properties Only:

**List the completed attachments related to the property's significance:**

**Narrative Boundary Description:** The property is located at 661 Brunswick Avenue in Elizabeth, Union County. It is associated with block 4 lot 300. It is bounded to the southeast by Pulaski Street and directly across is Fern Street.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

661-663 Brunswick Avenue; 663 Pulaski Street, Elizabeth, Union County



Southeast Elevation

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 661-663 Brunswick Avenue; 663 Pulaski Street

**Street Address:** Street #: 661      663      Apartment #: \_\_\_\_\_  
(Low)      (High)      (Low)      (High)

Prefix: \_\_\_\_\_ Street Name: Brunswick      Suffix: \_\_\_\_\_ Type: AVE

**County(s):** Union      **Zip Code:** 07202

**Municipality(s):** Elizabeth City      **Block(s):** 4

**Local Place Name(s):** Elizabeth      **Lot(s):** 300

**Ownership::** Private      **USGS Quad(s)** Elizabeth

## Description:

See building attachment

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

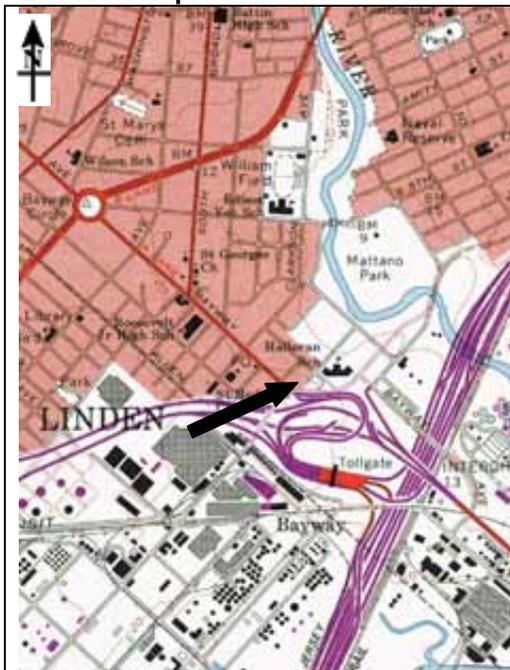
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Maptech. Terrain Navigator, Version 7.03a, 1996-2006.

Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

### INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** 661-663 Brunswick Avenue (663 Pulaski Street)  
**Historic Name:** Unknown  
**Present Use:** Residential, Permanent  
**Historic Use:** Residential, Permanent  
**Construction Date:** c. 1910 **Source:** Stylistic and material evidence  
**Alteration Date(s):** N/A **Source:** N/A  
**Designer:** Unknown **Physical Condition:** Good  
**Builder:** Unknown **Remaining Historic Fabric:** Medium  
**Style:** Colonial Revival  
**Form:** Gable front **Stories:** 2.5  
**Type:** Other **Bays:** 3  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials:** Aluminum siding

**Exterior Description:** Built in circa 1910, this two-story dwelling sits on a stuccoed foundation and features a rectangular-shaped footprint. The house is clad with aluminum siding, and across the full width of the façade is a hipped-roof porch supported by wood piers and a solid aluminum-clad balustrade. The first story is three bays wide with two bays across its second story. The porch shades a wood slab door with two upper lights on its south end and paired one-over-one aluminum windows on its second story. The pedimented gable peak is pierced with a six-over-one aluminum window. Other architectural features of the house include overhanging eaves and a boxed cornice.

**Interior Description:** N/A

### Setting:

Located on the northwest side of Pulaski Street, this dwelling is sited on a level lot. A concrete driveway stands to the southwest side of the house and a chain link fence lines the front of the property. Several foundation plantings front the house, which is set close to the street.

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. This dwelling is associated with the "Suburban Development" historic context.

**Significance:** Dating to circa 1910, the house at 661 Brunswick Avenue is a common suburban example of the Colonial Revival style. During the first half of the twentieth century, the Colonial Revival style became the most popular stylistic choice across the United States. Features such as a double-pile plan, gable roof, two to two and a half stories, dormers, and full-width front porches are typical characteristics of Colonial Revival-style houses of this period as seen with 661 Brunswick Avenue. Many of the houses in this style are modest and lack any great amount of detail. The Colonial Revival style became a popular choice among middle- and upper-class Americans for their houses, as well as other types such as ecclesiastical and institutional, during the first half of the twentieth century. The style's zenith occurred between the 1920s and 1950s and continues to be prevalent at the turn of the twenty-first century.

**Eligibility for New Jersey  
and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The dwelling at 661 Brunswick Avenue in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1910, the house at 661 Brunswick Avenue does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 661 Brunswick Avenue is unlikely to provide information important to our understanding of history (Criterion D).

### For Historic Districts Only:

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

### For Individual Properties Only:

List the completed attachments related to the property's significance:

**Narrative Boundary Description:** The property is located at 661 Brunswick Avenue in Elizabeth, Union County. It is associated with block 4 lot 300. It is bounded to the southeast by Pulaski Street and directly across is Fern Street.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

661-663 Brunswick Avenue; 663 Pulaski Street, Elizabeth, Union County



Southeast Elevation

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 655 Pulaski Street

**Street Address:** Street #: 655 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Pulaski Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 298

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

**Registration and Status Dates:** National Historic Landmark: \_\_\_\_\_ SHPO Opinion: \_\_\_\_\_  
National Register: \_\_\_\_\_ Local Designation: \_\_\_\_\_  
New Jersey Register: \_\_\_\_\_ Other Designation: \_\_\_\_\_  
Determination of Eligibility: \_\_\_\_\_ Other Designation Date: \_\_\_\_\_

## Photograph:

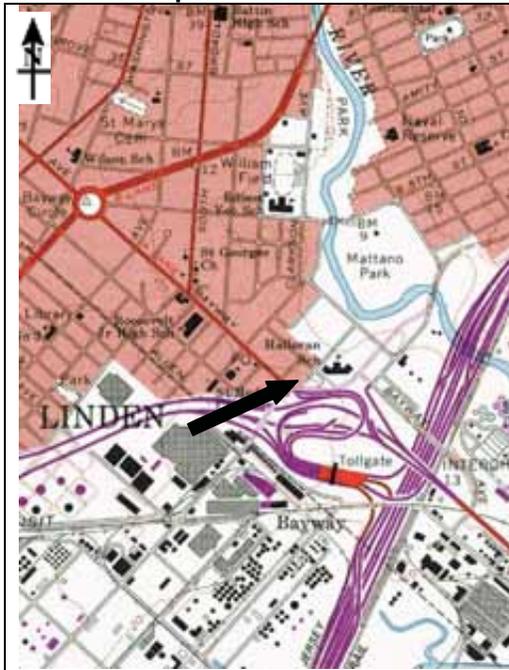


Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Maptech. Terrain Navigator, Version 7.03a, 1996-2006.

Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

## INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** 655 Pulaski Street

**Historic Name:** Unknown

**Present Use:** Residential, Permanent

**Historic Use:** Residential, Permanent

**Construction Date:** c. 1920      **Source:** Stylistic and material evidence

**Alteration Date(s):** N/A      **Source:** N/A

**Designer:** Unknown      **Physical Condition:** Good

**Builder:** Unknown      **Remaining Historic Fabric:** Medium

**Style:** Colonial Revival

**Form:** Side Hall      **Stories:** 2.5

**Type:** Other      **Bays:** 3

**Roof Finish Materials:** Asphalt shingle

**Exterior Finish Materials** Aluminum siding

**Exterior Description:** Built circa 1920, this Colonial Revival-style dwelling is two and a half stories and three bays wide capped with a hipped-roof with wide eaves. The house is set on a rock-faced concrete-block foundation and is clad with aluminum siding. A two-story three-sided bay is centered on the south elevation, and the house contains six-over-one wood windows on the first story and one-over-one vinyl replacement windows on the second story. Dominating the façade is a hipped-roof dormer, and rising above the asphalt-shingle roof is one brick corbeled chimney.

**Interior Description:** N/A

### Setting:

Located on the northwest side of Pulaski Street, this dwelling is sited on a level lot. A driveway stands to the southwest side of the house and several foundation plantings front the house, which is set close to the street.

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Survey Name: Goethals Bridge Replacement      Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. This dwelling is associated with the "Suburban Development" historic context.

**Significance:** Dating to circa 1920, the house at 655 Pulaski Street is a common example of the Colonial Revival style. During the first half of the twentieth century, the Colonial Revival style became the most popular stylistic choice across the United States. Features such as a double-pile plan, hipped roof, two stories, and dormers are typical characteristics of Colonial Revival-style houses of this period as seen with 655 Pulaski Street. Many of the houses in this style are modest and lack any great amount of detail. The Colonial Revival style became a popular choice among middle- and upper-class Americans for their houses, as well as other types such as ecclesiastical and institutional, during the first half of the twentieth century. The style's zenith occurred between the 1920s and 1950s and continues to be prevalent at the turn of the twenty-first century.

**Eligibility for New Jersey**

**and National Registers:**

Yes  No

**National**

**Register Criteria:**

A  B  C  D

**Level of Significance**

Local  State  National

**Justification of Eligibility/Ineligibility:** The dwelling at 655 Pulaski Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1920, the house at 655 Pulaski Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 655 Pulaski Street is unlikely to provide information important to our understanding of history (Criterion D). This dwelling also lacks integrity, which has been diminished with the replacement siding and several replacement windows.

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

**Narrative Boundary Description:** The property is located at 655 Pulaski Street in Elizabeth, Union County. It is associated with block 4 lot 298. It is bounded to the southeast by Pulaski Street.

Survey Name: Goethals Bridge Replacement Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 653 Pulaski Street

**Street Address:** Street #: 653 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Pulaski Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 297

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

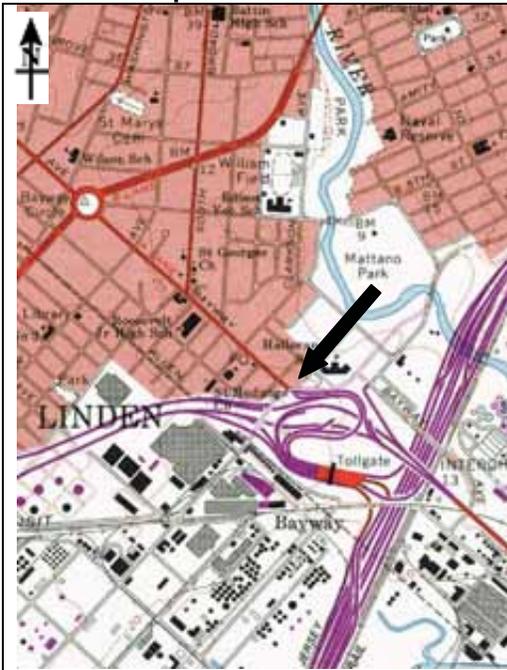
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Maptech. Terrain Navigator, Version 7.03a, 1996-2006.

Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

### INTENSIVE LEVEL USE ONLY

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** House, 653 Pulaski Street  
**Historic Name:** Unknown  
**Present Use:** Residential, Permanent  
**Historic Use:** Residential, Permanent  
**Construction Date:** c. 1920      **Source:** Stylistic and material evidence  
**Alteration Date(s):** N/A      **Source:** N/A  
**Designer:** Unknown      **Physical Condition:** Good  
**Builder:** Unknown      **Remaining Historic Fabric:** Medium  
**Style:** Colonial Revival  
**Form:** Other      **Stories:** 1.5  
**Type:** Other      **Bays:** 2  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials:** Vinyl siding

**Exterior Description:** Built circa 1920, the one-and-a-half-story wood-frame house at 653 Pulaski Street is set on a stuccoed foundation and topped by a hipped roof. It is clad with vinyl siding simulated to appear as hand-split cedar shakes. The main façade features two bays containing a side entry with a modern wood panel door and a grouped set of four aluminum casement windows. Large hipped-roof dormers with kicked eaves adorn both the facade and northeast and southwest side elevations. Each of the dormers contains paired one-over-one wood-sash windows. Windows throughout the house are one-over-one aluminum. The roof is sheathed with asphalt shingles and features wide eaves and an aluminum cornice.

**Interior Description:** N/A

**Setting:** Located on the northwest side of Pulaski Street, this dwelling is sited on a level lot. A driveway stands to the northeast side of the house. The house is fronted with foundation plantings and the property is dotted with a few large mature trees. The modern one-story brick garage stands to the west behind the house. The garage features stretcher-bond brick walling, gable-front roof with asphalt shingles, and an off-center overhead roll aluminum garage door. Aluminum siding covers the gable peak.

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Survey Name: Goethals Bridge Replacement      Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the river occurred in the last half of the twentieth century, particularly following World War II. This dwelling is associated with the "Suburban Development" historic context.

**Significance:** The property at 653 Pulaski Street is a typical vernacular dwelling dating to circa 1920 in Union County, New Jersey. This modest house lacks embellishment and its integrity has been diminished with the synthetic siding and replacement windows.

**Eligibility for New Jersey**

**and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The dwelling at 653 Pulaski Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1920, the house at 653 Pulaski Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 653 Pulaski Street is unlikely to provide information important to our understanding of history (Criterion D).

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

**Narrative Boundary Description:** The property is located at 653 Pulaski Street in Elizabeth, Union County. It is associated with block 4 lot 297. It is bounded to the southeast by Pulaski Street.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# CONTINUATION SHEET

Historic Sites #:

653 Pulaski Street, Elizabeth, Union County



653 Pulaski Street



Southeast Elevation

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## CONTINUATION SHEET

Historic Sites #:

653 Pulaski Street, Elizabeth, Union County



East Corner, Looking Northwest



Southeast Elevation

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

**Property Name:** 501 Richmond Street

**Street Address:** Street #: 501 Apartment #: \_\_\_\_\_  
(Low) (High) (Low) (High)

Prefix: \_\_\_\_\_ Street Name: Richmond Suffix: \_\_\_\_\_ Type: ST

**County(s):** Union **Zip Code:** 07202

**Municipality(s):** Elizabeth City **Block(s):** 4

**Local Place Name(s):** Elizabeth **Lot(s):** 1280.A

**Ownership::** Private **USGS Quad(s)** Elizabeth

## Description:

See building attachment

## Registration and Status Dates:

National Historic Landmark: \_\_\_\_\_

SHPO Opinion: \_\_\_\_\_

National Register: \_\_\_\_\_

Local Designation: \_\_\_\_\_

New Jersey Register: \_\_\_\_\_

Other Designation: \_\_\_\_\_

Determination of Eligibility: \_\_\_\_\_

Other Designation Date: \_\_\_\_\_

## Photograph:



Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

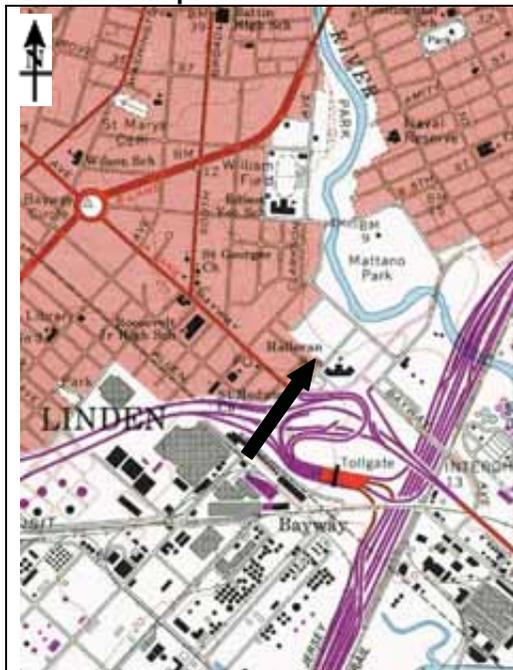
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# BASE FORM

Historic Sites #:

Location Map:



Site Map:



**Bibliography/Sources:** Maptech. Terrain Navigator, Version 7.03a, 1996-2006.

Union County, New Jersey. Real Property Tax Maps. Accessed on March 19, 2007 at <http://ucgis.ucnj.org/gismaps/onpoint>.

**Additional Information:** N/A

**More Research Needed?**  Yes  No

**INTENSIVE LEVEL USE ONLY**

**Attachments Included:**  Building  Structure  Object  Bridge  
 Landscape  Industry

**Within Historic District?**  Yes  No

**Status:**  Key-Contributing  Contributing  Non-Contributing

**Associated Archaeological Site/Deposit?**  Yes  
(Known or potential Sites – if yes, please describe briefly)

Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## BUILDING ATTACHMENT

Historic Sites #:

**Common Name:** 501 Richmond Street  
**Historic Name:** Unknown  
**Present Use:** Residential, Permanent  
**Historic Use:** Residential, Permanent  
**Construction Date:** c. 1950      **Source:** Stylistic and material evidence  
**Alteration Date(s):** N/A      **Source:** N/A  
**Designer:** Unknown      **Physical Condition:** Good  
**Builder:** Unknown      **Remaining Historic Fabric:** Medium  
**Style:** Colonial Revival  
**Form:** Other      **Stories:** 1.5  
**Type:** Other      **Bays:** 3  
**Roof Finish Materials:** Asphalt shingle  
**Exterior Finish Materials** Brick, stretcher bond

**Exterior Description:** This wood-frame, L-shaped dwelling is one and a half stories and three bays across on the façade set on a stuccoed foundation. The facade is covered with stretcher-bond brick and possesses a wood slab door with upper lights in the projecting front-gable protected by a metal hood supported by decorative metal posts. This front-gable section also contains a basement-level roll-up garage door and a triple one-over-one aluminum window with a brick rowlock sill. The facade also features a tripartite window with two-over-two horizontal wood-sash windows flanking a one-light fixed window with a continuous brick rowlock sill. The façade's front gable is clad with aluminum siding. The side and rear elevations are clad with wood shakes. In addition to the one-over-one and Chicago-style windows, the house contains several one-by-one sliding aluminum units. The rear elevation features a shed-roof two-bay dormer. An exterior-end brick half-shouldered chimney rises along the southeast elevation. Other architectural features of the dwelling include metal awnings on the façade, overhanging eaves, and a boxed aluminum-clad cornice.

According to a resident, the house at 501 Richmond Avenue was moved to its current location when Interchange 13 of the New Jersey Turnpike was constructed.

**Interior Description:** N/A

### Setting:

Facing southwest, this dwelling is sited on a level grassy lot, and a concrete driveway leads to the house. The house is fronted with foundation plantings and the property is dotted with a few trees and shrubs.

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Survey Name: Goethals Bridge Replacement      Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

## ELIGIBILITY WORKSHEET

Historic Sites #:

**History:** South of Newark, the city of Elizabeth developed along the shore of the Arthur Kill straddling the Elizabeth River. Much of the land on the north and east sides of the river was organized by the late nineteenth century in a linear fashion employing the use of rectangular blocks and lots. Through land-use planning from the early 1920s developed by the Olmsted Brothers Landscape Architecture Firm, a large extent of the Elizabeth River shoreline remained undeveloped in Elizabeth. One of the parks that the Olmsted firm designed in Union County was Elizabeth Park (1926-1958). Later renamed Mattano Park, it became part of a greenway system associated with the Elizabeth River Park. Starting in the late 1920s and early 1930s, the streets bordering the park as well as the Elizabeth River experienced slow development. The area around the park is notable for its mixture of speculative residential construction and single-family and multi-family buildings, as well as several mixed-use buildings (commercial and residential). Directly west of Mattano Park on the west side of the river is a block of speculative multi-family housing. To the south of the park and river is a combination of industrial, educational, and commercial buildings sited near the shoreline of the Elizabeth River. A wide variety of buildings occupy this small southern section of Elizabeth dating as early as the late nineteenth century; however, much of the development around the southern section of the River occurred in the last half of the 20<sup>th</sup> century, particularly following World War II. This property is associated with the "Modern New Jersey" historic context.

**Significance:** The property at 501 Richmond Street is a typical vernacular dwelling dating to circa 1950 in Union County, New Jersey. This modest house lacks any embellishments and is not an uncommon form within the area.

**Eligibility for New Jersey**

**and National Registers:**

Yes

No

**National**

**Register Criteria:**

A

B

C

D

**Level of Significance**

Local

State

National

**Justification of Eligibility/Ineligibility:** The dwelling at 501 Richmond Street in Elizabeth does not appear to meet National Register eligibility criteria. The property is not related to significant historic trends, events, or persons (Criteria A and B). Built circa 1950, the house at 501 Richmond Street does not represent any distinctive characteristics of the period, nor does it represent the work of a master, or possess high artistic values (Criterion C). The property at 501 Richmond Street is unlikely to provide information important to our understanding of history (Criterion D).

**For Historic Districts Only:**

**Property Count:** Key Contributing: \_\_\_\_\_ Contributing: \_\_\_\_\_ Non Contributing: \_\_\_\_\_

**For Individual Properties Only:**

**List the completed attachments related to the property's significance:**

**Narrative Boundary Description:** The property is located at 501 Richmond Street in Elizabeth, Union County. It is associated with block 4 lot 1280A. It is located at the north corner of Richmond Street, Pulaski Street.

Survey Name: Goethals Bridge Replacement

Date: May 3, 2007

Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer

Organization: The Louis Berger Group Inc.

# CONTINUATION SHEET

Historic Sites #:

501 Richmond Street, Elizabeth, Union County



West Corner



East Corner

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Survey Name: Goethals Bridge Replacement Date: May 3, 2007  
Surveyor: Stuart Dixon, Surveyor; Kristie Baynard, Preparer  
Organization: The Louis Berger Group Inc.

# **APPENDIX BC**

## **NYSOPRHP HISTORIC RESOURCE INVENTORY FORMS**

# HISTORIC RESOURCE INVENTORY FORM



NYS OFFICE OF PARKS, RECREATION  
& HISTORIC PRESERVATION  
P.O. BOX 189, WATERFORD, NY 12188  
(518) 237-8643

OFFICE USE ONLY
USN: _____

**IDENTIFICATION**

Property name(if any) \_\_\_\_\_  
 Address or Street Location 17 Lilac Court (Block 1707 Lot 5)  
 County Richmond Town/City Staten Island Village/Hamlet: \_\_\_\_\_  
 Owner David & Marie Troianiello Address 17 Lilac Court, Staten Island, NY 10303  
 Original use Dwelling Current use Dwelling  
 Architect/Builder, if known Unknown Date of construction, if known ca. 1915

**DESCRIPTION**

Materials -- please check those materials that are visible

Exterior Walls:	<input type="checkbox"/> wood clapboard	<input type="checkbox"/> wood shingle	<input type="checkbox"/> vertical boards	<input type="checkbox"/> plywood	
	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input type="checkbox"/> poured concrete	<input type="checkbox"/> concrete block	
	<input checked="" type="checkbox"/> vinyl siding	<input type="checkbox"/> aluminum siding	<input type="checkbox"/> cement-asbestos	<input type="checkbox"/> other: _____	
Roof:	<input checked="" type="checkbox"/> asphalt, shingle	<input type="checkbox"/> asphalt, roll	<input type="checkbox"/> wood shingle	<input type="checkbox"/> metal	<input type="checkbox"/> slate
Foundation:	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input checked="" type="checkbox"/> poured concrete	<input checked="" type="checkbox"/> concrete block	

Other materials and their location: brick and stone stoop; aluminum cornice; aluminum sash

Alterations, if known: one-story gable addition on rear Date: Unknown

Condition:  excellent  good  fair  deteriorated

**Photos**

Provide several clear, original photographs of the property proposed for nomination. Submitted views should represent the property as a whole. For buildings or structures, this includes exterior and interior views, general setting, outbuildings and landscape features. Color prints are acceptable for initial submissions.

Please staple one photograph providing a complete view of the structure or property to the front of this sheet. Additional views should be submitted in a separate envelope or stapled to a continuation sheet.

**Maps**

Attach a printed or drawn locational map indicating the location of the property in relationship to streets, intersections or other widely recognized features so that the property can be accurately positioned. Show a north arrow. Include a scale or estimate distances where possible.

Prepared by: Stuart Paul Dixon address The Louis Berger Group, Inc., 120 Halsted Street, East Orange, NJ 07018  
 Telephone: 973-678-1960 email sdixon@louisberger.com Date March 2007

**PLEASE PROVIDE THE FOLLOWING INFORMATION**

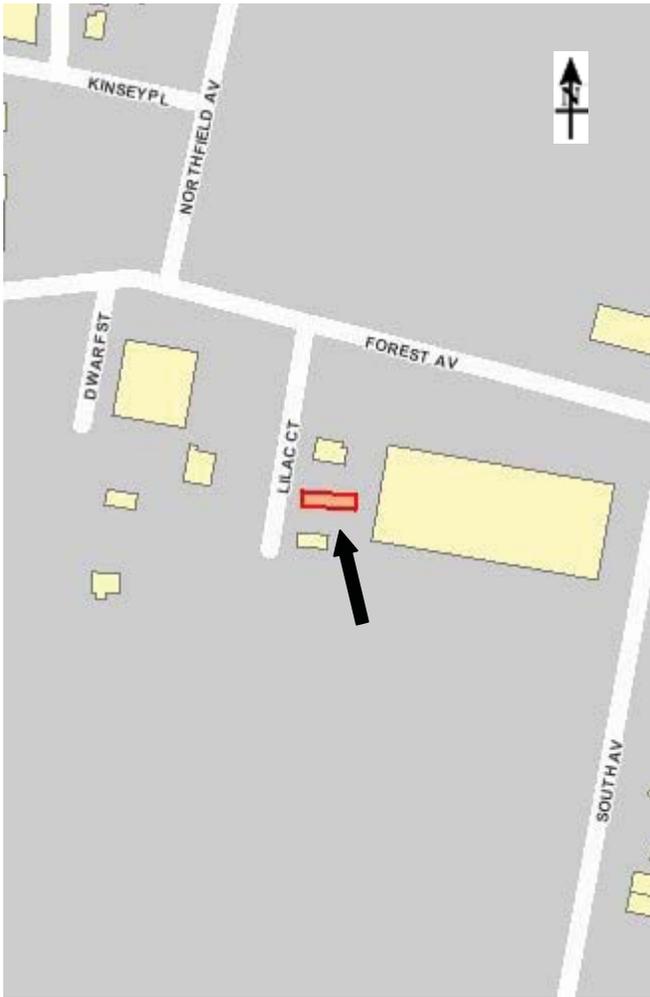
**IF YOU ARE PREPARING A NATIONAL REGISTER NOMINATION, PLEASE REFER TO THE ATTACHED INSTRUCTIONS**

**Narrative Description of Property:** Briefly describe the property and its setting. Include a verbal description of the location (e.g., north side of NY 17, west of Jones Road); a general description of the building, structure or feature including such items as architectural style (if known), number of stories, type and shape of roof (flat, gabled, mansard, shed or other), materials and landscape features. Identify and describe any associated buildings, structures or features on the property, such as garages, silos, privies, pools, gravesites. Identify any known exterior and interior alterations such as additions, replacement windows, aluminum or vinyl siding or changes in plan. Include dates of construction and alteration, if known. Attach additional sheets as needed.

Located on the eastern side of Lilac Court south of Forest Avenue, the house at 17 Lilac Court stands two stories tall with an asphalt-shingled gable-front roof, vinyl siding, a poured-concrete and concrete block foundation, and an aluminum cornice. A one-story gable-roofed addition is appended to its rear, east elevation. One bay wide on its west by two bays deep on its south elevation, the building displays a full-width hipped-roof enclosed porch on the west elevation's first story. A three-sided oriel composed of aluminum casement windows and an aluminum and glass door occupies the porch's west elevation. A brick and stone stoop sits below the porch entrance. Most window openings contain one-over-one aluminum sash. An octagonal wood-framed window penetrating the north elevation probably lights an interior stair landing. A four-panel wood door with two upper lights pierces the north elevation at grade and provides access to the basement level. The rear addition possesses paired French doors without a stoop and paired metal casements in its north elevation. The house is located in an area of mixed residential and commercial structures.

**Narrative Description of Significance:** Briefly describe those characteristics by which this property may be considered historically significant. Significance may include, but is not limited to, a structure being an intact representative of an architectural or engineering type or style (e.g., Gothic Revival style cottage, Pratt through-truss bridge); association with historic events or broad patterns of local, state or national history (e.g., a cotton mill from a period of growth in local industry, a seaside cottage representing a locale's history as a resort community, a structure associated with activities of the "underground railroad."); or by association with persons or organizations significant at a local, state or national level. Simply put, why is this property important to you and the community. Attach additional sheets as needed.

New York building data indicates that construction of the house at 17 Lilac Court occurred in 1915. This date is consistent with the period of popularity for gable-front residential construction during the early twentieth century. The building and Lilac Court are not depicted on an 1898 United States Geological Survey map of Staten Island. Owing to the nearly wholesale replacement of historic features and the appending of the rear addition, the building retains very little architectural integrity from the period of its construction and is therefore not a noteworthy example of a particular architectural style or method of construction (National Register Criterion C). Research has not identified any significant relationships between the building and historic events, trends, broad patterns, or individuals (Criteria A and B). The house is unlikely to provide information important to our understanding of history (Criterion D). The house at 17 Lilac Court is therefore recommended as not eligible for listing in the National Register.



17 Lilac Court, Staten Island

Source: <http://gis.nyc.gov/doitt/mp/Portal.do>

Scale not provided.



North and West Elevations



South and East Elevations

# HISTORIC RESOURCE INVENTORY FORM



NYS OFFICE OF PARKS, RECREATION  
& HISTORIC PRESERVATION  
P.O. BOX 189, WATERFORD, NY 12188  
(518) 237-8643

OFFICE USE ONLY
USN: _____

## IDENTIFICATION

Property name(if any) \_\_\_\_\_  
 Address or Street Location 11 Lilac Court (Block 1707 Lot 38)  
 County Richmond Town/City Staten Island Village/Hamlet: \_\_\_\_\_  
 Owner N. & M. Troianiello Address 11 Lilac Court, Staten Island, NY 10303  
 Original use Dwelling Current use Dwelling  
 Architect/Builder, if known Unknown Date of construction, if known ca. 1940

## DESCRIPTION

Materials -- please check those materials that are visible

Exterior Walls:	<input type="checkbox"/> wood clapboard	<input checked="" type="checkbox"/> wood shingle	<input type="checkbox"/> vertical boards	<input type="checkbox"/> plywood	
	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input type="checkbox"/> poured concrete	<input type="checkbox"/> concrete block	
	<input type="checkbox"/> vinyl siding	<input checked="" type="checkbox"/> aluminum siding	<input type="checkbox"/> cement-asbestos	<input type="checkbox"/> other: _____	
Roof:	<input checked="" type="checkbox"/> asphalt, shingle	<input type="checkbox"/> asphalt, roll	<input type="checkbox"/> wood shingle	<input type="checkbox"/> metal	<input type="checkbox"/> slate
Foundation:	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input checked="" type="checkbox"/> poured concrete	<input type="checkbox"/> concrete block	

Other materials and their location: brick stoop; aluminum cornice; stuccoed chimney; wood sash

Alterations, if known: one-story enclosed porch front; one-story shed roof addition on rear Date: Unknown

Condition:  excellent  good  fair  deteriorated

## Photos

Provide several clear, original photographs of the property proposed for nomination. Submitted views should represent the property as a whole. For buildings or structures, this includes exterior and interior views, general setting, outbuildings and landscape features. Color prints are acceptable for initial submissions.

Please staple one photograph providing a complete view of the structure or property to the front of this sheet. Additional views should be submitted in a separate envelope or stapled to a continuation sheet.

## Maps

Attach a printed or drawn locational map indicating the location of the property in relationship to streets, intersections or other widely recognized features so that the property can be accurately positioned. Show a north arrow. Include a scale or estimate distances where possible.

Prepared by: Stuart Paul Dixon address The Louis Berger Group, Inc., 120 Halsted Street, East Orange, NJ 07018  
 Telephone: 973-678-1960 email sdixon@louisberger.com Date March 2007

**PLEASE PROVIDE THE FOLLOWING INFORMATION**

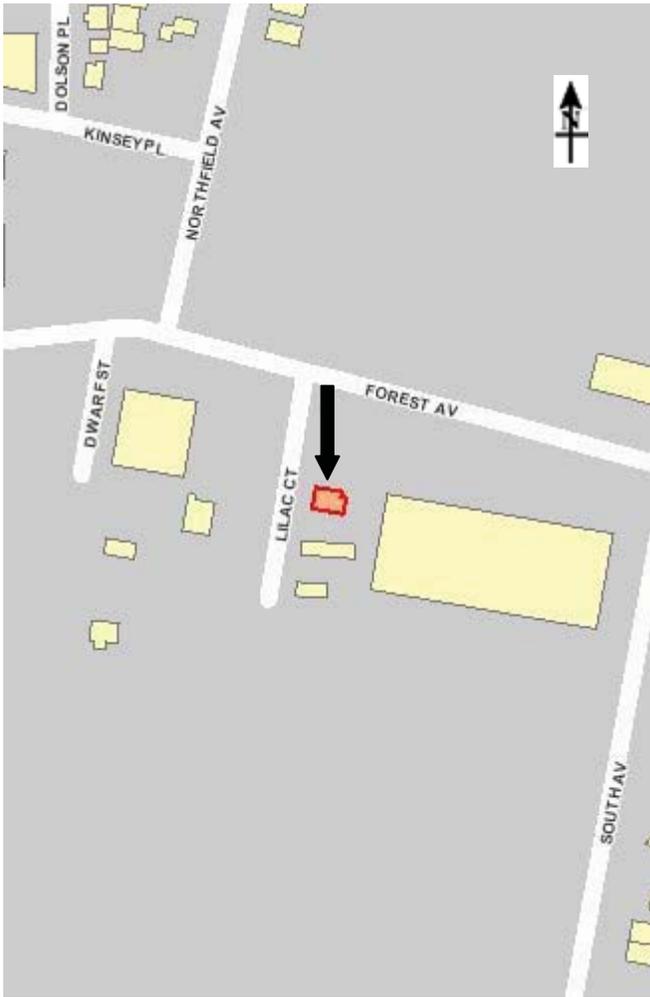
**IF YOU ARE PREPARING A NATIONAL REGISTER NOMINATION, PLEASE REFER TO THE ATTACHED INSTRUCTIONS**

**Narrative Description of Property:** Briefly describe the property and its setting. Include a verbal description of the location (e.g., north side of NY 17, west of Jones Road); a general description of the building, structure or feature including such items as architectural style (if known), number of stories, type and shape of roof (flat, gabled, mansard, shed or other), materials and landscape features. Identify and describe any associated buildings, structures or features on the property, such as garages, silos, privies, pools, gravesites. Identify any known exterior and interior alterations such as additions, replacement windows, aluminum or vinyl siding or changes in plan. Include dates of construction and alteration, if known. Attach additional sheets as needed.

Located on the east side of Lilac Court south of Forest Avenue, the dwelling at 11 Lilac Court stands two stories tall with an asphalt-shingled side-gable roof with a full-width, hipped roof addition along its west elevation facing Lilac and a shed-roof vestibule with a small deck on its southeastern corner. The house has wood shingle siding on its first story and aluminum siding on its upper story. Two bays penetrate its west elevation. A single bay occupies the first story of the addition's and the main structure's first story. A poured concrete foundation supports the building. A stuccoed chimney pile pierces the center of the main roof ridge. Most window openings feature single or paired one-over-one wood sash. A Chicago-style window composed of a large fixed light flanked by one-over-one sash units occupies the northern of the addition's two west elevation bays. The southern bay has a wood door with three bulls-eye panels and a brick stoop with a metal handrail. The house stands amidst a mixed residential and commercial neighborhood.

**Narrative Description of Significance:** Briefly describe those characteristics by which this property may be considered historically significant. Significance may include, but is not limited to, a structure being an intact representative of an architectural or engineering type or style (e.g., Gothic Revival style cottage, Pratt through-truss bridge); association with historic events or broad patterns of local, state or national history (e.g., a cotton mill from a period of growth in local industry, a seaside cottage representing a locale's history as a resort community, a structure associated with activities of the "underground railroad."); or by association with persons or organizations significant at a local, state or national level. Simply put, why is this property important to you and the community. Attach additional sheets as needed.

New York building data indicates that construction of the house at 11 Lilac Court occurred in 1940. The building's side-gable form reflects residential construction of the early twentieth century; however, the building retains very little architectural integrity from the period of its construction and is therefore not a noteworthy example of a particular architectural style or method of construction (National Register Criterion C). Research has not identified any significant relationships between the building and historic events, trends, broad patterns, or individuals (Criteria A and B). The house is unlikely to provide information important to our understanding of history (Criterion D). The house at 11 Lilac Court is therefore recommended as not eligible for listing in the National Register.



11 Lilac Court, Staten Island

Source: <http://gis.nyc.gov/doitt/mp/Portal.do>

Scale not provided.



West and South Elevations

# HISTORIC RESOURCE INVENTORY FORM



NYS OFFICE OF PARKS, RECREATION  
& HISTORIC PRESERVATION  
P.O. BOX 189, WATERFORD, NY 12188  
(518) 237-8643

OFFICE USE ONLY

USN:

## IDENTIFICATION

Property name(if any) \_\_\_\_\_  
Address or Street Location 885 Morrow Street (Block 1384 Lot 3)  
County Richmond Town/City Staten Island Village/Hamlet: \_\_\_\_\_  
Owner Delores Virga Address 885 Morrow Street, Staten Island, NY 10303  
Original use Dwelling Current use Abandoned  
Architect/Builder, if known Unknown Date of construction, if known ca. 1915

## DESCRIPTION

Materials -- please check those materials that are visible

Exterior Walls:	<input type="checkbox"/> wood clapboard	<input type="checkbox"/> wood shingle	<input type="checkbox"/> vertical boards	<input type="checkbox"/> plywood	
	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input type="checkbox"/> poured concrete	<input type="checkbox"/> concrete block	
	<input checked="" type="checkbox"/> vinyl siding	<input type="checkbox"/> aluminum siding	<input type="checkbox"/> cement-asbestos	<input type="checkbox"/> other: _____	
Roof:	<input checked="" type="checkbox"/> asphalt, shingle	<input type="checkbox"/> asphalt, roll	<input type="checkbox"/> wood shingle	<input type="checkbox"/> metal	<input type="checkbox"/> slate
Foundation:	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input type="checkbox"/> poured concrete	<input type="checkbox"/> concrete block	

Other materials and their location: wood sash

Alterations, if known: \_\_\_\_\_ Date: \_\_\_\_\_

Condition:  excellent  good  fair  deteriorated

## Photos

Provide several clear, original photographs of the property proposed for nomination. Submitted views should represent the property as a whole. For buildings or structures, this includes exterior and interior views, general setting, outbuildings and landscape features. Color prints are acceptable for initial submissions.

Please staple one photograph providing a complete view of the structure or property to the front of this sheet. Additional views should be submitted in a separate envelope or stapled to a continuation sheet.

## Maps

Attach a printed or drawn locational map indicating the location of the property in relationship to streets, intersections or other widely recognized features so that the property can be accurately positioned. Show a north arrow. Include a scale or estimate distances where possible.

Prepared by: Stuart Paul Dixon address The Louis Berger Group, Inc., 120 Halsted Street, East Orange, NJ 07018

Telephone: 973-678-1960 email sdixon@louisberger.com Date March 2007

**PLEASE PROVIDE THE FOLLOWING INFORMATION**

IF YOU ARE PREPARING A NATIONAL REGISTER NOMINATION, PLEASE REFER TO THE ATTACHED INSTRUCTIONS

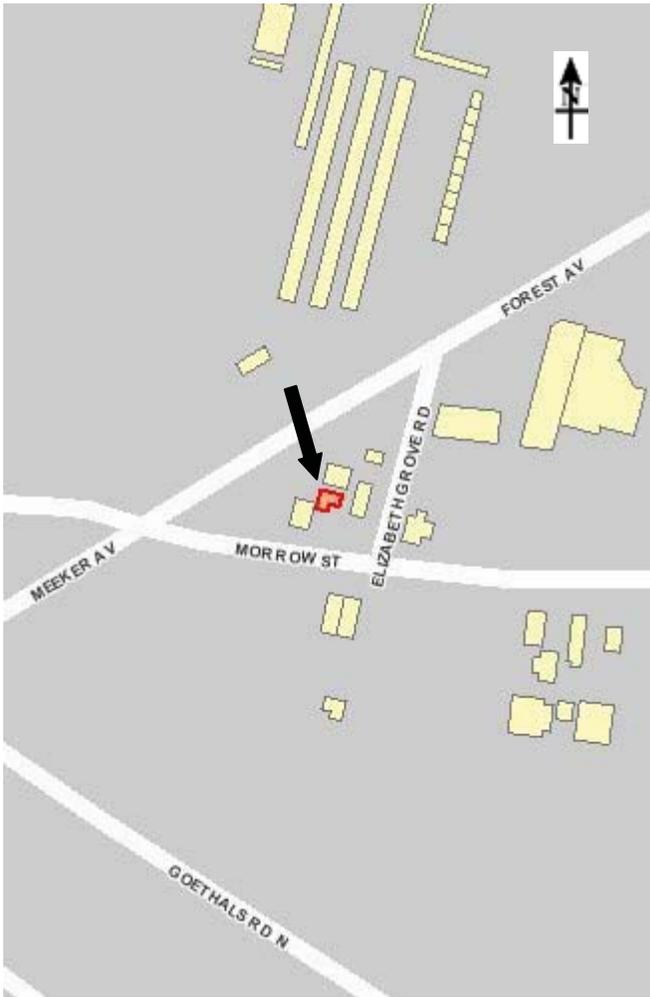
**Narrative Description of Property:** Briefly describe the property and its setting. Include a verbal description of the location (e.g., north side of NY 17, west of Jones Road); a general description of the building, structure or feature including such items as architectural style (if known), number of stories, type and shape of roof (flat, gabled, mansard, shed or other), materials and landscape features. Identify and describe any associated buildings, structures or features on the property, such as garages, silos, privies, pools, gravesites. Identify any known exterior and interior alterations such as additions, replacement windows, aluminum or vinyl siding or changes in plan. Include dates of construction and alteration, if known. Attach additional sheets as needed.

The former house at 885 Morrow Street, now abandoned with its former yard enclosed by a chain link fence and used for the storage of construction equipment, is located on the north side of Morrow Street east of its intersection with Forest Avenue. The house consists of a two-story gable-roofed ell-plan house with asphalt roof shingles, vinyl siding, and pedimented gable ends on its north and south gables. Two bays penetrate the building's second-story west and south elevations. An additional two bays occupy the ell's second-story south elevation. Window openings contain two-over-two wood sash units on the second story and small one-over-one aluminum sash units in the gable peaks. Access to the property was limited and the first-story and foundation elements were not visible.

The property also includes a one-story gable roofed garage on the rear, north portion of the lot. A flat-roofed full-width addition appended to the garage's west gable hides the garage's original vehicular opening. The garage features roll asphalt on its roof and wood shingle siding. The addition possesses double leaf wood slab doors and plywood siding. A building located on New York Map Portal maps southwest of the dwelling no longer stands on the property. A park is located west of the property on the west side of Forest Avenue. Mixed commercial and residential buildings are located east of the house.

**Narrative Description of Significance:** Briefly describe those characteristics by which this property may be considered historically significant. Significance may include, but is not limited to, a structure being an intact representative of an architectural or engineering type or style (e.g., Gothic Revival style cottage, Pratt through-truss bridge); association with historic events or broad patterns of local, state or national history (e.g., a cotton mill from a period of growth in local industry, a seaside cottage representing a locale's history as a resort community, a structure associated with activities of the "underground railroad."); or by association with persons or organizations significant at a local, state or national level. Simply put, why is this property important to you and the community. Attach additional sheets as needed.

New York building data indicates that construction of the house at 885 Morrow Street occurred in 1915. The building's overall form and materials is consistent with late nineteenth- or early twentieth-century construction. Although much of the present road network had not been laid out yet, an 1898 United States Geological Survey map of Staten Island appears to depict a building at or near the site of the present structure at 885 Morrow Street. A 1917 atlas map of the area does show a frame building at this address. The building, however, retains very little architectural integrity from the period of its construction and is therefore not a noteworthy example of a particular architectural style or method of construction (National Register Criterion C). Research has not identified any significant relationships between the building and historic events, trends, broad patterns, or individuals (Criteria A and B). The house is unlikely to provide information important to our understanding of history (Criterion D). The house at 885 Morrow Street is therefore recommended as not eligible for listing in the National Register.



885 Morrow Street, Staten Island

Source: <http://gis.nyc.gov/doitt/mp/Portal.do>

Scale not provided.



South Elevation



North Elevations

# HISTORIC RESOURCE INVENTORY FORM



NYS OFFICE OF PARKS, RECREATION  
& HISTORIC PRESERVATION  
P.O. BOX 189, WATERFORD, NY 12188  
(518) 237-8643

OFFICE USE ONLY

USN:

## IDENTIFICATION

Property name(if any) \_\_\_\_\_  
Address or Street Location 856 Morrow Street (Block 1717 Lot 56)  
County Richmond Town/City Staten Island Village/Hamlet: \_\_\_\_\_  
Owner Salvatore Coluccio Address 856 Morrow Street, Staten Island, NY 10303  
Original use Dwelling Current use Offices  
Architect/Builder, if known Unknown Date of construction, if known ca. 1940

## DESCRIPTION

Materials -- please check those materials that are visible

Exterior Walls:	<input type="checkbox"/> wood clapboard	<input type="checkbox"/> wood shingle	<input type="checkbox"/> vertical boards	<input type="checkbox"/> plywood	
	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input type="checkbox"/> poured concrete	<input checked="" type="checkbox"/> concrete block	
	<input type="checkbox"/> vinyl siding	<input checked="" type="checkbox"/> aluminum siding	<input checked="" type="checkbox"/> cement-asbestos	<input type="checkbox"/> other: _____	
Roof:	<input checked="" type="checkbox"/> asphalt, shingle	<input type="checkbox"/> asphalt, roll	<input type="checkbox"/> wood shingle	<input type="checkbox"/> metal	<input type="checkbox"/> slate
Foundation:	<input type="checkbox"/> stone	<input checked="" type="checkbox"/> brick	<input type="checkbox"/> poured concrete	<input checked="" type="checkbox"/> concrete block	

Other materials and their location: aluminum sash with wood surrounds; metal casement window

Alterations, if known: one-story concrete block garage wing northeast; gable-roofed addition southeast Date: Unknown

Condition:  excellent  good  fair  deteriorated

## Photos

Provide several clear, original photographs of the property proposed for nomination. Submitted views should represent the property as a whole. For buildings or structures, this includes exterior and interior views, general setting, outbuildings and landscape features. Color prints are acceptable for initial submissions.

Please staple one photograph providing a complete view of the structure or property to the front of this sheet. Additional views should be submitted in a separate envelope or stapled to a continuation sheet.

## Maps

Attach a printed or drawn locational map indicating the location of the property in relationship to streets, intersections or other widely recognized features so that the property can be accurately positioned. Show a north arrow. Include a scale or estimate distances where possible.

Prepared by: Stuart Paul Dixon address The Louis Berger Group, Inc., 120 Halsted Street, East Orange, NJ 07018

Telephone: 973-678-1960 email sdixon@louisberger.com Date March 2007

**PLEASE PROVIDE THE FOLLOWING INFORMATION**

**IF YOU ARE PREPARING A NATIONAL REGISTER NOMINATION, PLEASE REFER TO THE ATTACHED INSTRUCTIONS**

**Narrative Description of Property:** Briefly describe the property and its setting. Include a verbal description of the location (e.g., north side of NY 17, west of Jones Road); a general description of the building, structure or feature including such items as architectural style (if known), number of stories, type and shape of roof (flat, gabled, mansard, shed or other), materials and landscape features. Identify and describe any associated buildings, structures or features on the property, such as garages, silos, privies, pools, gravesites. Identify any known exterior and interior alterations such as additions, replacement windows, aluminum or vinyl siding or changes in plan. Include dates of construction and alteration, if known. Attach additional sheets as needed.

The former dwelling at 856 Morrow Street, now leased for office space, is located on the south side of Morrow at its intersection with Elizabeth Grove Road. The building consists of a two-story gable-front structure with a full-width one-story gable-roofed addition on its south side and a one- and two-story wing used as a garage appended to its east side. The original gable-front structure features asphalt roof shingles, asbestos siding, a brick foundation, and a full-width one-story hipped-roof enclosed porch along its north elevation. The two-bay-wide porch sits atop brick piers with concrete block installed between the piers. A metal storm door and a triple aluminum casement window occupy the porch's north elevation. The main two-story structure displays three symmetrically spaced windows on its north second story and two bays along its west side elevation. Most window openings contain single or paired one-over-one aluminum sash with wood surrounds and drip moldings. The eaves feature exposed rafter feet covered by a fascia board. A stuccoed chimney pile rises along the exterior of the two-story structure's south gable end. The two-bay-deep south addition displays a small aluminum one-over-one window shaded by an awning. The south bay is covered with asbestos shingles. A one-story gable-roofed addition sided with aluminum is appended to the addition's southwest corner. The eastern wing, composed of a rear two-story section and a one-story portion protruding north of the main structure's north elevation, may have been erected in two stages. The wing has concrete block walling, roll asphalt roofing, and a large overhead roll vehicle door in its north elevation. A metal slab pedestrian door penetrates the west elevation of the east wing's one-story portion. The house's former western yard is paved over and used as a parking lot.

**Narrative Description of Significance:** Briefly describe those characteristics by which this property may be considered historically significant. Significance may include, but is not limited to, a structure being an intact representative of an architectural or engineering type or style (e.g., Gothic Revival style cottage, Pratt through-truss bridge); association with historic events or broad patterns of local, state or national history (e.g., a cotton mill from a period of growth in local industry, a seaside cottage representing a locale's history as a resort community, a structure associated with activities of the "underground railroad."); or by association with persons or organizations significant at a local, state or national level. Simply put, why is this property important to you and the community. Attach additional sheets as needed.

New York building data indicates that construction of the house at 856 Morrow Street occurred in 1940. The building's gable-front form is indicative of late nineteenth- or early twentieth-century construction. An 1898 United States Geological Survey map of Staten Island does not depict a building at or near the site of the present structure at 856 Morrow Street. A 1917 atlas map of the area does show a frame building at this address. The appending of the garage wing and replacement of much of its historic fabric has left the building with little architectural integrity from the period of its construction. Thus the building is not a noteworthy example of a particular architectural style or method of construction (National Register Criterion C). Research has not identified any significant relationships between the building and historic events, trends, broad patterns, or individuals (Criteria A and B). The house is unlikely to provide information important to our understanding of history (Criterion D). The house at 856 Morrow Street is therefore recommended as not eligible for listing in the National Register.



856 Morrow Street, Staten Island

Source: <http://gis.nyc.gov/doitt/mp/Portal.do>

Scale not provided.



North and West Elevations



East and North Elevations

# HISTORIC RESOURCE INVENTORY FORM



NYS OFFICE OF PARKS, RECREATION  
& HISTORIC PRESERVATION  
P.O. BOX 189, WATERFORD, NY 12188  
(518) 237-8643

OFFICE USE ONLY

USN:

## IDENTIFICATION

Property name(if any) \_\_\_\_\_

Address or Street Location 834 Morrow Street (Block 1717 Lot 67)

County Richmond Town/City Staten Island Village/Hamlet: \_\_\_\_\_

Owner Charles & Dolores Vir Address 834 Morrow Street, Staten Island, NY 10303

Original use Dwelling Current use Offices

Architect/Builder, if known Unknown Date of construction, if known ca. 1920

## DESCRIPTION

Materials -- please check those materials that are visible

Exterior Walls:	<input type="checkbox"/> wood clapboard	<input type="checkbox"/> wood shingle	<input type="checkbox"/> vertical boards	<input type="checkbox"/> plywood	
	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input type="checkbox"/> poured concrete	<input type="checkbox"/> concrete block	
	<input type="checkbox"/> vinyl siding	<input type="checkbox"/> aluminum siding	<input checked="" type="checkbox"/> cement-asbestos	<input type="checkbox"/> other: _____	
Roof:	<input checked="" type="checkbox"/> asphalt, shingle	<input type="checkbox"/> asphalt, roll	<input type="checkbox"/> wood shingle	<input type="checkbox"/> metal	<input type="checkbox"/> slate
Foundation:	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input type="checkbox"/> poured concrete	<input type="checkbox"/> concrete block	

Other materials and their location: stuccoed foundation; aluminum sash

Alterations, if known: \_\_\_\_\_ Date: \_\_\_\_\_

Condition:  excellent  good  fair  deteriorated

## Photos

Provide several clear, original photographs of the property proposed for nomination. Submitted views should represent the property as a whole. For buildings or structures, this includes exterior and interior views, general setting, outbuildings and landscape features. Color prints are acceptable for initial submissions.

Please staple one photograph providing a complete view of the structure or property to the front of this sheet. Additional views should be submitted in a separate envelope or stapled to a continuation sheet.

## Maps

Attach a printed or drawn locational map indicating the location of the property in relationship to streets, intersections or other widely recognized features so that the property can be accurately positioned. Show a north arrow. Include a scale or estimate distances where possible.

Prepared by: Stuart Paul Dixon address The Louis Berger Group, Inc., 120 Halsted Street, East Orange, NJ 07018

Telephone: 973-678-1960 email sdixon@louisberger.com Date March 2007

**PLEASE PROVIDE THE FOLLOWING INFORMATION**

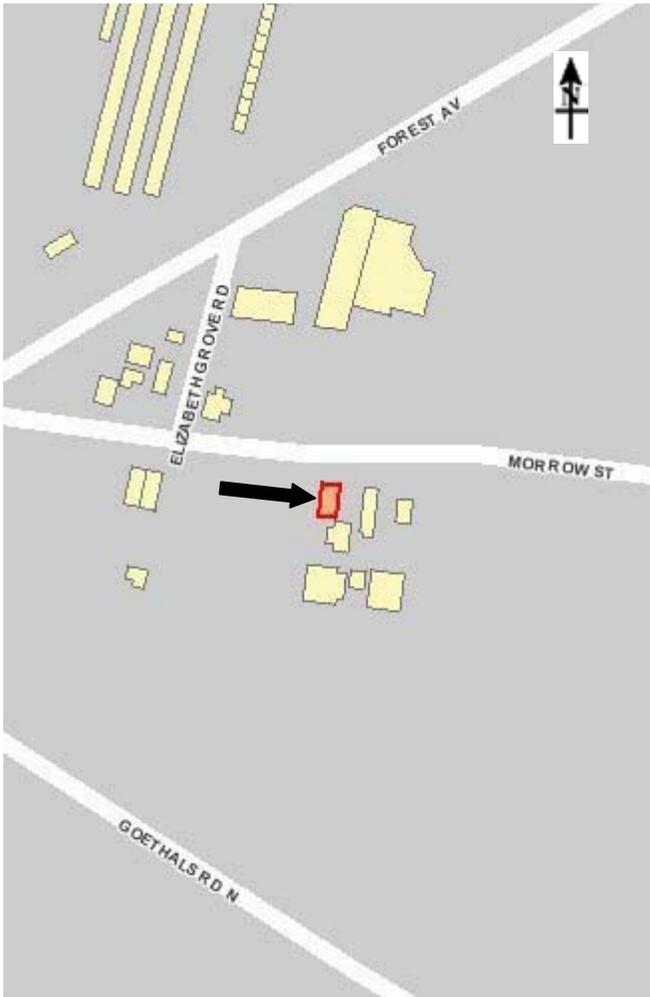
IF YOU ARE PREPARING A NATIONAL REGISTER NOMINATION, PLEASE REFER TO THE ATTACHED INSTRUCTIONS

**Narrative Description of Property:** Briefly describe the property and its setting. Include a verbal description of the location (e.g., north side of NY 17, west of Jones Road); a general description of the building, structure or feature including such items as architectural style (if known), number of stories, type and shape of roof (flat, gabled, mansard, shed or other), materials and landscape features. Identify and describe any associated buildings, structures or features on the property, such as garages, silos, privies, pools, gravesites. Identify any known exterior and interior alterations such as additions, replacement windows, aluminum or vinyl siding or changes in plan. Include dates of construction and alteration, if known. Attach additional sheets as needed.

The former dwelling located at 834 Morrow Street stands on the south side of Morrow east of its intersection with Elizabeth Grove Road. The area surrounding the house consists of mixed-use residential and commercial buildings. The house consists of a two-story gable-front structure, two bays wide by two bays deep along its west elevation, with asphalt roof shingles, asbestos siding, a stuccoed foundation, a box cornice, and a full-width one-story shed-roof addition on its rear south elevation. The building's main, north elevation facing Morrow Street features a two-story three-sided bay window on its west side and an enclosed one-story shed-roof porch that encloses the bay window's first-story eastern window. A modern wood slab door with upper lights penetrates the east side of the porch. Most window openings contain one-over-one aluminum sash with wood surrounds. The one-bay-deep south addition possesses a concrete block foundation and two one-by-one horizontal-slide aluminum cased windows. Mobile homes have been installed to the east and west side of the main building. To the rear or south of the main building stand a small one-story, gable-roofed modern frame shed and a larger metal-sided modern metal shop.

**Narrative Description of Significance:** Briefly describe those characteristics by which this property may be considered historically significant. Significance may include, but is not limited to, a structure being an intact representative of an architectural or engineering type or style (e.g., Gothic Revival style cottage, Pratt through-truss bridge); association with historic events or broad patterns of local, state or national history (e.g., a cotton mill from a period of growth in local industry, a seaside cottage representing a locale's history as a resort community, a structure associated with activities of the "underground railroad."); or by association with persons or organizations significant at a local, state or national level. Simply put, why is this property important to you and the community. Attach additional sheets as needed.

New York building data indicates that construction of the house at 834 Morrow Street occurred in 1920. The building's gable-front form reflects late nineteenth- or early twentieth-century construction. An 1898 United States Geological Survey map of Staten Island does not appear to depict a building at or near the site of the present structure at 834 Morrow Street. A 1917 atlas map of the area seems to show a frame building at this address. The installation of modern windows and the replacement of much of its historic fabric have left the building with little architectural integrity from the period of its construction. The building is therefore not a noteworthy example of a particular architectural style or method of construction (National Register Criterion C). Research has not identified any significant relationships between the building and historic events, trends, broad patterns, or individuals (Criteria A and B). The house is unlikely to provide information important to our understanding of history (Criterion D). The former house at 834 Morrow Street is therefore recommended as not eligible for listing in the National Register.



834 Morrow Street, Staten Island

Source: <http://gis.nyc.gov/doitt/mp/Portal.do>

Scale not provided.



North and West Elevations



South Elevation

# HISTORIC RESOURCE INVENTORY FORM



NYS OFFICE OF PARKS, RECREATION  
& HISTORIC PRESERVATION  
P.O. BOX 189, WATERFORD, NY 12188  
(518) 237-8643

OFFICE USE ONLY

USN:

## IDENTIFICATION

Property name(if any) \_\_\_\_\_

Address or Street Location 830 Morrow Street (Block 1717 Lot 72)

County Richmond Town/City Staten Island Village/Hamlet: \_\_\_\_\_

Owner Raymond Clay & Majorie M. Loony Address 830 Morrow Street, Staten Island, NY 10303

Original use Dwelling Current use Dwelling

Architect/Builder, if known Unknown Date of construction, if known ca. 1920

## DESCRIPTION

Materials -- please check those materials that are visible

Exterior Walls:	<input type="checkbox"/> wood clapboard	<input type="checkbox"/> wood shingle	<input type="checkbox"/> vertical boards	<input type="checkbox"/> plywood	
	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input type="checkbox"/> poured concrete	<input type="checkbox"/> concrete block	
	<input checked="" type="checkbox"/> vinyl siding	<input type="checkbox"/> aluminum siding	<input type="checkbox"/> cement-asbestos	<input type="checkbox"/> other: _____	
Roof:	<input checked="" type="checkbox"/> asphalt, shingle	<input type="checkbox"/> asphalt, roll	<input type="checkbox"/> wood shingle	<input type="checkbox"/> metal	<input type="checkbox"/> slate
Foundation:	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input type="checkbox"/> poured concrete	<input checked="" type="checkbox"/> concrete block	

Other materials and their location: stuccoed chimney interior; aluminum sash

Alterations, if known: one-story shed roof additions on rear Date: Unknown

Condition:  excellent  good  fair  deteriorated

## Photos

Provide several clear, original photographs of the property proposed for nomination. Submitted views should represent the property as a whole. For buildings or structures, this includes exterior and interior views, general setting, outbuildings and landscape features. Color prints are acceptable for initial submissions.

Please staple one photograph providing a complete view of the structure or property to the front of this sheet. Additional views should be submitted in a separate envelope or stapled to a continuation sheet.

## Maps

Attach a printed or drawn locational map indicating the location of the property in relationship to streets, intersections or other widely recognized features so that the property can be accurately positioned. Show a north arrow. Include a scale or estimate distances where possible.

Prepared by: Stuart Paul Dixon address The Louis Berger Group, Inc., 120 Halsted Street, East Orange, NJ 07018

Telephone: 973-678-1960 email sdixon@louisberger.com Date March 2007

**PLEASE PROVIDE THE FOLLOWING INFORMATION**

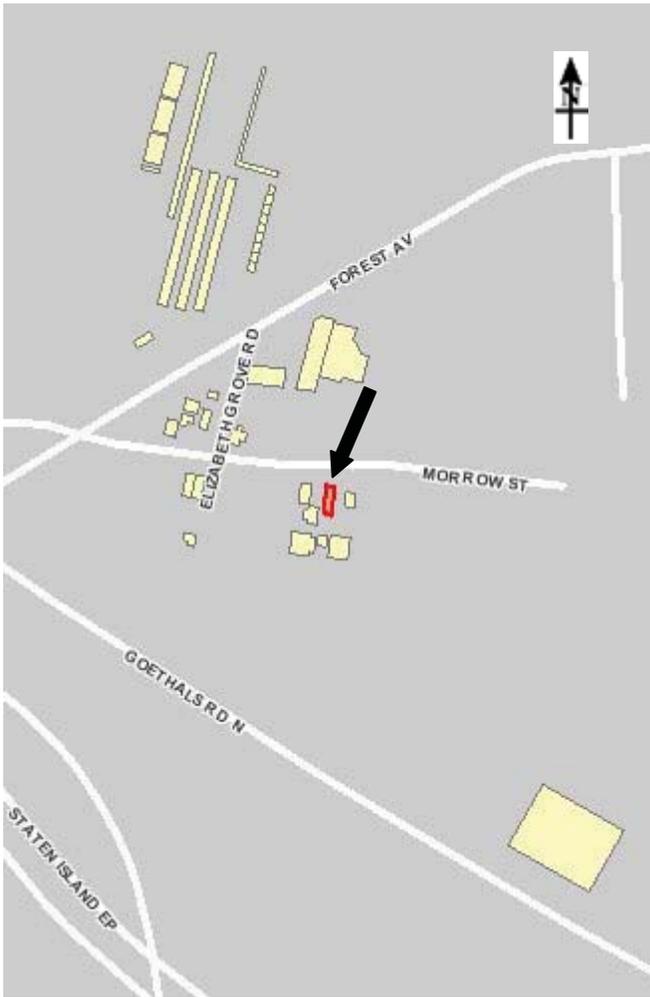
**IF YOU ARE PREPARING A NATIONAL REGISTER NOMINATION, PLEASE REFER TO THE ATTACHED INSTRUCTIONS**

**Narrative Description of Property:** Briefly describe the property and its setting. Include a verbal description of the location (e.g., north side of NY 17, west of Jones Road); a general description of the building, structure or feature including such items as architectural style (if known), number of stories, type and shape of roof (flat, gabled, mansard, shed or other), materials and landscape features. Identify and describe any associated buildings, structures or features on the property, such as garages, silos, privies, pools, gravesites. Identify any known exterior and interior alterations such as additions, replacement windows, aluminum or vinyl siding or changes in plan. Include dates of construction and alteration, if known. Attach additional sheets as needed.

Built in several stages and currently evoking the appearance of a salt-box-type dwelling, the house at 830 Morrow Street stands on the south side of Morrow east of its intersection with Elizabeth Grove Road. The surrounding neighborhood consists primarily of mixed residential and commercial structures. Morrow Street now terminates a short distance east of the subject house in an area of second growth timber. The house features a two-story gable-roofed central section with a shallowly sloped north slope and a steeply sloped rear, south roof line. A full-width shed-roof enclosed porch occupies the north elevation's first story. The porch displays rock-faced concrete block piers and posts with concrete block infill between the older, rock-faced vertical elements. The rear, south elevation also features a full-width one-story addition with a shed roof. A half-width shed-roof porch supported by wood posts is appended to the southeast side of the rear elevation. A small one-story gable roofed structure protrudes from the building west elevation. The house possesses vinyl siding, asphalt roof shingles, and an interior brick chimney piercing the rear, south slope of the main roof. Most window openings contain one-over-one aluminum replacement sash. Three evenly spaced bays penetrate the north elevation's second story. A small one-light aluminum casement window occupies the west elevation. A vinyl-clad slab door provides entry into the house through the enclosed porch.

**Narrative Description of Significance:** Briefly describe those characteristics by which this property may be considered historically significant. Significance may include, but is not limited to, a structure being an intact representative of an architectural or engineering type or style (e.g., Gothic Revival style cottage, Pratt through-truss bridge); association with historic events or broad patterns of local, state or national history (e.g., a cotton mill from a period of growth in local industry, a seaside cottage representing a locale's history as a resort community, a structure associated with activities of the "underground railroad."); or by association with persons or organizations significant at a local, state or national level. Simply put, why is this property important to you and the community. Attach additional sheets as needed.

New York building data indicates that construction of the house at 830 Morrow Street occurred in 1920, a date verified by the building's owner. The building may have originally reflected the bungalow architectural style of the early twentieth century. An indentation in the vinyl siding of the west elevation follows the roof line of the porch to the peak of the main ridge, evidence that the second story may actually be a full-width dormer either original to or an addition to the original building. The main roof slope then shaded a cut-away porch, a feature common to bungalowoid architecture. Bungalowoid buildings also often featured prominent roof dormers. A 1917 atlas map of the area does not depict a building at this address. The removal of nearly all historic fabric has left the building with little architectural integrity from the period of its construction. The building is not a noteworthy example of a particular architectural style or method of construction (National Register Criterion C). Research has not identified any significant relationships between the building and historic events, trends, broad patterns, or individuals (Criteria A and B). The house is unlikely to provide information important to our understanding of history (Criterion D). The house at 830 Morrow Street is therefore recommended as not eligible for listing in the National Register.



830 Morrow Street, Staten Island

Source: <http://gis.nyc.gov/doitt/mp/Portal.do>

Scale not provided.



North and West Elevations



South and East Elevations

# HISTORIC RESOURCE INVENTORY FORM



NYS OFFICE OF PARKS, RECREATION  
& HISTORIC PRESERVATION  
P.O. BOX 189, WATERFORD, NY 12188  
(518) 237-8643

OFFICE USE ONLY

USN:

## IDENTIFICATION

Property name(if any) \_\_\_\_\_  
Address or Street Location 851 Morrow Street (Block 1380 Lot 41)  
County Richmond Town/City Staten Island Village/Hamlet: \_\_\_\_\_  
Owner Gerard Kirby Anderson Address 851 Morrow Street, Staten Island, NY 10303  
Original use Dwelling Current use Dwelling  
Architect/Builder, if known Unknown Date of construction, if known ca. 1915

## DESCRIPTION

Materials -- please check those materials that are visible

Exterior Walls:	<input type="checkbox"/> wood clapboard	<input type="checkbox"/> wood shingle	<input type="checkbox"/> vertical boards	<input type="checkbox"/> plywood	
	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input type="checkbox"/> poured concrete	<input type="checkbox"/> concrete block	
	<input type="checkbox"/> vinyl siding	<input checked="" type="checkbox"/> aluminum siding	<input type="checkbox"/> cement-asbestos	<input type="checkbox"/> other: _____	
Roof:	<input checked="" type="checkbox"/> asphalt, shingle	<input type="checkbox"/> asphalt, roll	<input type="checkbox"/> wood shingle	<input type="checkbox"/> metal	<input type="checkbox"/> slate
Foundation:	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input type="checkbox"/> poured concrete	<input type="checkbox"/> concrete block	

Other materials and their location: stuccoed foundation; aluminum cornice; stuccoed chimney pile; wood and aluminum sash

Alterations, if known: one-story glass enclosed porch/deck on east; two-story addition northeast Date: Unknown

Condition:  excellent  good  fair  deteriorated

## Photos

Provide several clear, original photographs of the property proposed for nomination. Submitted views should represent the property as a whole. For buildings or structures, this includes exterior and interior views, general setting, outbuildings and landscape features. Color prints are acceptable for initial submissions.

Please staple one photograph providing a complete view of the structure or property to the front of this sheet. Additional views should be submitted in a separate envelope or stapled to a continuation sheet.

## Maps

Attach a printed or drawn locational map indicating the location of the property in relationship to streets, intersections or other widely recognized features so that the property can be accurately positioned. Show a north arrow. Include a scale or estimate distances where possible.

Prepared by: Stuart Paul Dixon address The Louis Berger Group, Inc., 120 Halsted Street, East Orange, NJ 07018

Telephone: 973-678-1960 email sdixon@louisberger.com Date March 2007

**PLEASE PROVIDE THE FOLLOWING INFORMATION**

**IF YOU ARE PREPARING A NATIONAL REGISTER NOMINATION, PLEASE REFER TO THE ATTACHED INSTRUCTIONS**

**Narrative Description of Property:** Briefly describe the property and its setting. Include a verbal description of the location (e.g., north side of NY 17, west of Jones Road); a general description of the building, structure or feature including such items as architectural style (if known), number of stories, type and shape of roof (flat, gabled, mansard, shed or other), materials and landscape features. Identify and describe any associated buildings, structures or features on the property, such as garages, silos, privies, pools, gravesites. Identify any known exterior and interior alterations such as additions, replacement windows, aluminum or vinyl siding or changes in plan. Include dates of construction and alteration, if known. Attach additional sheets as needed.

The house at 851 Morrow Street is located on the northeast corner of Morrow Street's intersection with Elizabeth Grove Road. The surrounding neighborhood consists of mixed residential and commercial buildings. The subject house stands two stories tall with an asphalt-shingled gable-front roof, aluminum siding and cornice, and a stuccoed foundation. The two-bay-wide by two-bay-deep building also possesses a stuccoed chimney piercing the center of the structure's ridge. A full-width shed-roof porch on the building's main, south elevation facing Morrow Street has been enclosed and its roof extended around the southwest corner of the west elevation one bay in order to shade a new entrance and stoop penetrating the west elevation. The entrance features a brick and stone stoop and decorative metal posts. The porch displays a large Chicago-style window composed of a large fixed light flanked by one-over-one wood sash in its south elevation. Most other windows opening throughout the building contain one-over-one aluminum replacement sash. Small one-over-one wood sash occupy the gable peaks. The west elevation also contains paired three-light metal casement windows on its first story. A two-story, one-bay-wide by two-bays-deep ell is appended to the main structure's northeast corner. The building's east elevation has a one-story, glass-enclosed shed roof porch.

**Narrative Description of Significance:** Briefly describe those characteristics by which this property may be considered historically significant. Significance may include, but is not limited to, a structure being an intact representative of an architectural or engineering type or style (e.g., Gothic Revival style cottage, Pratt through-truss bridge); association with historic events or broad patterns of local, state or national history (e.g., a cotton mill from a period of growth in local industry, a seaside cottage representing a locale's history as a resort community, a structure associated with activities of the "underground railroad."); or by association with persons or organizations significant at a local, state or national level. Simply put, why is this property important to you and the community. Attach additional sheets as needed.

New York building data indicates that construction of the house at 851 Morrow Street occurred in 1915. The building's gable-front form reflects late nineteenth- or early twentieth-century construction. An 1898 United States Geological Survey map of Staten Island may depict a building at or near the site of the present structure at 851 Morrow Street. A 1917 atlas map of the area does depict a two-story frame building at this address. However, the appending of the rear and east additions and the replacement of much of its historic fabric has left the building with little architectural integrity from the period of its construction. The building is therefore not a noteworthy example of a particular architectural style or method of construction (National Register Criterion C). Research has not identified any significant relationships between the building and historic events, trends, broad patterns, or individuals (Criteria A and B). The house is unlikely to provide information important to our understanding of history (Criterion D). The house at 851 Morrow Street is therefore recommended as not eligible for listing in the National Register.



851 Morrow Street, Staten Island

Source: <http://gis.nyc.gov/doitt/mp/Portal.do>

Scale not provided.



South and East Elevations



North and West Elevations

# HISTORIC RESOURCE INVENTORY FORM



NYS OFFICE OF PARKS, RECREATION  
& HISTORIC PRESERVATION  
P.O. BOX 189, WATERFORD, NY 12188  
(518) 237-8643

OFFICE USE ONLY

USN:

## IDENTIFICATION

Property name(if any) \_\_\_\_\_  
Address or Street Location 2431 Forest Avenue (Block 1348 Lot 102)  
County Richmond Town/City Staten Island Village/Hamlet: \_\_\_\_\_  
Owner Ahmed Aziz Address 2431 Forest Avenue, Staten Island, NY 10303  
Original use Dwelling Current use Dwelling  
Architect/Builder, if known Unknown Date of construction, if known ca. 1915

## DESCRIPTION

Materials -- please check those materials that are visible

Exterior Walls:	<input type="checkbox"/> wood clapboard	<input type="checkbox"/> wood shingle	<input type="checkbox"/> vertical boards	<input type="checkbox"/> plywood	
	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input type="checkbox"/> poured concrete	<input type="checkbox"/> concrete block	
	<input type="checkbox"/> vinyl siding	<input type="checkbox"/> aluminum siding	<input type="checkbox"/> cement-asbestos	<input checked="" type="checkbox"/> other: <u>stucco</u>	
Roof:	<input checked="" type="checkbox"/> asphalt, shingle	<input type="checkbox"/> asphalt, roll	<input type="checkbox"/> wood shingle	<input type="checkbox"/> metal	<input type="checkbox"/> slate
Foundation:	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input type="checkbox"/> poured concrete	<input type="checkbox"/> concrete block	

Other materials and their location: stuccoed foundation; aluminum cornice; stuccoed chimney; wood and aluminum sash

Alterations, if known: one-story screen addition on east Date: Unknown

Condition:  excellent  good  fair  deteriorated

## Photos

Provide several clear, original photographs of the property proposed for nomination. Submitted views should represent the property as a whole. For buildings or structures, this includes exterior and interior views, general setting, outbuildings and landscape features. Color prints are acceptable for initial submissions.

Please staple one photograph providing a complete view of the structure or property to the front of this sheet. Additional views should be submitted in a separate envelope or stapled to a continuation sheet.

## Maps

Attach a printed or drawn locational map indicating the location of the property in relationship to streets, intersections or other widely recognized features so that the property can be accurately positioned. Show a north arrow. Include a scale or estimate distances where possible.

Prepared by: Stuart Paul Dixon address The Louis Berger Group, Inc., 120 Halsted Street, East Orange, NJ 07018

Telephone: 973-678-1960 email sdixon@louisberger.com Date March 2007

**PLEASE PROVIDE THE FOLLOWING INFORMATION**

IF YOU ARE PREPARING A NATIONAL REGISTER NOMINATION, PLEASE REFER TO THE ATTACHED INSTRUCTIONS

**Narrative Description of Property:** Briefly describe the property and its setting. Include a verbal description of the location (e.g., north side of NY 17, west of Jones Road); a general description of the building, structure or feature including such items as architectural style (if known), number of stories, type and shape of roof (flat, gabled, mansard, shed or other), materials and landscape features. Identify and describe any associated buildings, structures or features on the property, such as garages, silos, privies, pools, gravesites. Identify any known exterior and interior alterations such as additions, replacement windows, aluminum or vinyl siding or changes in plan. Include dates of construction and alteration, if known. Attach additional sheets as needed.

The house at 2431 Forest Avenue stands two and one-half stories tall with an asphalt-shingled hipped roof, stuccoed walls and foundation, and an exterior stuccoed chimney pile along its northwest corner. The three-bay-wide by two-bay deep house, located along the north side of Forest Avenue east of its intersection with Morrow Street, features a full-width one-bay-deep one-story hipped roof enclosed porch on its south elevation facing Forest Avenue, and a full-width one-story gable-roofed wing extending four bays to the north. The porch possesses a central vinyl-covered slab door flanked by one-over-one wood sash on its south elevation and paired one-over-one wood sash on its side elevations. The main structure's symmetrical, south elevation second story features one-over-one wood sash as well ornamented by poured concrete lintels and sills. The centrally placed hipped roof dormer contains a smaller one-over-one aluminum sash unit. The west elevation's fenestration consists of unevenly spaced windows on the first and second story and an at-grade wood slab door providing entry to the basement level. A half-sized window on the first story's south end contains a multi-paned fixed bungalow-style window with smaller colored lights surrounding a larger clear light. The rear wing's west elevation features a metal storm door flanked by a single and paired one-over-one aluminum sash. The main structure's and the wing's evenly spaced east elevation windows contain aluminum sash.

To the rear of the house on the north portion of the property stands a one-story metal Quonset building. The area surrounding the house consists of mixed residential and commercial buildings interspersed with overgrown vegetation. A short distance west of the house is a modern "big box"-type retail establishment.

**Narrative Description of Significance:** Briefly describe those characteristics by which this property may be considered historically significant. Significance may include, but is not limited to, a structure being an intact representative of an architectural or engineering type or style (e.g., Gothic Revival style cottage, Pratt through-truss bridge); association with historic events or broad patterns of local, state or national history (e.g., a cotton mill from a period of growth in local industry, a seaside cottage representing a locale's history as a resort community, a structure associated with activities of the "underground railroad."); or by association with persons or organizations significant at a local, state or national level. Simply put, why is this property important to you and the community. Attach additional sheets as needed.

New York building data indicates that construction of the house at 2431 Forest Avenue occurred in 1915. The main two-and-one-half-story portion of the building reflects Four-square architectural style, a form popular during the early twentieth century. An 1898 United States Geological Survey map of Staten Island may depict a building at or near the site of the present structure at 2431 Forest Avenue. However, the building is an example of a common building type possessing little architectural integrity from the period of its construction. The building is not a noteworthy example of a particular architectural style or method of construction (National Register Criterion C). Research has not identified any significant relationships between the building and historic events, trends, broad patterns, or individuals (Criteria A and B). The house is unlikely to provide information important to our understanding of history (Criterion D). The house at 2431 Forest Avenue is therefore recommended as not eligible for listing in the National Register.



2431 Forest Avenue, Staten Island

Source: <http://gis.nyc.gov/doitt/mp/Portal.do>

Scale not provided.



West and South Elevations



South and East Elevations

# HISTORIC RESOURCE INVENTORY FORM



NYS OFFICE OF PARKS, RECREATION  
& HISTORIC PRESERVATION  
P.O. BOX 189, WATERFORD, NY 12188  
(518) 237-8643

OFFICE USE ONLY

USN:

## IDENTIFICATION

Property name(if any) \_\_\_\_\_

Address or Street Location 2409 Forest Avenue (Block 1290 Lot 15)

County Richmond Town/City Staten Island Village/Hamlet: \_\_\_\_\_

Owner Biaggio J. Severino Address 2409 Forest Avenue, Staten Island, NY 10303

Original use Unknown Current use Office

Architect/Builder, if known Unknown Date of construction, if known ca. 1931

## DESCRIPTION

Materials -- please check those materials that are visible

Exterior Walls:	<input type="checkbox"/> wood clapboard	<input type="checkbox"/> wood shingle	<input checked="" type="checkbox"/> vertical boards	<input type="checkbox"/> plywood	
	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input type="checkbox"/> poured concrete	<input type="checkbox"/> concrete block	
	<input type="checkbox"/> vinyl siding	<input type="checkbox"/> aluminum siding	<input type="checkbox"/> cement-asbestos	<input type="checkbox"/> other: _____	
Roof:	<input checked="" type="checkbox"/> asphalt, shingle	<input type="checkbox"/> asphalt, roll	<input type="checkbox"/> wood shingle	<input type="checkbox"/> metal	<input type="checkbox"/> slate
Foundation:	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input type="checkbox"/> poured concrete	<input type="checkbox"/> concrete block	

Other materials and their location: wood sash; corbelled brick chimney

Alterations, if known: \_\_\_\_\_ Date: \_\_\_\_\_

Condition:  excellent  good  fair  deteriorated

## Photos

Provide several clear, original photographs of the property proposed for nomination. Submitted views should represent the property as a whole. For buildings or structures, this includes exterior and interior views, general setting, outbuildings and landscape features. Color prints are acceptable for initial submissions.

Please staple one photograph providing a complete view of the structure or property to the front of this sheet. Additional views should be submitted in a separate envelope or stapled to a continuation sheet.

## Maps

Attach a printed or drawn locational map indicating the location of the property in relationship to streets, intersections or other widely recognized features so that the property can be accurately positioned. Show a north arrow. Include a scale or estimate distances where possible.

Prepared by: Stuart Paul Dixon address The Louis Berger Group, Inc., 120 Halsted Street, East Orange, NJ 07018

Telephone: 973-678-1960 email sdixon@louisberger.com Date March 2007

**PLEASE PROVIDE THE FOLLOWING INFORMATION**

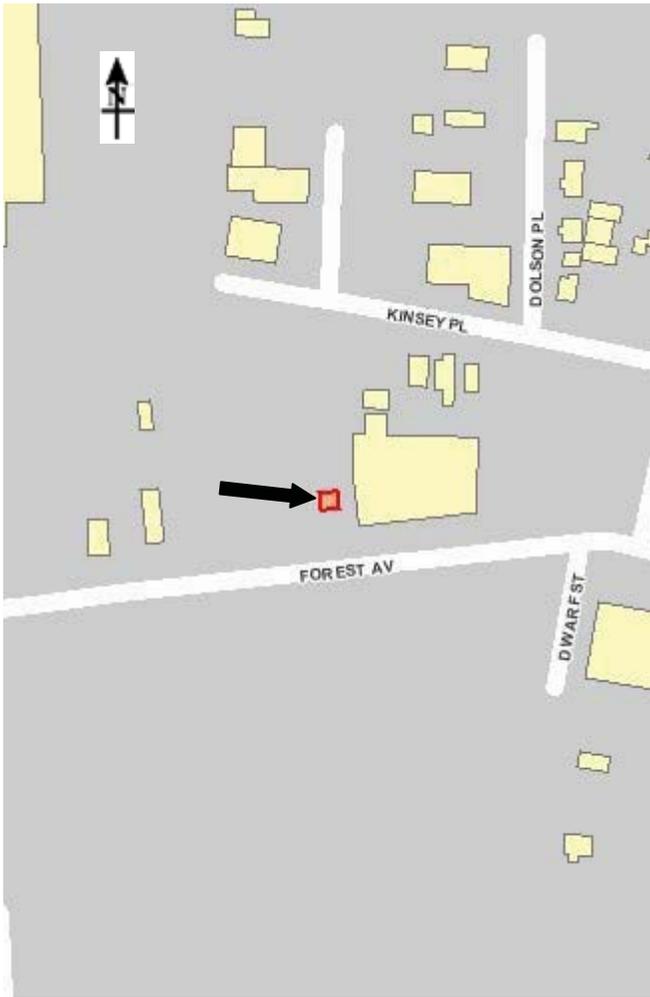
IF YOU ARE PREPARING A NATIONAL REGISTER NOMINATION, PLEASE REFER TO THE ATTACHED INSTRUCTIONS

**Narrative Description of Property:** Briefly describe the property and its setting. Include a verbal description of the location (e.g., north side of NY 17, west of Jones Road); a general description of the building, structure or feature including such items as architectural style (if known), number of stories, type and shape of roof (flat, gabled, mansard, shed or other), materials and landscape features. Identify and describe any associated buildings, structures or features on the property, such as garages, silos, privies, pools, gravesites. Identify any known exterior and interior alterations such as additions, replacement windows, aluminum or vinyl siding or changes in plan. Include dates of construction and alteration, if known. Attach additional sheets as needed.

The one-story building at 2409 Forest Avenue stands amidst a dirt and gravel lot on the north side of Forest Avenue west of its intersection with Northfield Avenue. The surrounding neighborhood consists primarily of mixed residential and commercial structures. The building at 2409 Forest consists of a one-story hipped-roof building, one bay wide by two bays deep and rectangular in plan, with asphalt roof shingles, vertical plank siding, a stuccoed foundation, and a boxed cornice. An exterior corbelled brick chimney pile penetrates the building's eaves along its northwest corner. A six-panel vinyl door occupies the building's sole south elevation fenestral opening. The west elevation features two one-over-one wood sash with artificial six-over-six inserts.

**Narrative Description of Significance:** Briefly describe those characteristics by which this property may be considered historically significant. Significance may include, but is not limited to, a structure being an intact representative of an architectural or engineering type or style (e.g., Gothic Revival style cottage, Pratt through-truss bridge); association with historic events or broad patterns of local, state or national history (e.g., a cotton mill from a period of growth in local industry, a seaside cottage representing a locale's history as a resort community, a structure associated with activities of the "underground railroad."); or by association with persons or organizations significant at a local, state or national level. Simply put, why is this property important to you and the community. Attach additional sheets as needed.

New York building data indicates that construction of the building at 2409 Forest Avenue occurred in 1931. The building reflects utilitarian construction of the early twentieth century and possesses little architectural integrity from the period of its construction. The building is not a noteworthy example of a particular architectural style or method of construction (National Register Criterion C). Research has not identified any significant relationships between the building and historic events, trends, broad patterns, or individuals (Criteria A and B). The building is unlikely to provide information important to our understanding of history (Criterion D). The building at 2409 Forest Avenue is therefore recommended as not eligible for listing in the National Register.



2409 Forest Avenue, Staten Island

Source: <http://gis.nyc.gov/doitt/mp/Portal.do>

Scale not provided.

2409 Forest Avenue, Staten Island



West and South Elevations

# HISTORIC RESOURCE INVENTORY FORM



NYS OFFICE OF PARKS, RECREATION  
& HISTORIC PRESERVATION  
P.O. BOX 189, WATERFORD, NY 12188  
(518) 237-8643

OFFICE USE ONLY

USN:

## IDENTIFICATION

Property name(if any) \_\_\_\_\_

Address or Street Location 15 Kinsey Place (Block 1290 Lot 66)

County Richmond Town/City Staten Island Village/Hamlet: \_\_\_\_\_

Owner Gene Panaro Address 15 Kinsey Place, Staten Island, NY 10303

Original use Dwelling Current use Dwelling

Architect/Builder, if known Unknown Date of construction, if known ca. 1915

## DESCRIPTION

Materials -- please check those materials that are visible

Exterior Walls:	<input type="checkbox"/> wood clapboard	<input type="checkbox"/> wood shingle	<input type="checkbox"/> vertical boards	<input type="checkbox"/> plywood	
	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input type="checkbox"/> poured concrete	<input type="checkbox"/> concrete block	
	<input checked="" type="checkbox"/> vinyl siding	<input type="checkbox"/> aluminum siding	<input type="checkbox"/> cement-asbestos	<input type="checkbox"/> other: _____	
Roof:	<input checked="" type="checkbox"/> asphalt, shingle	<input type="checkbox"/> asphalt, roll	<input type="checkbox"/> wood shingle	<input type="checkbox"/> metal	<input type="checkbox"/> slate
Foundation:	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input type="checkbox"/> poured concrete	<input checked="" type="checkbox"/> concrete block	

Other materials and their location: vinyl sash

Alterations, if known: \_\_\_\_\_ Date: \_\_\_\_\_

Condition:  excellent  good  fair  deteriorated

## Photos

Provide several clear, original photographs of the property proposed for nomination. Submitted views should represent the property as a whole. For buildings or structures, this includes exterior and interior views, general setting, outbuildings and landscape features. Color prints are acceptable for initial submissions.

Please staple one photograph providing a complete view of the structure or property to the front of this sheet. Additional views should be submitted in a separate envelope or stapled to a continuation sheet.

## Maps

Attach a printed or drawn locational map indicating the location of the property in relationship to streets, intersections or other widely recognized features so that the property can be accurately positioned. Show a north arrow. Include a scale or estimate distances where possible.

Prepared by: Stuart Paul Dixon address The Louis Berger Group, Inc., 120 Halsted Street, East Orange, NJ 07018

Telephone: 973-678-1960 email sdixon@louisberger.com Date March 2007

**PLEASE PROVIDE THE FOLLOWING INFORMATION**

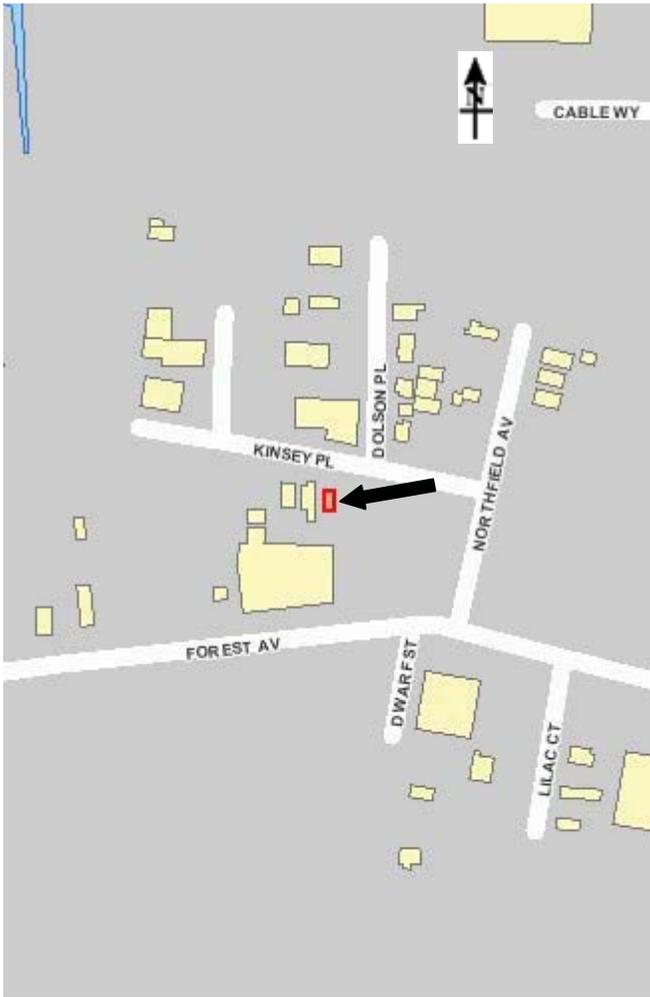
**IF YOU ARE PREPARING A NATIONAL REGISTER NOMINATION, PLEASE REFER TO THE ATTACHED INSTRUCTIONS**

**Narrative Description of Property:** Briefly describe the property and its setting. Include a verbal description of the location (e.g., north side of NY 17, west of Jones Road); a general description of the building, structure or feature including such items as architectural style (if known), number of stories, type and shape of roof (flat, gabled, mansard, shed or other), materials and landscape features. Identify and describe any associated buildings, structures or features on the property, such as garages, silos, privies, pools, gravesites. Identify any known exterior and interior alterations such as additions, replacement windows, aluminum or vinyl siding or changes in plan. Include dates of construction and alteration, if known. Attach additional sheets as needed.

The house at 15 Kinsey Place stands along the south side of Kinsey west of its intersection with Dolson Place. The house is surrounded by a primarily mixed residential and commercial neighborhood. A metal Quonset-type building stands on the southern, rear portion of the lot. The building is one story tall with an asphalt-shingled side-gable roof, vinyl siding, and a rusticated concrete block foundation. A full-width shed-roof addition on the building's rear south elevation gives the structure's roof line a salt-box appearance. The three-bay-wide by two-bay-deep building features a vinyl door with upper fanlight off-center on its north elevation and one-over-one vinyl replacement sash in most of its window openings. The east gable peak is clipped.

**Narrative Description of Significance:** Briefly describe those characteristics by which this property may be considered historically significant. Significance may include, but is not limited to, a structure being an intact representative of an architectural or engineering type or style (e.g., Gothic Revival style cottage, Pratt through-truss bridge); association with historic events or broad patterns of local, state or national history (e.g., a cotton mill from a period of growth in local industry, a seaside cottage representing a locale's history as a resort community, a structure associated with activities of the "underground railroad."); or by association with persons or organizations significant at a local, state or national level. Simply put, why is this property important to you and the community. Attach additional sheets as needed.

New York building data indicates that construction of the house at 15 Kinsey Place occurred in 1915. The building's side-gable form is consistent with early twentieth-century construction; however, the house is an example of a common building type possessing little architectural integrity from the period of its construction. The building is not a noteworthy example of a particular architectural style or method of construction (National Register Criterion C). Research has not identified any significant relationships between the building and historic events, trends, broad patterns, or individuals (Criteria A and B). The house is unlikely to provide information important to our understanding of history (Criterion D). The house at 15 Kinsey Place is therefore recommended as not eligible for listing in the National Register.



15 Kinsey Place, Staten Island

Source: <http://gis.nyc.gov/doitt/mp/Portal.do>

Scale not provided.



North Elevation



North and West Elevations

# HISTORIC RESOURCE INVENTORY FORM



NYS OFFICE OF PARKS, RECREATION  
& HISTORIC PRESERVATION  
P.O. BOX 189, WATERFORD, NY 12188  
(518) 237-8643

OFFICE USE ONLY

USN:

## IDENTIFICATION

Property name(if any) \_\_\_\_\_  
Address or Street Location 17 Kinsey Place (Block 1290 Lot 64)  
County Richmond Town/City Staten Island Village/Hamlet: \_\_\_\_\_  
Owner Donna M. Pagano Address 17 Kinsey Place, Staten Island, NY 10303  
Original use Dwelling Current use Dwelling  
Architect/Builder, if known Unknown Date of construction, if known ca. 1920

## DESCRIPTION

Materials -- please check those materials that are visible

Exterior Walls:	<input type="checkbox"/> wood clapboard	<input type="checkbox"/> wood shingle	<input type="checkbox"/> vertical boards	<input type="checkbox"/> plywood	
	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input type="checkbox"/> poured concrete	<input type="checkbox"/> concrete block	
	<input type="checkbox"/> vinyl siding	<input checked="" type="checkbox"/> aluminum siding	<input type="checkbox"/> cement-asbestos	<input type="checkbox"/> other: _____	
Roof:	<input checked="" type="checkbox"/> asphalt, shingle	<input type="checkbox"/> asphalt, roll	<input type="checkbox"/> wood shingle	<input type="checkbox"/> metal	<input type="checkbox"/> slate
Foundation:	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input type="checkbox"/> poured concrete	<input type="checkbox"/> concrete block	

Other materials and their location: stuccoed foundation; aluminum and wood sash; corbelled brick chimney

Alterations, if known: \_\_\_\_\_ Date: \_\_\_\_\_

Condition:  excellent  good  fair  deteriorated

## Photos

Provide several clear, original photographs of the property proposed for nomination. Submitted views should represent the property as a whole. For buildings or structures, this includes exterior and interior views, general setting, outbuildings and landscape features. Color prints are acceptable for initial submissions.

Please staple one photograph providing a complete view of the structure or property to the front of this sheet. Additional views should be submitted in a separate envelope or stapled to a continuation sheet.

## Maps

Attach a printed or drawn locational map indicating the location of the property in relationship to streets, intersections or other widely recognized features so that the property can be accurately positioned. Show a north arrow. Include a scale or estimate distances where possible.

Prepared by: Stuart Paul Dixon address The Louis Berger Group, Inc., 120 Halsted Street, East Orange, NJ 07018

Telephone: 973-678-1960 email sdixon@louisberger.com Date March 2007

**PLEASE PROVIDE THE FOLLOWING INFORMATION**

**IF YOU ARE PREPARING A NATIONAL REGISTER NOMINATION, PLEASE REFER TO THE ATTACHED INSTRUCTIONS**

**Narrative Description of Property:** Briefly describe the property and its setting. Include a verbal description of the location (e.g., north side of NY 17, west of Jones Road); a general description of the building, structure or feature including such items as architectural style (if known), number of stories, type and shape of roof (flat, gabled, mansard, shed or other), materials and landscape features. Identify and describe any associated buildings, structures or features on the property, such as garages, silos, privies, pools, gravesites. Identify any known exterior and interior alterations such as additions, replacement windows, aluminum or vinyl siding or changes in plan. Include dates of construction and alteration, if known. Attach additional sheets as needed.

The house at 17 Kinsey Place stands along the south side of Kinsey west of its intersection with Dolson Place. The house is primarily surrounded by a mixed residential and commercial neighborhood. The house at 17 Kinsey Place consists of a one-story gable-front structure with a partial-width gable-roofed enclosed porch on its main, north elevation, a one-story gable-roofed wing appended to its southeast corner, and a shed-roof addition attached to the wing's south elevation. The house features asphalt roof shingles, a stuccoed foundation, aluminum siding and box cornice, and one-over-one aluminum replacement sash, some paired, in most window openings. The enclosed porch contains banked aluminum sash on three sides. A modern wood panel door with an upper fanlight occupies the east side of the porch's north elevation. Paired three-light metal casement windows occupy the north gable peak of the main structure. An interior stuccoed chimney penetrates the west roof slope.

**Narrative Description of Significance:** Briefly describe those characteristics by which this property may be considered historically significant. Significance may include, but is not limited to, a structure being an intact representative of an architectural or engineering type or style (e.g., Gothic Revival style cottage, Pratt through-truss bridge); association with historic events or broad patterns of local, state or national history (e.g., a cotton mill from a period of growth in local industry, a seaside cottage representing a locale's history as a resort community, a structure associated with activities of the "underground railroad."); or by association with persons or organizations significant at a local, state or national level. Simply put, why is this property important to you and the community. Attach additional sheets as needed.

New York building data indicates that construction of the house at 17 Kinsey Place occurred in 1920. The building's gable-front form is consistent with early twentieth-century residential construction; however, the house is an example of a common building type possessing little architectural integrity from the period of its construction. The building is not a noteworthy example of a particular architectural style or method of construction (National Register Criterion C). Research has not identified any significant relationships between the building and historic events, trends, broad patterns, or individuals (Criteria A and B). The house is unlikely to provide information important to our understanding of history (Criterion D). The house at 17 Kinsey Place is therefore recommended as not eligible for listing in the National Register.



17 Kinsey Place, Staten Island

Source: <http://gis.nyc.gov/doitt/mp/Portal.do>

Scale not provided.



East and North Elevations



West and North Elevations

# HISTORIC RESOURCE INVENTORY FORM



NYS OFFICE OF PARKS, RECREATION  
& HISTORIC PRESERVATION  
P.O. BOX 189, WATERFORD, NY 12188  
(518) 237-8643

OFFICE USE ONLY

USN:

## IDENTIFICATION

Property name(if any) \_\_\_\_\_

Address or Street Location 17 Kinsey Place (Block 1290 Lot 64)

County Richmond Town/City Staten Island Village/Hamlet: \_\_\_\_\_

Owner Donna M. Pagano Address 17 Kinsey Place, Staten Island, NY 10303

Original use Dwelling Current use Dwelling

Architect/Builder, if known Unknown Date of construction, if known ca. 1920

## DESCRIPTION

Materials -- please check those materials that are visible

Exterior Walls:	<input type="checkbox"/> wood clapboard	<input type="checkbox"/> wood shingle	<input type="checkbox"/> vertical boards	<input type="checkbox"/> plywood	
	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input type="checkbox"/> poured concrete	<input type="checkbox"/> concrete block	
	<input type="checkbox"/> vinyl siding	<input checked="" type="checkbox"/> aluminum siding	<input type="checkbox"/> cement-asbestos	<input type="checkbox"/> other: _____	
Roof:	<input checked="" type="checkbox"/> asphalt, shingle	<input type="checkbox"/> asphalt, roll	<input type="checkbox"/> wood shingle	<input type="checkbox"/> metal	<input type="checkbox"/> slate
Foundation:	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input type="checkbox"/> poured concrete	<input type="checkbox"/> concrete block	

Other materials and their location: stuccoed foundation; aluminum and wood sash; corbelled brick chimney

Alterations, if known: \_\_\_\_\_ Date: \_\_\_\_\_

Condition:  excellent  good  fair  deteriorated

## Photos

Provide several clear, original photographs of the property proposed for nomination. Submitted views should represent the property as a whole. For buildings or structures, this includes exterior and interior views, general setting, outbuildings and landscape features. Color prints are acceptable for initial submissions.

Please staple one photograph providing a complete view of the structure or property to the front of this sheet. Additional views should be submitted in a separate envelope or stapled to a continuation sheet.

## Maps

Attach a printed or drawn locational map indicating the location of the property in relationship to streets, intersections or other widely recognized features so that the property can be accurately positioned. Show a north arrow. Include a scale or estimate distances where possible.

Prepared by: Stuart Paul Dixon address The Louis Berger Group, Inc., 120 Halsted Street, East Orange, NJ 07018

Telephone: 973-678-1960 email sdixon@louisberger.com Date March 2007

**PLEASE PROVIDE THE FOLLOWING INFORMATION**

**IF YOU ARE PREPARING A NATIONAL REGISTER NOMINATION, PLEASE REFER TO THE ATTACHED INSTRUCTIONS**

**Narrative Description of Property:** Briefly describe the property and its setting. Include a verbal description of the location (e.g., north side of NY 17, west of Jones Road); a general description of the building, structure or feature including such items as architectural style (if known), number of stories, type and shape of roof (flat, gabled, mansard, shed or other), materials and landscape features. Identify and describe any associated buildings, structures or features on the property, such as garages, silos, privies, pools, gravesites. Identify any known exterior and interior alterations such as additions, replacement windows, aluminum or vinyl siding or changes in plan. Include dates of construction and alteration, if known. Attach additional sheets as needed.

The house at 17 Kinsey Place stands along the south side of Kinsey west of its intersection with Dolson Place. The house is primarily surrounded by a mixed residential and commercial neighborhood. The house at 17 Kinsey Place consists of a one-story gable-front structure with a partial-width gable-roofed enclosed porch on its main, north elevation, a one-story gable-roofed wing appended to its southeast corner, and a shed-roof addition attached to the wing's south elevation. The house features asphalt roof shingles, a stuccoed foundation, aluminum siding and box cornice, and one-over-one aluminum replacement sash, some paired, in most window openings. The enclosed porch contains banked aluminum sash on three sides. A modern wood panel door with an upper fanlight occupies the east side of the porch's north elevation. Paired three-light metal casement windows occupy the north gable peak of the main structure. An interior stuccoed chimney penetrates the west roof slope.

**Narrative Description of Significance:** Briefly describe those characteristics by which this property may be considered historically significant. Significance may include, but is not limited to, a structure being an intact representative of an architectural or engineering type or style (e.g., Gothic Revival style cottage, Pratt through-truss bridge); association with historic events or broad patterns of local, state or national history (e.g., a cotton mill from a period of growth in local industry, a seaside cottage representing a locale's history as a resort community, a structure associated with activities of the "underground railroad."); or by association with persons or organizations significant at a local, state or national level. Simply put, why is this property important to you and the community. Attach additional sheets as needed.

New York building data indicates that construction of the house at 17 Kinsey Place occurred in 1920. The building's gable-front form is consistent with early twentieth-century residential construction; however, the house is an example of a common building type possessing little architectural integrity from the period of its construction. The building is not a noteworthy example of a particular architectural style or method of construction (National Register Criterion C). Research has not identified any significant relationships between the building and historic events, trends, broad patterns, or individuals (Criteria A and B). The house is unlikely to provide information important to our understanding of history (Criterion D). The house at 17 Kinsey Place is therefore recommended as not eligible for listing in the National Register.



17 Kinsey Place, Staten Island

Source: <http://gis.nyc.gov/doitt/mp/Portal.do>

Scale not provided.



East and North Elevations



West and North Elevations

# HISTORIC RESOURCE INVENTORY FORM



NYS OFFICE OF PARKS, RECREATION  
& HISTORIC PRESERVATION  
P.O. BOX 189, WATERFORD, NY 12188  
(518) 237-8643

OFFICE USE ONLY

USN:

## IDENTIFICATION

Property name(if any) \_\_\_\_\_

Address or Street Location 19 Kinsey Place (Block 1290 Lot 63)

County Richmond Town/City Staten Island Village/Hamlet: \_\_\_\_\_

Owner Helen & Al Sorensen Address 19 Kinsey Place, Staten Island, NY 10303

Original use Dwelling Current use Dwelling

Architect/Builder, if known Unknown Date of construction, if known ca. 1920

## DESCRIPTION

Materials -- please check those materials that are visible

Exterior Walls:	<input type="checkbox"/> wood clapboard	<input type="checkbox"/> wood shingle	<input type="checkbox"/> vertical boards	<input type="checkbox"/> plywood	
	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input type="checkbox"/> poured concrete	<input type="checkbox"/> concrete block	
	<input type="checkbox"/> vinyl siding	<input type="checkbox"/> aluminum siding	<input type="checkbox"/> cement-asbestos	<input checked="" type="checkbox"/> other: <u>vinyl shakes</u>	
Roof:	<input checked="" type="checkbox"/> asphalt, shingle	<input type="checkbox"/> asphalt, roll	<input type="checkbox"/> wood shingle	<input type="checkbox"/> metal	<input type="checkbox"/> slate
Foundation:	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input type="checkbox"/> poured concrete	<input type="checkbox"/> concrete block	

Other materials and their location: stuccoed foundation; wood sash; stuccoes chimney

Alterations, if known: \_\_\_\_\_ Date: \_\_\_\_\_

Condition:  excellent  good  fair  deteriorated

## Photos

Provide several clear, original photographs of the property proposed for nomination. Submitted views should represent the property as a whole. For buildings or structures, this includes exterior and interior views, general setting, outbuildings and landscape features. Color prints are acceptable for initial submissions.

Please staple one photograph providing a complete view of the structure or property to the front of this sheet. Additional views should be submitted in a separate envelope or stapled to a continuation sheet.

## Maps

Attach a printed or drawn locational map indicating the location of the property in relationship to streets, intersections or other widely recognized features so that the property can be accurately positioned. Show a north arrow. Include a scale or estimate distances where possible.

Prepared by: Stuart Paul Dixon address The Louis Berger Group, Inc., 120 Halsted Street, East Orange, NJ 07018

Telephone: 973-678-1960 email sdixon@louisberger.com Date March 2007

**PLEASE PROVIDE THE FOLLOWING INFORMATION**

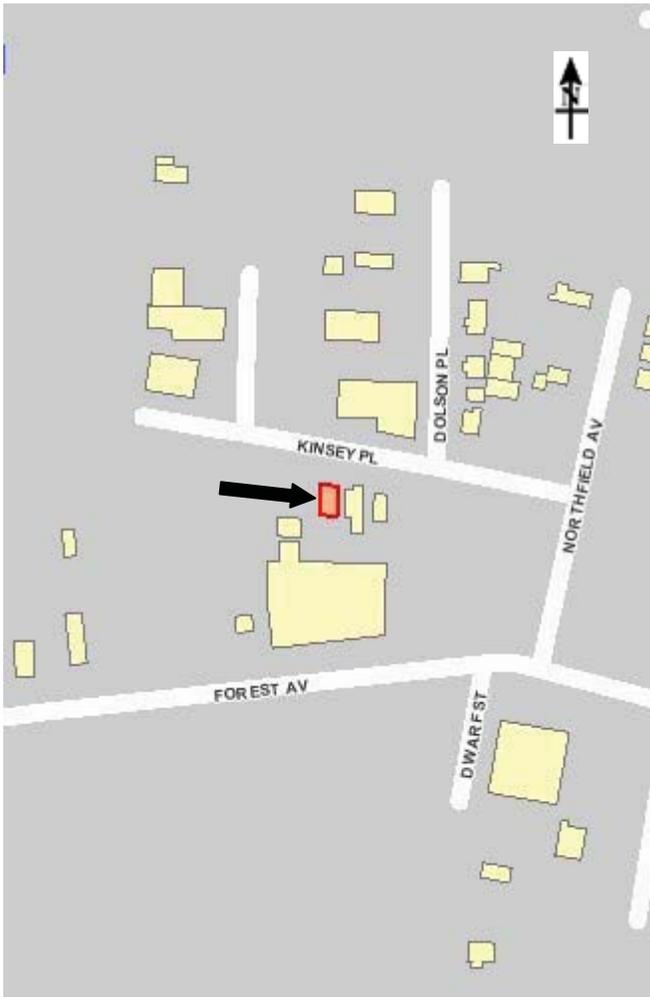
**IF YOU ARE PREPARING A NATIONAL REGISTER NOMINATION, PLEASE REFER TO THE ATTACHED INSTRUCTIONS**

**Narrative Description of Property:** Briefly describe the property and its setting. Include a verbal description of the location (e.g., north side of NY 17, west of Jones Road); a general description of the building, structure or feature including such items as architectural style (if known), number of stories, type and shape of roof (flat, gabled, mansard, shed or other), materials and landscape features. Identify and describe any associated buildings, structures or features on the property, such as garages, silos, privies, pools, gravesites. Identify any known exterior and interior alterations such as additions, replacement windows, aluminum or vinyl siding or changes in plan. Include dates of construction and alteration, if known. Attach additional sheets as needed.

The dwelling at 19 Kinsey Place stands along the south side of Kinsey west of its intersection with Dolson Place. The house is surrounded by a primarily mixed residential and commercial neighborhood. The house stands one and one-half stories tall with an asphalt-shingled hipped roof perpendicular to Kinsey Place, a stuccoed foundation, and vinyl or plastic siding formed to look like wood shakes. The two-bay-wide by four-bays-deep house features paired and single one-over-one-wood sash, a hipped roof dormer centrally placed on its north slope, and a stuccoed chimney protruding from the west roof slope's south side. The main, north elevation possesses a bank of three one-over-one sash on its east side, and a wood slab door with three stepped lights and a single one-over-one sash on its west side.

**Narrative Description of Significance:** Briefly describe those characteristics by which this property may be considered historically significant. Significance may include, but is not limited to, a structure being an intact representative of an architectural or engineering type or style (e.g., Gothic Revival style cottage, Pratt through-truss bridge); association with historic events or broad patterns of local, state or national history (e.g., a cotton mill from a period of growth in local industry, a seaside cottage representing a locale's history as a resort community, a structure associated with activities of the "underground railroad."); or by association with persons or organizations significant at a local, state or national level. Simply put, why is this property important to you and the community. Attach additional sheets as needed.

New York building data indicates that construction of the house at 19 Kinsey Place occurred in 1920. The building's form is consistent with early twentieth-century residential construction; however, the house is an example of a common building type possessing little architectural integrity from the period of its construction. The building is not a noteworthy example of a particular architectural style or method of construction (National Register Criterion C). Research has not identified any significant relationships between the building and historic events, trends, broad patterns, or individuals (Criteria A and B). The house is unlikely to provide information important to our understanding of history (Criterion D). The house at 19 Kinsey Place is therefore recommended as not eligible for listing in the National Register.



19 Kinsey Place, Staten Island

Source: <http://gis.nyc.gov/doitt/mp/Portal.do>

Scale not provided.



East and North Elevations



North and West Elevations

# HISTORIC RESOURCE INVENTORY FORM



NYS OFFICE OF PARKS, RECREATION  
& HISTORIC PRESERVATION  
P.O. BOX 189, WATERFORD, NY 12188  
(518) 237-8643

OFFICE USE ONLY

USN:

## IDENTIFICATION

Property name(if any) \_\_\_\_\_  
Address or Street Location 2949 Goethals Road North (Block 1394 Lot 101)  
County Richmond Town/City Staten Island Village/Hamlet: \_\_\_\_\_  
Owner Texas East Trans CP Address 2949 Goethals Road North, Staten Island, NY 10303  
Original use Pumping Station Current use Pumping Station  
Architect/Builder, if known Unknown Date of construction, if known ca. 1950

## DESCRIPTION

Materials -- please check those materials that are visible

Exterior Walls:	<input type="checkbox"/> wood clapboard	<input type="checkbox"/> wood shingle	<input type="checkbox"/> vertical boards	<input type="checkbox"/> plywood	
	<input type="checkbox"/> stone	<input checked="" type="checkbox"/> brick	<input type="checkbox"/> poured concrete	<input type="checkbox"/> concrete block	
	<input type="checkbox"/> vinyl siding	<input type="checkbox"/> aluminum siding	<input type="checkbox"/> cement-asbestos	<input type="checkbox"/> other: _____	
Roof:	<input type="checkbox"/> asphalt, shingle	<input type="checkbox"/> asphalt, roll	<input type="checkbox"/> wood shingle	<input type="checkbox"/> metal	<input type="checkbox"/> slate
Foundation:	<input type="checkbox"/> stone	<input type="checkbox"/> brick	<input checked="" type="checkbox"/> poured concrete	<input type="checkbox"/> concrete block	

Other materials and their location: poured concrete coping; aluminum sash

Alterations, if known: \_\_\_\_\_ Date: \_\_\_\_\_

Condition:  excellent  good  fair  deteriorated

## Photos

Provide several clear, original photographs of the property proposed for nomination. Submitted views should represent the property as a whole. For buildings or structures, this includes exterior and interior views, general setting, outbuildings and landscape features. Color prints are acceptable for initial submissions.

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## Maps

Attach a printed or drawn locational map indicating the location of the property in relationship to streets, intersections or other widely recognized features so that the property can be accurately positioned. Show a north arrow. Include a scale or estimate distances where possible.

Prepared by: Stuart Paul Dixon address The Louis Berger Group, Inc., 120 Halsted Street, East Orange, NJ 07018

Telephone: 973-678-1960 email sdixon@louisberger.com Date March 2007

**PLEASE PROVIDE THE FOLLOWING INFORMATION**

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The pumping station located at the northeast corner of Goethals Road North and Western Avenue stands amidst an area of dispersed industrial and warehouse structures. The Staten Island Expressway (Interstate 278) parallels the south side of Goethals Road North a short distance south of the road. A tall chain link fence encloses the building and limits fieldworkers' access to the structure. A modern one-story brick building stands immediately north of the subject pumping station. The building consists of a one-story rectangular-plan brick structure with five-to-one common bond walls, a poured concrete foundation, and a gable roof with metal coping. A brick parapet wall penetrates the building's roof toward its western end. The five-bay-wide by three-bay-deep building possesses one-by-one horizontal sliding windows in most fenestral openings. A metal slab door with two wire-lights occupies the east bay of the south elevation and two bays of the north elevation. Smaller window openings penetrate the gable ends. Rowlock sills ornament the most window openings.

**Narrative Description of Significance:** Briefly describe those characteristics by which this property may be considered historically significant. Significance may include, but is not limited to, a structure being an intact representative of an architectural or engineering type or style (e.g., Gothic Revival style cottage, Pratt through-truss bridge); association with historic events or broad patterns of local, state or national history (e.g., a cotton mill from a period of growth in local industry, a seaside cottage representing a locale's history as a resort community, a structure associated with activities of the "underground railroad."); or by association with persons or organizations significant at a local, state or national level. Simply put, why is this property important to you and the community. Attach additional sheets as needed.

New York building data do not provide a construction date for the building at 2949 Goethals Road North, part of a gas transmission facility. Judging from its materials and form, construction of the building probably occurred in circa 1950. The building is utilitarian in form and possesses little architectural integrity from the period of its construction. The building is not a noteworthy example of a particular architectural style or method of construction (National Register Criterion C). Research has not identified any significant relationships between the building and historic events, trends, broad patterns, or individuals (Criteria A and B). The building is unlikely to provide information important to our understanding of history (Criterion D). The building at 2949 Goethals Road North is therefore recommended as not eligible for listing in the National Register.



2949 Goethals Road North, Staten Island

Source: <http://gis.nyc.gov/doitt/mp/Portal.do>

Scale not provided.



North and West Elevations



South and East Elevations

# **APPENDIX BD**

## **RESUMES OF KEY PERSONNEL**

**STUART PAUL DIXON**  
*The Louis Berger Group, Inc.*  
*Senior Architectural Historian*

**EDUCATION**

- M.A., United States History, University of Delaware, 1990.
- Graduate Certificate, Museum Studies, University of Delaware, 1990.
- B.A., History, University of Delaware, 1980.

**TECHNICAL TRAINING**

- Survey and National Register Workshop. Raleigh, North Carolina, 2004. North Carolina Department of Cultural Resources.
- Integrating Section 106 and the National Environmental Policy Act. Washington, D. C., 2004. The SRI Foundation.
- Integrating Cultural Resource and Environmental Compliance. Washington, D.C., 1997. National Preservation Institute.
- The Pennsylvania History Code. Philadelphia, Pennsylvania, 1996. PA Engineers in Practice.
- Cultural Resource Management Plans. Washington, D.C., 1996. National Preservation Institute.
- Advanced Seminar on Preparing Agreement Documents. Washington, D.C., 1994. Advisory Council on Historic Preservation and the University of Nevada, Reno.
- Introduction to Federal Projects and Historic Preservation Law. Philadelphia, Pennsylvania, 1993. Advisory Council on Historic Preservation and the General Services Administration.

**AWARDS**

- **American Cultural Resources Association, Quality Product Award, 2004.** For “Mitigation Projects in Canaan Township, Pennsylvania.”
- **Consulting Engineers Council of New Jersey, Distinguished Award, 2003.** For “U.S. Penitentiary – Canaan Township, Pennsylvania.”

**PROFESSIONAL AFFILIATIONS**

- |  |                                      |
|--|--------------------------------------|
| ■ Historical Society of Delaware           | ■ Society for Commercial Archaeology |
| ■ Historical Society of Pennsylvania       | ■ Society for Industrial Archaeology |
| ■ Maryland Historical Society              | ■ Vernacular Architecture Forum      |
| ■ National Trust for Historic Preservation |                                      |

**PROFESSIONAL EXPERIENCE**

Mr. Dixon has over 19 years of professional experience managing and conducting architectural and historical investigations for a variety of federal, state, and local agencies throughout the mid-Atlantic United States, New York, New Hampshire, Maine, Kentucky, Iowa, Kansas, California, Washington, and Puerto Rico. Primarily undertaken in fulfillment of Sections 106 and 110 of the National Historic Preservation Act of 1966, as amended, and the National Environmental Policy Act of 1969 (NEPA), these projects have entailed fieldwork, research, documentation, and National Register eligibility evaluation of a wide range of historic property types. Mr. Dixon has also prepared HABS/HAER narrative reports and measured drawings and National Register nominations, developed historic preservation and cultural resource management plans, and produced various public-oriented media including popular reports, monographs, museum exhibit boards, historical markers, and educational documentary videos. Mr. Dixon also prepares proposals and provides

QA/QC and staff scheduling for the architectural historian section. His project experience includes the following.

- **Potomac Annex Investigations, Old U.S. Naval Observatory, Washington, D.C.** Principal Investigator for standing structures research and evaluation of National Register-listed historic district and National Historic Landmark property. For the Naval Facilities Engineering Command, Engineering Field Activity - Chesapeake.
- **Congressional Cell Tower Site Section 106 Investigations, Washington, D.C.** Principal Investigator for fieldwork and CO Submission Packet Form 621 preparation for cell tower installation. For Trileaf Corporation.
- **Utility Franchise Research, City of Newport News, Virginia.** Principal Investigator for historic research concerning twentieth-century utility franchises in Newport News and its predecessor municipal agencies. Research undertaken at the Library of Virginia, the Virginia State Records Center, and city agencies. For the City of Newport News, Virginia.
- **Stapleton Waterfront Development Project, Staten Island, New York.** Principal Investigator for New York City Historic Landmarks eligibility study conducted as part of the New York City Environmental Quality Review (CEQR). For the New York City Economic Development Corporation.
- **Centreville Road – Frying Pan Historic Context, Fairfax County, Virginia.** Principal Investigator for historic research and historic context preparation in support of cultural resources investigations. For the Virginia Department of Transportation.
- **Helix House (Building NA-5), Naval Radio Transmitting Facility, Greenbury Point, Annapolis, Maryland.** Principal Investigator for National Register eligibility evaluation. For the United States Naval Academy.
- **Big Slackwater Historic Walls and Towpath Research, Washington County, Maryland.** Principal Investigator for historical research of historic towpath and stone retaining walls comprising part of the Chesapeake and Ohio Canal. Researched archival materials at the National Archives, College Park, Maryland, and the Washington County Historical Society and Library, Hagerstown, Maryland. For the National Park Service, National Capital Region.
- **Route 11 Bypass, Rockbridge County, Virginia.** Principal Investigator for historical research of circa 1875 fish hatchery. Authored historic context and provided National Register eligibility evaluation. For the Virginia Department of Transportation.
- **Independence National Historical Park Security Screening Environmental Assessment, City of Philadelphia, Pennsylvania.** Principal Investigator for assessment of potential impacts to historic structures and cultural landscape resulting from proposed installation of security screening facilities at Independence Mall and Independence Square. For Independence National Historical Park, National Park Service.
- **City College of New York Expansion State Environmental Quality Review Document, Manhattan Borough, New York.** Principal Investigator for National Register eligibility survey for construction of new residence hall. For the Dormitory Authority of the State of New York.

- **Goethals Bridge Replacement Project Environmental Impact Statement, Union County, New Jersey, and Richmond Borough, New York.** Principal Investigator for historic structures National Register evaluation survey and NEPA impacts analysis for replacement of 1920s bridge spanning Arthur Kill. For the Port Authority of New York and New Jersey.
- **Historic Buildings Research and Documentation, Martin Luther King, Jr., Bridge Replacement, City of Petersburg, Virginia.** Principal Investigator for research and documentation of 25 buildings formerly standing within Petersburg Old Town Historic District with views of historic bridge. Researched historical maps and atlases, photographic collections, and insurance surveys. Prepared synopses of buildings' architectural features. For the Virginia Department of Transportation.
- **Harlem Hospital Documentation, New York, New York.** Principal Investigator for documentation and historical research of three hospital buildings. Prepared New York Office of Parks, Recreation and Historic Preservation (SHPO) survey forms. For the Dormitory Authority of the State of New York and the New York City Health and Hospitals Corporation.
- **Villa Manyana Site Evaluation, Niagara Center, Buffalo, New York.** Principal Investigator for building assessment and reuse evaluation. For Acquest Development Company.
- **Mitigation Projects, United States Penitentiary, Canaan Township, Wayne County, Pennsylvania.** Managed implementation of MOA mitigating adverse effects to agricultural complex and former railroad right-of-way determined eligible for the National Register. Directed large-format photographic documentation of agricultural complex, and removal and architectural conservation of railroad markers. Performed field recordation of agricultural complex and prepared HABS Level II architectural recordation report. Directed research and preparation of general-interest histories, historical monographs, museum-quality interpretive panels, roadside markers, and educational documentary videos for both resources. Authored general-interest history and historical monograph of railroad branch line, and text for interpretive panels and roadside markers. Coordinated meetings with federal agencies, SHPO, consulting parties, and public. For the Federal Bureau of Prisons.
- **Interstate Highway System Historic Context Study.** Principal Investigator for development of historic context describing evolution and significance of the Interstate Highway System throughout the United States in order to provide basis for National Register evaluation of identified property types. Prepared comprehensive bibliography. Identified historic chronology and preliminary historic themes based upon literature search. Assisted and coordinated agency symposium with advisory committee. Conducted archival and historical research. Completed context followed National Register's Multiple Property Documentation Form format. For the Federal Highway Administration.
- **King William Reservoir Historic Research, King William County, Virginia.** Principal Investigator for historic research undertaken in support of environmental investigations. Led research team's archival research at state, local, and national repositories to determine historic settlement patterns and the cultural geography of the Pamunkey Neck region of Colonial Virginia during the seventeenth and eighteenth centuries. Created multi-volume annotated historical time line containing copies of gathered primary source material. For Troutman Sanders LLP.
- **Standard Treatment Specifications for Rehabilitation of Historic Exterior Windows, Historic Exterior Masonry, Historic Exterior Wood, and Historic Roofing, Nationwide.** Principal Investigator for research and development of approximately 250 specifications of standard

rehabilitation treatments for national implementation. For the United States Army Medical Research Acquisition Activity and Naval Facilities Engineering Command Environmental Program.

- **Garden State Parkway Interchange 10 Improvements, Cape May County, New Jersey.** Principal Investigator for eligibility study of approximately 30 historic standing structures. For the New Jersey Turnpike Authority.
- **New Jersey Turnpike Expansion, Exits 6-8A, Burlington, Mercer, and Middlesex Counties.** Principal Investigator for National Register eligibility survey of historic structures associated with highway expansion. For the New Jersey Turnpike Authority.
- **Arlington House Rehabilitation Environmental Assessment, Arlington County, Virginia.** Principal Investigator for assessment of potential impacts to historic buildings and cultural landscape resulting from proposed installation of safety and fire suppression systems into antebellum home of Robert E. Lee and related outbuildings. For the National Park Service, National Capital Region.
- **Beulah Road (Route 716) Evaluations, Rockingham County, Virginia.** Principal Investigator for preparation of reconnaissance-level Data Sharing System (DSS) survey forms and eligibility evaluations of nine historic resources. For the Virginia Department of Transportation.
- **Ironbound Road Improvement Project, James City County and City of Williamsburg, Virginia.** Principal Investigator for National Register eligibility evaluation and DSS reconnaissance and intensive level survey form preparation of the Eastern State Hospital. For the Virginia Department of Transportation.
- **Building 101 Photo-documentation, Washington Naval Yard, Washington, D.C.** Principal Investigator for photo-documentation of former architectural drawing and model vaults. For Naval Facilities Engineering Command Washington.
- **U.S. Route 1 Delaware River Bridge Improvement Study, Morrisville, Pennsylvania.** Principal Investigator for documentation and National Register evaluation of 12 resources. For the Delaware River Joint Toll Bridge Commission.
- **Woodloch Intersection Improvement Project, Pike County, Pennsylvania.** Principal Investigator for documentation and National Register evaluation of circa 1830 dwelling. For the Pennsylvania Department of Transportation.
- **North Anna Battlefield Delineation Study, Route 30 State Fair Project, Caroline and Hanover Counties, Virginia.** Principal Investigator for delineation of National Register boundaries for Civil War battlefield. For the Virginia Department of Transportation.
- **Naval Air Warfare Center Aircraft Division Trenton Documentary Video, Mercer County, New Jersey.** Produced, wrote, and directed 30-minute educational documentary of former naval test facility. For Engineering Field Activity Northeast.
- **Arlington Boat House Site Evaluations, Arlington, Virginia.** Principal Investigator for cultural resources evaluations of cultural landscape and historic structures for Environmental Assessment (EA) undertaken for construction of new boat house facilities along Potomac River. For the National Park Service, National Capital Region.

- **National Register of Historic Places Eligibility Study and Cultural Resource Management Plan, Marine Corps Base, Camp Lejeune, Jacksonville, North Carolina.** Principal Investigator for eligibility study of four potential historic districts and 20 individual resources documenting Camp Lejeune's World War II mission. Conducted field and photographic documentation, and historical research. Completed National Register nomination forms for resources recommended as eligible. Prepared Cultural Resource Management Plans for eligible resources. For Camp Lejeune and the Wilmington District, United States Army Corps of Engineers.
- **Skyline Drive Road and Overlooks Rehabilitations, Shenandoah National Park, Virginia.** Principal Investigator for cultural resources evaluations of cultural landscape undertaken for EA undertaken for road improvements in park. For the National Park Service, Denver Service Center.
- **Architectural Survey and Evaluation, Southeast High Speed Rail Corridor, City of Petersburg and Dinwiddie, Brunswick and Mecklenburg Counties, Virginia.** Principal Investigator for Phase II architectural survey and DSS intensive level form preparation for 30 historic resources, and development of National Register boundaries for six Civil War battlefields. Managed team of three architectural historians and coordinated report preparation. For the North Carolina Department of Transportation.
- **Architectural Survey of CBA 2, Route 460 Location Study, Prince George, Sussex, Southampton, Surry, and Isle of Wight Counties, Virginia.** Principal Investigator for reconnaissance level survey form preparation of approximately 100 historic resources along proposed 50-mile highway corridor. Managed project field team of three architectural historians. Coordinated production of two-volume National Register eligibility report. For the Virginia Department of Transportation.
- **Longview Power Plant Cultural Resources Investigations, Monongalia County, West Virginia.** Principal Investigator for National Register eligibility study. Conducted fieldwork, research, survey form documentation, and report preparation. For Marshall Miller and Associates, Inc.
- **Sellers Mill Brochure, Rockingham County, Virginia.** Principal Investigator for historical research and preparation of educational brochure. Conducted research on agriculture and milling trends in Rockingham County, and prepared historic narrative for educational brochure. Managed design and preparation of brochure. For the Virginia Department of Transportation.
- **Phase I and Phase IA Cultural Resource Survey, Proposed Federal Bureau of Prisons Facilities, McCreary County, Kentucky.** Principal Investigator for architectural investigations of four tracts proposed for new prison. Performed field documentation, historical research, and National Register evaluations of 12 individual resources. Coordinated report preparation. For the U.S. Department of Justice, Federal Bureau of Prisons.
- **Executive Order 13287 ("Preserve America") Section 3 Report Preparation.** Researched and documented federal agency compliance with directives of EO 13287. Assisted preparation of report for submittal to the Advisory Council on Historic Preservation. For the Department of Homeland Security, Washington, D.C.
- **Stevens Canyon Road Rehabilitation, Mount Rainier National Park, Washington.** Principal Investigator for cultural resources evaluations of National Historic Landmark's cultural landscape undertaken for EA. For the National Park Service, Denver Service Center.

- **Montrose Chapel Historic Structures Report, Baltimore County, Maryland.** Principal Investigator for documentation of 1840s rural estate chapel. Conducted research and authored historic context portion of report. Coordinated report preparation with historic architect and structural engineer. For the Baltimore District, United States Army Corps of Engineers.
- **Rock Creek and Potomac Parkway and Beach Drive Improvements, Rock Creek Park, Washington, D.C.** Principal Investigator for documentation and analysis of standing structures and cultural landscapes potentially affected by proposed road improvements to National Register-eligible parkway and listed park for EA document. For the National Park Service, National Capital Region.
- **River Road Rehabilitation, Delaware Water Gap National Recreation Area, Pennsylvania.** Principal Investigator for analysis of standing structures and cultural landscapes potentially affected by proposed road improvements to historic roadway and landscape as part of EA. For the National Park Service, Denver Service Center.
- **House Office Buildings Planning Document, Washington, D.C.** Principal Investigator for identification and analysis of above-ground cultural resources for environmental constraints planning document. Conducted research and documented known historic properties limiting proposed improvements and expansions of the House Office Buildings complex. For the Architect of the Capitol.
- **Potomac Annex Windows, Old U.S. Naval Observatory, Washington, D.C.** Principal Investigator for architectural study of historic windows on former observatory building first built in 1840s. Conducted fieldwork and historical research. Authored report outlining results of window analysis and providing recommendations for preservation, rehabilitation, and/or replacement. For the Naval Facilities Engineering Command, Engineering Field Activity - Chesapeake.
- **Housing Units D and E, United States Penitentiary, Lompoc, Santa Barbara County, California.** Principal Investigator for HABS documentation of two residential wings at former U.S. Army Disciplinary Barracks erected in the 1940s. Conducted field recordation, authored narrative report, and managed preparation of HABS report. For the Federal Bureau of Prisons.
- **Route 29 and Gallows Road Intersection, Fairfax County, Virginia.** Principal Investigator for National Register evaluation survey of six historic resources. Conducted research and fieldwork. Prepared six reconnaissance-level DSS forms. For the Virginia Department of Transportation.
- **Route 337 Nansemond Parkway, City of Suffolk, Virginia.** Principal Investigator for National Register evaluation survey of 15 historic resources. Conducted research and fieldwork. Prepared 15 reconnaissance-level DSS forms. For the Virginia Department of Transportation.
- **Integrated Cultural Resource Management Plan, Marine Corps Reserve Center Syracuse, Onondaga County, New York.** Principal Investigator for development of ICRMP for Marine Corps facility. Conducted research and prepared ICRMP. For Engineering Field Activity Northeast.
- **Rock Creek Park Cell Tower Environmental Assessment, Washington, D.C.** Principal Investigator for viewshed effects analysis and evaluation for installation of two cell towers within National Register-listed historic district. Conducted fieldwork and prepared environmental assessment documentation. For the National Park Service, National Capital Region.

- **S.R. 706 Improvement Project, Susquehanna County, Pennsylvania.** Principal Investigator for National Register eligibility study of nearly 100 historic structures for 25-mile corridor study in preparation for highway improvements. Conducted fieldwork, research, documentation, and report preparation. Managed team of three architectural historians and historians. For the Pennsylvania Department of Transportation, Engineering District 4-0.
- **Kempsville Intersection Architectural Investigations, City of Virginia Beach, Virginia.** Principal Investigator for National Register evaluation survey of 14 historic resources. Conducted research and fieldwork. Prepared four intensive-level DSS forms and 10 reconnaissance-level DSS forms. For the Virginia Department of Transportation.
- **Archaeological and Historical Investigations for Improvements to the Intersection of Routes 28 and 97, Montgomery County, Maryland.** Principal Investigator for historic context development for archaeological investigations. Conducted context and site research. Prepared context document. For the Maryland State Highway Administration.
- **Proposed Community Corrections Center, Topeka, Kansas.** Principal Investigator for consultations with the Kansas SHPO concerning proposed half-way house for former inmates. For the Federal Bureau of Prisons.
- **Byberry African-American Cemetery, Philadelphia, Pennsylvania.** Principal Investigator for research of former African-American graveyard. For the Department of Park Planning, Pennsylvania Bureau of State Parks.
- **Environmental Assessment, Proposed Federal Prison Camp, Fort Dix, New Jersey.** Principal Investigator for standing structures component of cultural resources investigations conducted for transfer of United States Army property. For the Federal Bureau of Prisons.
- **Henryton Center Historic and Architectural Resources Survey, Carroll County, Maryland.** Principal Investigator for Maryland Register of Historic Properties eligibility evaluation of African-American tuberculosis sanatorium. Researched and developed historic context for tuberculosis and other public health facilities in Maryland. Prepared Maryland Inventory forms for one historic district and 23 individual resources. For the Maryland Department of Health and Mental Hygiene.
- **Cultural Resources Survey, Marine Corps Reserve Center Syracuse, Onondaga County, New York.** Principal Investigator of National Register eligibility evaluation of two Cold War-era military resources. Conducted fieldwork and research. Prepared survey form documentation. For Northern Division, Naval Facilities Engineering Command.
- **Proposed Federal Correctional Facility, Northumberland County, Pennsylvania.** Principal Investigator for National Register eligibility architectural study and Phase IA archaeological investigation of four prospective sites as part of NEPA alternatives analysis. Directing architectural survey of 70 individual historic resources, six historic districts, and a potential rural historic landscape. Prepared combined architectural and archaeological investigations report. For the Federal Bureau of Prisons.
- **Phase I Architectural Resource Survey, U.S. Highway 30, Harrison County, Iowa.** Principal Investigator for eligibility study of 20 standing structures. Conducted fieldwork, research, documentation, and National Register evaluations. For the Iowa Department of Transportation.

- **Standing Structures Investigations, Bridge 174 Replacement, New Castle County, Delaware.** Principal Investigator for National Register eligibility study. For the Delaware Department of Transportation.
- **Architectural Investigations of Route 47, Glassboro, Gloucester County, New Jersey.** Principal Investigator for eligibility study of 76 standing structures. Conducted fieldwork, research, documentation, evaluations, and report preparation. For the New Jersey Department of Transportation.
- **Proposed United States Penitentiary, Canaan Township, Wayne County, Pennsylvania.** Principal Investigator for National Register eligibility study of 15 historical resources. Conducted fieldwork, research, resource documentation, and National Register evaluations. For the U.S. Department of Justice, Federal Bureau of Prisons.
- **Standing Structures Investigations, Grubb Road, Naamans Road to Marsh Road, New Castle County, Delaware.** Principal Investigator for National Register eligibility study of 23 resources. Managed fieldwork, documentation, site research, and report preparation. For the Delaware Department of Transportation.
- **Naval Hospital Philadelphia, Philadelphia, Pennsylvania.** Principal Investigator for HABS recordation of 50-acre hospital complex containing over 70 buildings involving Narrative Format report for overall complex, and Outline and Short Format reports of nine individual buildings. For the Northern Division, Naval Facilities Engineering Command.
- **Virginia Route 10, Surry County, Improvement Project.** Principal Investigator for architectural investigations and National Register evaluations of eight resources undertaken. Prepared Integrated Preservation Software (IPS) survey forms and management summary. Analyzed potential project effects. For the Virginia Department of Transportation.
- **Western Maryland Railway Historic District National Register of Historic Places Nomination Form.** Principal Investigator for preparation of National Register nomination describing linear historic district and development of regional railroads. Conducted research and field documentation. For the Allegany County Department of Community Services.
- **Hampton Roads Crossing Study, Cities of Chesapeake, Hampton, Newport News, Norfolk, Portsmouth, and Suffolk, Virginia.** Principal Investigator for eligibility evaluation of over 100 resources along three alternatives. Managed fieldwork team, documentation, and National Register assessments. Identified potential effects on National Register-eligible and National Historic Landmark properties. Prepared architectural investigations management summary. Coordinated preparation of combined archaeological and architectural investigations report. For the Virginia Department of Transportation.
- **Maine Historic Building Record Documentations, Naval Air Station Brunswick, Brunswick, Maine.** Principal Investigator for recordation and written report documentation of five buildings related to the air station's World War II and Cold War missions. Reports followed HABS/HAER guidelines. For the Northern Division, Naval Facilities Engineering Command.
- **Environmental Compliance Assessment, Washington Aqueduct, Washington, D.C.** Principal Investigator for review of Aqueduct compliance with federal, state, and local historic preservation

legislation regarding standing structures. Prepared findings and narrative summary. For the Baltimore District, United States Army Corps of Engineers.

- **Environmental Assessment, Naval Surface Warfare Center, Indian Head, Charles County, Maryland.** Principal Investigator for historic structures component of Environmental Assessment for proposed wastewater treatment system renovation. For Engineering Field Activity-Chesapeake and Naval Surface Warfare Center, Indian Head.
- **Architectural Resource Investigations for Elkton Road, New London Road, and Main Street Intersection Improvements, Newark, Delaware.** Principal Investigator for fieldwork documentation, historical research, and report preparation for National Register eligibility evaluation of two potential historic districts and four individual resources. For the Delaware Department of Transportation.
- **DelDOT Archaeology and Architectural Report Series Abstracts, Statewide, Delaware.** Senior Architectural Historian. Abstracted summary information from DelDOT architectural reports for creation of Internet database. For the Delaware Department of Transportation.
- **Archaeological Investigations at Site 44CU40 – Kelly’s Mill, Route 620, Culpeper and Fauquier Counties, Virginia.** Senior Architectural Historian for Phase II archaeological investigation of nineteenth-century mill site. Conducted archival research and authored historic narrative describing evolution of site. For the Virginia Department of Transportation.

## PREVIOUS PROFESSIONAL EXPERIENCE

**Senior Historian, KCI Technologies, Inc., Mechanicsburg and Bensalem, Pennsylvania, 1991-1998.** Managed and directed fieldwork investigations, architectural and historical research, National Register eligibility evaluations, and project effects assessments in accordance with Sections 106 and 110 and NEPA in New York, Pennsylvania, West Virginia, Maryland, and Maine. Prepared National Register eligibility and effects reports. Recommended mitigation measures for adverse project effects. Managed and coordinated field survey teams. Selected projects include the following.

- **S.R. 0041, Section STY, Avondale Transportation Improvement Project, Chester County, Pennsylvania.** Principal Investigator for research, fieldwork, and report preparation for reconnaissance survey of over 700 historic resources for the Pennsylvania Department of Transportation, Engineering District 6-0. Managed research and fieldwork for National Register eligibility study of 120 individual resources and two historic districts. Coordinated and conducted public meeting with local township supervisors, borough council officials, and local historical commission members.
- **U.S. 219 in Oakland Improvement Project, Garrett County, Maryland.** Principal Investigator for National Register eligibility study of 220 historic resources for the Maryland State Highway Administration. Managed fieldwork, research, and report preparation.
- **S.R. 0119 Improvement Project, Homer City to S.R. 0022, Indiana County, Pennsylvania.** Principal Investigator for 12-mile corridor study undertaken for PennDOT Engineering District 10-0. Directed fieldwork, research, and National Register eligibility report preparation for 82 individual historic resources and three historic districts.

- **Cross County Corridor Major Investment Study/Draft Environmental Impact Statement, Bucks, Chester and Montgomery Counties, Pennsylvania.** Principal Investigator for historic resource study of 53-mile rail corridor for the Southeastern Pennsylvania Transportation Authority. Directed documentary research and identification of historic resources.
- **West Virginia Route 10, Man to Logan, Logan County, West Virginia.** Senior Historian. Conducted historical research and architectural fieldwork for 10-mile corridor study for the West Virginia Division of Highways. Authored detailed historic context and completed National Register eligibility evaluations.
- **State Roads 98 and 31 Intersection Improvement Project, Town of Albion, Orleans County, New York.** Senior Historian. Undertook reconnaissance survey and documentation of 120 historic resources for New York State Education Department /New York Department of Transportation project. Evaluated National Register eligibility of one historic district and three individual resources.
- **U.S. 301 South Corridor Transportation Study, Prince George's and Charles Counties, Maryland.** Principal Investigator for historical research and detailed historic context development for 50-mile Maryland State Highway Administration corridor study. Principal author for historic component of combined prehistoric-historic context.
- **Phase I Bridge Survey Statewide, State of Maine.** Senior Historian. Conducted research and co-authored comprehensive historic narrative describing statewide bridge building patterns and transportation trends for the Maine Department of Transportation Office of Environmental Services.
- **Mon-Fayette Transportation Project, Monongalia County, West Virginia, and Fayette County, Pennsylvania.** Principal Investigator for historical and architectural fieldwork, resource documentation, and report preparation for the Pennsylvania Turnpike Commission. Directed determinations of eligibility of 100 individual resources and thematic district, effects evaluation of 23 National Register-eligible resources, and preparation of draft MOA.
- **Spacecraft Magnetic Test Facility Historic Preservation Plan, Goddard Space Flight Center, Prince George's County, Maryland.** Principal Investigator for National Aeronautics and Space Administration (NASA) project developing historic preservation plan implementing provisions of Programmatic Agreement for NASA National Historic Landmarks.
- **Historic Standing Structures Determination of Eligibility Study, Goddard Space Flight Center, Prince George's County, Maryland.** Principal Investigator for National Register eligibility evaluations for NASA. Managed fieldwork, research, documentation, and report preparation.
- **S.R. 0068, Section 350, East Brady Improvement Project, Armstrong and Clarion Counties, Pennsylvania.** Principal Investigator for determination of eligibility study for the Pennsylvania Department of Transportation, Engineering District 10-0. Managed and performed National Register evaluation of historic district, bridge, and two dwellings.
- **Bethlehem Pike/Lafayette Avenue Intersection Project, Montgomery County, Pennsylvania.** Principal Investigator for eligibility study and effects analysis undertaken for the Pennsylvania Department of Transportation, Engineering District 6-0.

- **Glenrose Bridge Replacement Project, Chester County, Pennsylvania.** Principal Investigator for research, fieldwork, National Register evaluations, and report preparation for the Pennsylvania Department of Transportation, Engineering District 6-0.
- **Eldora/South Fairmont Interchange with I-79, Marion County, West Virginia.** Principal Investigator performing fieldwork, research, National Register eligibility evaluations, and report documentation for the West Virginia Division of Highways.
- **S.R. 0074, Sections 008 and 009, York County, Pennsylvania; S.R. 0741, Section 004, Lancaster County, Pennsylvania; and S.R. 4015, Section 002, Franklin County, Pennsylvania.** Principal Investigator for reconnaissance architectural survey, background research, and preliminary cultural resource survey form documentation for 15 intersection improvements for the Pennsylvania Department of Transportation, Engineering District 8-0.
- **I-95 Improvement Project, Prince George's County, Maryland.** Principal Investigator for fieldwork, research, National Register eligibility evaluations, and report preparation for the Maryland State Highway Administration.
- **Cornog Bridge Replacement Project, Chester County, Pennsylvania.** Principal Investigator for eligibility study including fieldwork, research and report preparation, for the Pennsylvania Department of Transportation, Engineering District 6-0.
- **Maryland Statewide Concrete Beam Bridge Analysis.** Principal Investigator for documentation and National Register evaluation study of 130 bridges for the Maryland State Highway Administration.
- **Woodbourne and Langhorne-Yardley Road Intersection, S.R. 2033, Section 004, Bucks County, Pennsylvania.** Principal Investigator for fieldwork, research, and report preparation for National Register eligibility study and effects evaluation for the Pennsylvania Department of Transportation, Engineering District 6-0.
- **S.R. 0072 Traffic Relief Route, Lebanon County, Pennsylvania.** Principal Investigator for National Register eligibility study of one historic district and 29 individual historic resources for the Pennsylvania Department of Transportation, Engineering District 8-0.
- **Newton Hamilton Bridge Replacement Project, Mifflin County, Pennsylvania.** Principal Investigator managing fieldwork, historical research, and National Register evaluations for the Pennsylvania Department of Transportation, Engineering District 2-0.
- **Hessdale Intersection Project, Lancaster County, Pennsylvania.** Senior Historian. Conducted research, fieldwork, and National Register analyses of historic district for the Pennsylvania Department of Transportation, Engineering District 8-0.
- **U.S. 113 Improvement Project, Worcester County, Maryland.** Senior Historian. Undertook reconnaissance survey, detailed fieldwork, and historical research for eligibility study for the Maryland State Highway Administration.

- **Johnsonburg Improvement Project, Elk County, Pennsylvania.** Senior Historian. Conducted architectural survey and historical research for the Pennsylvania Department of Transportation, Engineering District 10-0.
- **S.R. 0219, Section C11 (Brandy Camp), Elk County, Pennsylvania.** Senior Historian. Completed historical research, architectural investigations, and report preparation for the Pennsylvania Department of Transportation, Engineering District 10-0.

**Historic Preservation Consultant, Philadelphia, Pennsylvania, 1988-1992.** Provided preservation services, including historical research, architectural documentation, and National Register eligibility analyses to local governments and preservation agencies. Projects included the following.

- **Waterfront Industrial Zone Intensive Level Survey, City of Wilmington (DE) Office of Planning.** Managed architectural survey, historical research, and National Register of Historic Places eligibility study of 122 industrial resources. Prepared report providing historic context for industrial development of Wilmington, documenting condition of resources, and evaluating National Register eligibility of identified resources.
- **Fishtown Architectural and Archaeological Industrial Survey, Philadelphia (PA) Historical Commission.** Developed methodology for architectural survey and historical research of Philadelphia neighborhood. Directed survey and research efforts that identified 51 historic industrial resources. Prepared National Register eligibility report and Pennsylvania state survey cards.
- **Speedwell Forge Mansion and Stock Farm, draft National Register of Historic Places Nomination, for the Historic Preservation Trust of Lancaster County, Pennsylvania.** Researched and documented architectural fabric and historic background of property associated with eighteenth-century charcoal iron forge and nineteenth-century standardbred horse farm.
- **Historic Mapping Project, Willistown Township (Chester County, PA) Historical Commission.** Researched historic landscape development of township through deed transactions. Drafted pencil drawings of historic subdivision of township property at 25-year intervals between 1700 and 1875. Final product comprised seven ink-on-Mylar maps depicting land tenure within township.
- **Preservation Plan Mapping Project, Lower Merion Township (Montgomery County, Pennsylvania) Planning Commission.** Correlated present township building stock with Historic Preservation Plan historic periods. Color-coded maps identifying historic periods of resources and potential historic landscapes and significant viewsheds for future preservation planning.

**Research Assistant, Center for Historic Architecture and Engineering, University of Delaware, Newark, 1987-1989.** Participated in variety of historic preservation survey and research projects, including the following.

- **Greenbank/Newport Gap Pike Recordation, New Castle County, Delaware.** Supervised fieldwork and research for Section 106 mitigation of three historic properties. Developed historic context and National Register documentation. Measured and produced ink-on-Mylar floor plans for HABS recordation.
- **New Jersey Statewide Comprehensive Historic Preservation Plan.** Researched and developed property type, geographic zone, and historic theme descriptions for model preservation plan.

- **Kent Manor/Long Hook Farm Recordation, New Castle County, Delaware.** Measured and produced plan and section drawings of late seventeenth-century dwelling for HABS documentation.
- **Wilson-Warner Stable Recordation, New Castle County, Delaware.** Measured eighteenth-century stable for HABS documentation.

## PAPERS

- The Honesdale Branch of the Delaware & Hudson Railroad, Successor to its Gravity Railroad. D&H Symposium II, Waymart, Pennsylvania, 2001.
- The Fishtown Architectural and Archaeological Industrial Survey. Co-presenter with Sara Jane Elk and Carmen A. Weber. Preservation Pennsylvania Annual Meeting, Pittsburgh, Pennsylvania, 1989.
- The Development of a City-Wide Industrial Survey: The Fishtown Architectural and Archaeological Industrial Survey, A Case Study. Presented at the Pennsylvania Historical and Museum Commission Conference on State Industrial Surveys, Harrisburg, Pennsylvania, 1989.

## TECHNICAL PUBLICATIONS

- *The Honesdale Branch of the Delaware & Hudson Railroad, Rails through Canaan.* Prepared for the United States Department of Justice, Federal Bureau of Prisons (2004).
- *Links to the Past, The Honesdale Branch of the Delaware and Hudson Railroad.* Prepared for the United States Department of Justice, Federal Bureau of Prisons (2002).
- *The Wilmington Waterfront Analysis Area Intensive Level Architectural Survey.* Prepared for the City of Wilmington (DE) Office of Planning (1992).
- “Fishtown,” *Workshop of the World, The Industrial Archaeology of Philadelphia.* John R. Bowie, editor. Oliver Evans Press (1990).
- *The Fishtown Architectural and Archaeological Industrial Survey.* Prepared for the Philadelphia Historical Commission (1989). Principal author with Sara Jane Elk and Carmen A. Weber.
- *Historic American Buildings Survey: Long Hook.* Center for Historic Architecture and Engineering, University of Delaware. Contributing author with David L. Ames, Bernard L. Herman, Rebecca J. Siders, Hubert F. Jicha III, and Gabrielle M. Lanier.
- *Architectural Assessment of Route 41 (Newport Gap Pike), Rt. 2 (Kirkwood Highway) to Washington Avenue, New Castle County, Delaware.* Prepared for the Delaware Department of Transportation by the Center for Historic Architecture and Engineering, University of Delaware (1988). Principal author with Cheryl C. Powell, Bernard L. Herman, and Rebecca J. Siders.
- *New Jersey Statewide Comprehensive Historic Preservation Plan.* Prepared for the Office of New Jersey Heritage by the Center for Historic Architecture and Engineering, University of Delaware (1987). Contributing author with David Ames, Bernard L. Herman, Rebecca J. Siders, Philip J. Deters, and Cheryl C. Powell.

## ILLUSTRATIONS

- Peters Barn. In *Everyday Architecture of the Mid-Atlantic: Looking at Buildings and Landscapes*, Gabrielle M. Lanier and Bernard L. Herman, The Johns Hopkins University Press, Baltimore, 1997.

#### NATIONAL REGISTER NOMINATIONS

- ***Assault Amphibian Base Historic District, Onslow County, North Carolina (1999)***. Prepared for Marine Corps Base, Camp Lejeune, and the Wilmington District, United States Army Corps of Engineers.
- ***Command Services Historic District, Onslow County, North Carolina (1999)***. Prepared for Marine Corps Base, Camp Lejeune, and the Wilmington District, United States Army Corps of Engineers.
- ***Montford Point Camp No. 1 Historic District, Onslow County, North Carolina (1999)***. Prepared for Marine Corps Base, Camp Lejeune, and the Wilmington District, United States Army Corps of Engineers.
- ***Montford Point Camps Nos. 2 and 2A Historic District, Onslow County, North Carolina (1999)***. Prepared for Marine Corps Base, Camp Lejeune, and the Wilmington District, United States Army Corps of Engineers.
- ***Parachute Training Historic District, Onslow County, North Carolina (1999)***. Prepared for Marine Corps Base, Camp Lejeune, and the Wilmington District, United States Army Corps of Engineers.
- ***Regimental Area No. 3 Historic District, Onslow County, North Carolina (1999)***. Prepared for Marine Corps Base, Camp Lejeune, and the Wilmington District, United States Army Corps of Engineers.
- ***Barrage Balloon Classroom, Onslow County, North Carolina (1999)***. Prepared for Marine Corps Base, Camp Lejeune, and the Wilmington District, United States Army Corps of Engineers.
- ***Naval Hospital, Onslow County, North Carolina (1999)***. Prepared for Marine Corps Base, Camp Lejeune, and the Wilmington District, United States Army Corps of Engineers.
- ***Training Pool Buildings, Onslow County, North Carolina (1999)***. Prepared for Marine Corps Base, Camp Lejeune, and the Wilmington District, United States Army Corps of Engineers.
- ***Western Maryland Railway Connellsville Subdivision Historic District, Allegany County, Maryland (1999)***. Prepared for the Allegany County Department of Community Services, Cumberland, Maryland.
- ***Speedwell Forge Mansion and Stock Farm, Lancaster County, Pennsylvania (1991)***. Prepared for the Historic Preservation Trust of Lancaster County, Lancaster, Pennsylvania.
- ***William Elliot House, New Castle County, Delaware (1988)***. Prepared for the Delaware Department of Transportation.

- **Andrew Jackson Williams House, New Castle County, Delaware (1988).** Prepared for the Delaware Department of Transportation.

#### HABS/HAER DOCUMENTATIONS

- **Housing Units D and E, United States Penitentiary, Lompoc, Santa Barbara County, California.** HABS-level II narrative report describing the physical characteristics and historical significance of former U.S. Army disciplinary barracks erected in 1946 following the telephone plan of prison construction. Prepared for the Federal Bureau of Prisons. 2004.
- **Landaff Bridge, Town of Landaff, Grafton County, New Hampshire.** HAER narrative report describing physical attributes and historical significance of continuous rigid frame concrete bridge built in 1933 incorporating two plate girder spans. Prepared for the New Hampshire Department of Transportation. 2003.
- **Walter Reed Army Medical Center, Washington, D.C.** Principal Investigator for HABS recordation of eight officers' quarters and interior survey of former Nurses' Quarters. HABS documentation included photographs of each dwelling, photographic reproduction of historic architectural drawings, measured drawings of four elevations of one dwelling, and eight Short Format narrative reports. Interior survey entailed documentation of original fabric of National Register-eligible Nurses' Quarters and development of treatment plan as part of pilot study for management of facility's significant interior spaces. Reports document dwellings originally erected during the 1920s as part of suburban development and subsequently acquired by the medical center for officers' quarters. For Walter Reed Army Medical Center and Baltimore District, United States Army Corps of Engineers. 2002.
- **Farview State Hospital Agricultural Complex, Canaan Township, Wayne County, Pennsylvania.** Narrative Report following HABS level II standards prepared for the Pennsylvania Bureau for Historic Preservation. Report documents twentieth century institutional farm associated with state hospital for the criminally insane. 2001.
- **Naval Hospital Philadelphia, Philadelphia, Philadelphia County, Pennsylvania.** Narrative Format report for 50-acre hospital complex. Original complex erected between 1932 and 1935; greatly enlarged during World War II. 1999.
- **Hospital (Building 1), and Nurses' Quarters (Building 3), Naval Hospital Philadelphia, Philadelphia, Philadelphia County, Pennsylvania.** Outline Format reports of 15-story Art Deco main hospital building and quarters built 1932-1935. 1999.
- **Ward 10 (Building 10), Naval Hospital Philadelphia, Philadelphia, Philadelphia County, Pennsylvania.** Short Format report of hospital ward building erected in 1942. 1999.
- **Buildings 35, 523/524, and Hangar 2, Naval Air Station Brunswick, Brunswick, Cumberland County, Maine.** Narrative reports of Cold War-era buildings prepared for the Maine Historic Preservation Commission following HABS and HAER Narrative Format and Short Format. 1999.
- **Building 373, Naval Air Station Brunswick, Topsham, Sagadahoc County, Maine.** Written report documenting Cold War-era building prepared for the Maine Historic Preservation Commission following HABS Narrative Format. 1999.

- **Long Hook Farm, Wilmington vicinity, New Castle County, Delaware.** With David L. Ames, Bernard L. Herman, Rebecca J. Siders, Hubert F. Jicha III, and Gabrielle M. Lanier. Measured and field recorded circa-1680 dwelling. 1989.
- **William Elliot House, Wilmington vicinity, New Castle County, Delaware.** Measured 1870s dwelling and prepared ink-on-Mylar floor plans. 1988.
- **Wilson-Warner Stable, Odessa, New Castle County, Delaware.** Measured and field recorded circa-1780 stable. 1988.
- **Andrew Jackson Williams House, Wilmington vicinity, New Castle County, Delaware.** Measured circa 1870 dwelling and prepared ink-on-mylar floor plans. 1988.
- **Achmester Smokehouse, Armstrong Corner vicinity, New Castle County, Delaware.** Measured and prepared ink-on-mylar elevations, floor plan, and axonometric view of 1840s smokehouse. 1986.
- **Peters Barn, Mill Creek Hundred, New Castle County, Delaware.** Measured and prepared ink-on-mylar elevations, floor plans, and section of circa-1800 bank barn. 1985.

**KRISTIE BAYNARD**  
*The Louis Berger Group Inc.*  
*Architectural Historian*

**EDUCATION**

- M.S., Historic Preservation, Columbia University, 2000
- B.A., Historic Preservation, Mary Washington College, 1996

**PROFESSIONAL AFFILIATIONS**

- Vernacular Architecture Forum
- National Trust for Historic Preservation
- American Association for State and Local History

**PROFESSIONAL EXPERIENCE**

Ms. Baynard has experience as a professional architectural and cultural resource historian across the states of Virginia, Maryland, New Jersey, West Virginia, Kentucky, and the District of Columbia. Her experience includes managing and conducting architectural surveys and historical research for a variety of federal, state, and local agencies. Projects include large- and small-scale surveys of historic and non-historic resources, cultural resource landscapes, and rural historic districts. She has prepared numerous nominations of districts and individual resources for listing in the National Register of Historic Places and prepared National Historic Landmark evaluations for the National Park Service. Projects undertaken in fulfillment of Section 106 of the National Historic Preservation Act of 1966 include National Register eligibility evaluations, impact assessments, and HABS-level measured drawings under mitigation requirements. A sample of recent projects is listed below.

- **City of Newport News Franchise Research, Newport News, Virginia.** Conducted research regarding franchise agreements between City of Newport News and its predecessors and Virginia Power.
- **Sentinel Pipeline, Belle Mead, New Jersey.** Prepared New Jersey Inventory forms and eligibility evaluations on six properties.
- **Broadcast Center One, Washington, D.C.** Conducted archival research and authored findings regarding five alley lots.

**PREVIOUS PROFESSIONAL EXPERIENCE**

**Principal, Arcadia Preservation, LLC, Keswick, Virginia and Frederick, Maryland.** Conducted and managed large-scale reconnaissance surveys as well as intensive surveys of historic properties in Virginia and Maryland. Researched, authored, and edited National Register Nominations and National Historic Landmark applications for historic districts, individual properties, and rural historic districts. Produced measured drawings for HABS-level documentation on individual sites. Responsible for seeking requests for proposals, writing proposals, interviews with potential clients, project management, and presentations at public meetings. 2004-2006. Selected projects are listed below.

- **Albemarle County Survey, Albemarle County, Virginia.** Principal Investigator for a reconnaissance survey of 93 properties. A cost-share project between Albemarle County, Virginia, and Virginia Department of Historic Resources (VDHR).

- **Southern Albemarle Rural Historic District, Albemarle County, Virginia.** Principal Investigator for a reconnaissance survey of a rural historic district in Albemarle County. District included the survey of 1,570 properties and covered approximately 80,000 acres. Conducted and managed the survey, organized materials, managed and entered GIS data, entered information into Virginia's Data Sharing System (DSS) for newly surveyed properties and updated existing surveys of previously recorded properties, and prepared all survey products for submittal to Virginia Department of Historic Resources. Co-authored National Register nomination. For Southern Albemarle Community Association.
- **Bel Aire, National Register Nomination, Albemarle County, Virginia.** Prepared the National Register nomination on Bel Aire (1827), a Federal-style house significant for its Germanic architectural influences and association with local Albemarle County politics.
- **Mechum River Farm, National Register Nomination, Albemarle County, Virginia.** Prepared National Register nomination on Mechum River Farm (1820/1850), a modest Gothic Revival-style farm cottage.
- **Schuyler Historic District, National Register Nomination, Nelson County, Virginia.** Conducted survey of 152 properties within Schuyler, significant as a soapstone mining town and as home of Earl Hamner, Jr., creator of the *The Waltons*. Wrote nomination with Jennifer B. Hallock. Cost-share project between Nelson County and VDHR.
- **Kenridge, Preliminary Evaluation Form, Charlottesville, Virginia.** Prepared PIF of Kenridge (1922), significant as an excellent local example of a Neo-classical dwelling.
- **Black Meadow, National Register Nomination, Orange County, Virginia.** Conducted research, survey, and co-authored nomination on Black Meadow (1856), significant for its Greek Revival style and its renovation in the early twentieth century reflecting cultural and economic changes.
- **Oak Hill, HABS-Level Documentation, Fairfax, Virginia.** Fieldwork for measured drawings of Oak Hill for Northern Virginia Conservation Trust.
- **Shreve House, HABS-Level Documentation, Arlington, Virginia.** Fieldwork for measured drawings of Shreve House for Northern Virginia Conservation Trust.
- **824 Sophia Street, Fredericksburg, Virginia.** Prepared an historical evaluation and provided professional opinion to City Council regarding demolition. Consultant for local businessmen in conjunction with Historic Fredericksburg Foundation.
- **Taylorstown Historic District Boundary Expansion, National Register Nomination, Loudoun County, Virginia.** Principal Investigator for survey of five properties expanding the original district. Prepared for local residents.

**Architectural Historian and Project Manager, EHT Tracerics, Inc., Washington, D.C.** Managed and conducted large-scale surveys of historic properties, including county and city-wide reconnaissance survey projects as well as county-wide thematic surveys. Conducted numerous intensive surveys in Maryland and Virginia. Authored National Register Nominations on historic districts, individual properties, and rural historic districts. Conducted surveys and authored determinations of eligibility for

Section 106 projects proposed by the Maryland Department of Transportation. Prepared HABS documentation on individual sites including measured drawings. 2001-2004. Selected projects are listed below.

- **African-American Thematic Study of Baltimore County, Maryland.** Project Manager for the survey of all African-American communities in Baltimore County totaling 39 areas and ranging from one building to over 1,000 properties. Conducted historical research and prepared all forms for the Maryland Inventory of Historic Properties (MIHP). Authored survey report and provided evaluations of National Register eligibility on each district and significant individual properties. Cost-share project between Baltimore County and Maryland Historical Trust (MHT).
- **Hyattsville Historic District Boundary Expansion, Prince Georges County, Maryland.** Project Manager for the survey of the original historic district and boundary expansion. Co-author with Laura V. Trieschmann.
- **County-wide Survey of Arlington County, Virginia.** Project Manager of four phases of survey in Arlington County, including 5,000 properties. Cost-share projects between Arlington County and VDHR.
- **County-wide Survey of Baltimore County, Maryland.** Project Manager of second phase of survey of 330 properties. Conducted survey (both reconnaissance and intensive level), research, and prepared all products submitted to Maryland Historical Trust.
- **City-wide Survey of Fairfax City, Virginia.** Conducted survey of 500 properties dating to mid-twentieth century for the City of Fairfax.
- **Ballentine Place Historic District Survey, City of Norfolk, Virginia.** Conducted survey of 649 properties of an African-American community. Cost-share project with City of Norfolk and VDHR.
- **Chesterfield Heights Historic District Survey, City of Norfolk, Virginia.** Conducted survey of 326 properties of an African-American community. Cost-share project with City of Norfolk and VDHR.
- **County-wide Survey of Rappahannock County, Virginia.** Conducted survey of 300 properties of various types and periods. Cost-share project between Rappahannock County and VDHR.
- **Leesburg Survey, Loudoun County, Virginia.** Conducted survey of 50 properties for the Town of Leesburg.
- **Ashton Heights Historic District, Arlington County, Virginia.** Project Manager. Conducted survey, research, and prepared National Register nomination. Cost-share project between Arlington County and VDHR.
- **Ashburn Historic District Draft Preliminary Information Form.** Researched and authored PIF for VDHR.
- **Sunnyside National Register Nomination, Rappahannock County, Virginia.** Prepared National Register Nomination on Sunnyside, significant as the oldest continuously operated

commercial orchard within the county and for its assemblage of domestic and agricultural buildings dating to 1785 to 1954.

- **Lomax African Methodist Episcopal Zion Church, Arlington, Virginia.** Prepared National Register Nomination for this church significant as the oldest extant church in Arlington County, Virginia.
- **West End Historic District, City of Suffolk, Virginia Preliminary Information Form.** Researched and authored PIF for West End Historic District. Cost-share project between City of Suffolk and VDHR.

**Cultural Resource Historian, Cultural Resource Analysts, Inc., Lexington, Kentucky.** Conducted cultural historic surveys, made determinations of eligibility, developed historic contexts, proposed mitigation procedures, and authored Section 106 reports regarding projects proposed by the Kentucky Transportation Cabinet and cellular communication companies. Other responsibilities included developing historic preservation plans and preparing HABS documentation on individual sites involving AutoCAD measured drawings. 2000-2001. Selected projects are listed below.

- **Cultural Historic Survey of the Proposed Bridge Replacement and Reconstruction of KY 22, Henry and Owen Counties, Kentucky.**
- **Cultural Historic Survey of the Proposed Georgetown Northwest Bypass, Scott County, Kentucky.**
- **Cultural Historic Survey of the Proposed Widening of KY 55 from Big Springs Branch to Big Springs Presbyterian Church, Nelson County, Kentucky.**
- **Cultural Historic Survey of the Proposed I-64/Hurstborne Lane Interchange in Jefferson County, Kentucky.**
- **State Level Historic Documentation of Cook's Saloon, Campbell County, Kentucky.**
- **Cultural Historic Overview for the Proposed Reconstruction of the Outer Loop (KY 1065), Fegenbush Lane, and Beulah Church Road Intersection in Jefferson County, Kentucky.**
- **Cultural Historic Overview for the Proposed Construction of the West Albany Bypass in Clinton County, Kentucky.**
- **Historic Documentation of the Barrett's Creek Bridge, Carter County, Kentucky.**
- **Cultural Historic Assessment of the Ritchie Family Property, Perry County, Kentucky.**
- **A Cultural Historic Survey of the Proposed Widening of U.S. 31E from KY 245 to Plum Run Road, Bardstown, Nelson County, Kentucky.**
- **Cultural Historic Survey of the Proposed Widening of U.S. 31W from KY 1008 to the Tennessee State Line, Simpson County, Kentucky.**

- **A Cultural Historic Survey of the Proposed Smith Broadbent Cellular Communication Tower Site (KY0387), Montgomery, Trigg County, Kentucky.**
- **An Archaeological Investigation of a Proposed Cellular Communication Tower Site near the Community of Burna in Livingston County, Kentucky.**
- **Cultural Historic Assessment of Cook's Saloon, Campbell County, Kentucky.**
- **Cultural Historic Survey of the Proposed Reconstruction of U.S. 60 from KY 425 to the Intersection with U.S. 41A, Henderson County, Kentucky.**
- **Cultural Historic Survey of the Proposed Reconstruction of U.S. 62 from I-64 to the Georgetown Bypass, Scott County, Kentucky.**
- **Cultural Historic Survey of the Proposed Reconstruction of U.S. 42 from KY 227 to Jordan Way Road, Carroll County, Kentucky.**
- **Phase I Archaeological Survey and Architectural Assessment for a Proposed Cell Tower Site in Huntington, Cabell County, West Virginia.**

#### **PAPERS**

- **An Interpretation of Black and White Spaces in John Smith's Row Houses in Antebellum Petersburg, Virginia. Vernacular Architecture Forum, Newport, Rhode Island. 2001.**
- **African-American Settlement Patterns in Baltimore County, Maryland. Vernacular Architecture Forum, Harrisburg, Pennsylvania. 2004.**